



## **BLUE FOLDER ITEMS**

*Blue folder items are additional back up material to administrative reports and/or public comments received after the printing and distribution of the agenda packet for receive and file.*

### **Regular Meeting of the Planning Commission December 17, 2015**

#### **IX. New Business**

##### **9. Overview of Mixed-Use Zoning**

- Comment letters received after the distribution of the agenda
  - Peter Verenkovff dated December 17, 2015
  - Catherine M. Carlisle dated December 17, 2015
  - Carol Perry dated December 17, 2015

17 December 2015

To: Redondo Beach Planning Commission  
c/o: Aaron Jones, Community Development Director  
City of Redondo Beach  
425 Diamond Street  
Redondo Beach, CA 90277



Subject: **OVERVIEW & DISCUSSION OF MIXED USE ZONING DISTRICTS AND DEVELOPMENT REGULATIONS/STANDARDS**

Redondo Beach Planning Commissioners:

After comparing the City's Land Use Element excerpts and the City's Planning Ordinance excerpts provided in the Administrative Report topic #9, there appear to be some differences in language that are problematic. One extremely important example is how residential density is discussed in various documents.

In the attached document review file, I have annotated certain items which are either in conflict, ambiguous and/or potentially misleading. I suggest the City consider adding clear definitions to the development regulations and standards. A sample definition for net residential density might be:

NET RESIDENTIAL DENSITY

"Dwelling units-per-net-acre density only uses the land being developed with residential content. Net residential density excludes land dedications and other undevelopable land areas including land not specifically part of the residential or mixed use development."

I believe the differences in language highlighted in the attachment are a source of confusion between the City, Developers, and Residents when discussing Mixed Use developments. To specifically highlight this confusion, I offer the following example:

In February, a developer submitted a 180 Apt. Mixed Use project for approval. This project asked for a 20% density bonus and claimed a residential density of 54.9 DU/AC as part of 3.28 Acre Mixed Use Development project (149 Apt. - 45 DU/AC without DBL allowance).

In July, this same developer submitted a revised 149 Apt. Mixed Use project for approval. This proposal claimed a residential density of 35 DU/AC based on 4.275 Acres (using the gross lot size).

The two project proposals are on the same lot and have the same land area being developed for Mixed Use. In this case, the difference in Residential Density depends on whether land occupied by an existing hotel is included in determining Mixed-Use residential density. The hotel is being remodeled not redeveloped. The remodeled hotel will not have a residential component. Therefore, the hotel is NOT part of the Mixed-Use project being proposed. The hotel land area should NOT play a role in determining Mixed-Use residential density for this project.

The language used across the Cities' documents needs to ensure that the intent of the Redondo Beach General Plan is not circumvented. Using the above example, if the Hotel land area was an undevelopable ravine, the City and Residents would still be faced with the same overly dense project being proposed by a developer wishing to maximize the buildable portion of his lot.

A second standard/regulation that should be reviewed is the Floor-Area-Ratio (F.A.R.). Allowing the F.A.R. to increase from the commercial zoning maximum of 1.0 to 1.5 for Mixed-Use creates visually massive projects that will typically produce more traffic than the developments they replace. According to language in the General Plan



Housing Element, this increase in permissible F.A.R. is provided to induce developers to redevelop commercial properties with Mixed Use projects containing affordable housing. However, this increased intensity likely produces more commercial space than is needed by community and does not appear to be a necessary inducement for development of Mixed Use, at least in some MU zoned areas.

There are four primary MU/CR areas in Redondo Beach. The Residential Density and F.A.R. maximums are currently the same for all. Some areas are more urban than others and there is widely differing access to mass transit between them. Having the same Density and Intensity Standards for all MU/CR areas does not seem appropriate.

It is my hope that the Planning Commission will take this opportunity to direct the City Staff to recommend language changes to the development regulations/standards to bring them into alignment with the Redondo Beach General Plan and consider other changes that would be ensure unnecessary overdevelopment does not occur.

Thank you again for listening and working with the residents for the future of Redondo Beach.

Respectfully submitted,

[signed]

Peter B Verenkoff  
533 Via La Selva  
Redondo Beach, CA 90277  
Email: [petev@vsdev.com](mailto:petev@vsdev.com)  
Phone: (424) 634-9073

Ref: NEW BUSINESS DISCUSSION ITEM # 9 - Planning Commission 12/17/2015  
Attachment:

**When were the Mixed Use development regulations/standards amended and for what reason?**

General Plan Amendments:

The General Plan originally adopted the majority of the existing "Goals, Objectives, and Policies" (attached to this report) as part of the City's comprehensive General Plan update in 1992. Since their original adoption in 1992 there have been very few amendments to Mixed Use policies other than some strategic amendments in 2011 that were purposed to strengthen the desire of the City to make accommodations and ensure that any retail and professional office be developed in a "pedestrian-oriented" manner.

Zoning Ordinance Amendments:

Unlike the City's Mixed Use General Plan "Goals, Objectives, and Policies", which are largely unchanged since their original adoption, the City, conducted a thorough examination of the City's existing standards in 2010. The following is a very brief "line item list" of the changes that resulted from the City's investigations in 2010/2011:

- **Specific Purposes:** Every zoning district in the City's ordinance begins with a section titled "Specific Purposes". This section articulates the specific purposes of each unique zoning district. Two (2) new statements of purpose were added to the Mixed Use zones.
  - **1** The primary character of mixed-use developments should be commercial in nature, so as to integrate with and enhance the quality of the surrounding business district."
  - "Provide high quality, public open spaces equivalent to a minimum 10% of a project's Floor Area Ratio (FAR)."
- **2** **Hotels:**
  - Hotels, which had been prohibited, are now a conditionally permitted use.
- **Car Wash:**
  - Due to historical development patterns an allowance for this use was added and is now conditionally permitted in the MU-3A and MU-3B Zones, but not within a mixed use project
- **Offices:**
  - The previous development regulations only permitted offices on the ground floor to the rear of other permitted retail or service uses, or on the second floor.
  - Due to high vacancy rates of commercial uses on the ground floor the previous restrictions were changed to allow up to 50% of the ground floor frontage commercial space to be developed with Office.

# Summary of Comments on Verenkoff LTR to Planning Commission 12-17-2015.pdf

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[T] Number: 1 Author: PBV Subject: Highlight Date: 12/17/2015 10:18:39 AM

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What about mixed-use areas surrounded largely by residential? Where the current commercial is only a strip development?

[T] Number: 2 Author: PBV Subject: Highlight Date: 12/17/2015 10:23:46 AM

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Some MU/CR areas encourage hotels and others only allow "overnight accommodations" by reference (1.16.1). Why is the language not more direct?

- **Residential Density:**
  - The residential density for all mixed-use zones is currently a maximum of one unit for every 1,245 square feet of lot area or approximately 35 units per acre.
  - The residential density for these zones has occasionally been considered too high by the public and the Planning Commission. Primary concerns related to density have been traffic congestion and a lack of parking.
  - To address the past concerns (which remain today) it was proposed by the Community Development Department and recommended by the Planning Commission in 2010 to decrease the maximum residential density requirement to one unit for every 1,452 square feet of lot area or 30 units per acre.
    - While the Planning Commission and Staff recommended to the City Council a reduction in residential density to 30 units per acre, it was determined that such a change would trigger the requirement for a general election under Article 27 of the City Charter. The City Council accepted all recommendations of the Planning Commission except the density reduction due to the estimated \$180,000 cost of a public vote.
    - It is important to note that any future recommended change in density would likely trigger the need for an election on the matter.
- **Minimum Lot Size, Mixed-Use Projects:**
  - The prior standard stated that no projects containing both commercial and residential uses shall be permitted on lots with less than 15,000 square feet of lot area. At the time that the mixed-use standards were first developed there was an opinion that such projects could only actualize in a large-scale scenario.
  - The current standard still requires a 15,000 square foot minimum lot area for mixed-use projects. The Planning Commission may consider recommending that mixed use be allowed on smaller sites. While this would potentially result in smaller scale developments, staff should note that community members have challenged this proposed amendment as potentially triggering a public vote requirement by allowing an increase in the number of potential residential development units. A contrasting opinion would be that the same allowable land area zoned for mixed use would result in the same number of potential units regardless of lot size. Both arguments are worth consideration if the Commission desires to pursue a smaller lot size allowance for mixed use development.
- **Building Height:**
  - Previously projects that included both commercial and residential uses were subject to a 45 foot height limit. Height compatibility with surrounding land uses and unobstructed views of scenic vistas are among the main concerns with mixed-use developments, especially those in coastal zones. Therefore, a decrease in the maximum height allowance from the 45 foot height limit to a 38 foot height limit was approved.

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Number: 1 Author: PBV Subject: Highlight Date: 12/17/2015 10:26:43 AM

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This language is not consistent with the General Plan. Should say "buildable lot area or approximately 35 units per net acre."

<p><b>Residential density.</b></p> <ul style="list-style-type: none"><li>• The maximum number of dwelling units on a lot shall be no more than one unit for each 1,245 square feet of lot area.</li></ul>
<p><b>Minimum lot size, mixed-use projects.</b></p> <ul style="list-style-type: none"><li>• 15,000 square feet of lot area.</li></ul>
<p><b>Building height.</b></p> <ul style="list-style-type: none"><li>• <b>Commercial uses*</b>. For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet. (Not Applicable to MU-3C Zone Only)</li><li>• <b>Mixed-use</b>. For projects including both commercial and residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.<ul style="list-style-type: none"><li>• <b>Mixed-use*</b>. For projects including both commercial and residential uses, no building or structure shall exceed a height of forty-five (45) feet. (MU-2 Zone Only)</li></ul></li><li>• <b>Residential uses*</b>. For projects containing only residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review. [MU-1 Zone Only (Artesia Boulevard). Residential Only Not Permitted in MU-3, MU-3A, MU-3B, and MU-3C Zones]</li><li>• <b>Residential uses*</b>. For projects containing only residential uses, no building or structure shall exceed a height of forty-five (45) feet. (MU-2 Zone Only)</li></ul>
<p><b>Stories.</b></p> <ul style="list-style-type: none"><li>• <b>Commercial uses*</b>. For projects containing only commercial uses, no building shall exceed two (2) stories. (Not Applicable to MU-3C Zone Only)</li><li>• <b>Mixed-use</b>. For projects including both commercial and residential uses, no building shall exceed three (3) stories.</li><li>• <b>Residential uses*</b>. For projects containing only residential uses, no building shall exceed three (3) stories. [MU-1 and MU-2 Zone Only. "Residential Only" Not Permitted in MU-3, MU-3A, MU-3B, and MU-3C Zones.]</li></ul>
<p><b>Setbacks.</b> The minimum setback requirements shall be as follows:</p> <ul style="list-style-type: none"><li>• <b>Front setback.</b><ul style="list-style-type: none"><li>○ <b>Minimum required*</b>. There shall be a minimum front setback average of five (5) feet (MU-1 Zone) and fifteen (15) feet (MU-2 Zone) ten (10) feet (MU-3, MU-3A, and MU-3B Zones) and three (3) feet (MU-3C</li></ul></li></ul>



Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<p><b>Goal 1H</b> <b>General Corridor</b></p>	<p><b>Pacific Coast Highway:</b> Continue and enhance existing commercial districts which contribute revenue to the City and are compatible with adjacent residential neighborhoods.</p> <p>Pacific Coast Highway is South Redondo's main commercial street and north-south artery. Previously, almost the entire length of Pacific Coast Highway was under one commercial zoning designation. In the development of the General Plan, one of the objectives for Pacific Coast Highway was to differentiate sections of the corridor in terms of both the types and intensities of uses. The intent of this strategy was to (1) provide aesthetic relief and contrast along this long linear corridor, and (2) enhance the economic vitality of the corridor by "breaking up" the supply of land for different uses.</p> <p>The economic study prepared for the General Plan showed that there was too much commercial development capacity in relation to forecasted demand. By changing some portions of Pacific Coast Highway to multiple-family residential, this creates a better balance between the supply of land and the economic demand for commercial and residential uses</p>
<p style="text-align: center;"><b>Sub-Area 1: Mixed-Use Node-Palos Verdes Boulevard and South</b></p> <p>For a general discussion of mixed use development, see Artesia Boulevard: Sub-Area 3.</p> <p>This area was designated for mixed use development ("MU-3") primarily because of its physical suitability for development of this scale. In particular, this area features lot depths in excess of 300 feet and is adjoined to the rear by high density apartment complexes situated at a higher elevation. Because of these factors, this area is more capable of supporting larger scale, higher intensity development without creating undue impacts. This fairly large area also provides a significant opportunity for the production of new affordable multiple-family housing.</p> <p>The standards for this area include a few differences from other mixed use areas. These are (1) residential units can be developed on the ground floor of buildings located behind buildings with ground floor commercial uses; and (2) the minimum commercial floor area ratio of 0.3 applies only the first 130 feet of property depth. These standards were instituted since it was not felt to be economically or physically practicable to extend commercial uses entirely to the rear of these relatively deep sites.</p>	
<p><b>Objective 1.21</b></p>	<p>Provide for the development of community-serving retail and office commercial and mixed-use projects integrating residential with commercial uses southeast of Palos Verdes Boulevard as a primary activity center of the City.</p>

Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<p><b>Policies 1.21.1</b> <i>Permitted Uses</i></p>	<p><sup>1</sup>Accommodate the development of pedestrian-oriented retail, professional office, and related land uses as permitted by Policies 1.16.1 and 1.17.2 on parcels designated as "MU-3" (II.1).</p>
<p><b>Policies 1.21.2</b> <i>Permitted Uses</i></p>	<p>Accommodate residential uses according to the following standards: a. along the street frontage: on the second floor or higher of structures developed with commercial uses on the lower levels; and b. structures located behind street-facing mixed retail and residential buildings: on any floor (including the ground floor) or on the second level or higher with retail or parking located on the ground floor (II.1).</p>
<p><b>Policies 1.21.3</b> <i>Density/Intensity and Height</i></p>	<p>Permit development of sites exclusively for commercial uses to a maximum intensity of a floor area ratio of 1.0 and height of two stories (30 feet) (II.1).</p>
<p><b>Policies 1.21.4</b> <i>Density/Intensity and Height</i></p>	<p><sup>2</sup>Permit the development of mixed-use structures integrating residential with commercial uses to a maximum intensity of a floor area ratio of 1.5 and three stories (45 feet), providing that: a. all floor area exceeding the ratio of 0.7 is developed for residential units; <sup>3</sup>b. the maximum residential density for "market-rate" units does not exceed 35 units per net acre; c. residential densities exceeding 35 units per net acre shall be developed for units affordable for low and moderate income households; and <sup>4</sup>d. a minimum floor area ratio of 0.3, applied to the first 130 feet of property depth from Pacific Coast Highway, is developed for commercial uses. (This shall not be interpreted to limit the siting of commercial uses to the first 130 feet of lot depth) (II.1).</p>
<p><b>Policies 1.21.5</b> <i>Design and Development</i></p>	<p>Require that commercial and mixed-use structures be designed to promote pedestrian activity in accordance with Policy 1.17.5 (II.1, II.7, II.18).</p>
<p><b>Policies 1.21.6</b> <i>Design and Development</i></p>	<p>Require that mixed-use structures be designed to mitigate potential conflicts in accordance with Policy 1.18.8 (II.1, II.7, II.18).</p>
<p><b>Policies 1.21.7</b> <i>Design and Development</i></p>	<p>Require that new development be sited and designed to convey a "village" character, including the: a. siting of structures on common pedestrian walkways, courtyards, and other open spaces; b. incorporation of arcades and other setbacks along the street frontage;</p>

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T Number: 1 Author: PBV Subject: Highlight Date: 12/17/2015 10:41:42 AM

The broad inclusion of 1.16.1 and 1.71.2 is problematic. Is it to be assumed that all land uses in these policies are "related land uses" and to be "developed" in this particular sub-area (subject to conditional approval)?

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T Number: 2 Author: PBV Subject: Highlight Date: 12/17/2015 11:00:50 AM

The maximum intensity shown for this MU area is probably higher than it should to be and should be examined. New commercial development exacerbates traffic issues and cannot be supported by the existing infrastructure.

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T Number: 3 Author: PBV Subject: Highlight Date: 12/17/2015 10:57:46 AM

This Existing General Plan explicitly specifies a maximum residential density based on net acreage. This policy was not directly translated into the City's development standards causing confusion. A comprehensive definition "net acre" is needed along with ensuring the same language in the development standards documents.

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T Number: 4 Author: PBV Subject: Highlight Date: 12/17/2015 11:01:21 AM

The maximum intensity shown for this MU area is probably higher than it should to be and should be examined. New commercial development exacerbates traffic issues and cannot be supported by the existing infrastructure.

Existing General Plan Goals-Objectives-Policies for Mixed Use Development

	<p>c. use of multiple building volumes and masses to reduce the “sense” of large scale “boxes” and create a visual fabric of multiple buildings;  d. incorporation of extensive facade modulation and articulation and design details;  e. use of roofline and height variations to break up massing and provide visual interest;  f. use of unified architectural design styles;  g. clear identification of building entrances;  h. extensive use of landscape (planting beds, raised planters, containers, or window boxes) which provides a three-dimensional character;  i. use of pedestrian-oriented signage (e.g., projecting signs) (II.1, II.7, II.18).</p>
<p><b>Policies 1.21.8</b>  <i>Design and Development</i></p>	<p>Require that building elevations above the second floor be set back in accordance with Policy 1.16.3 (II.1).</p>

Policies It shall be the policy of the City of Redondo Beach to:

Permitted Uses

1.16.1 Accommodate a mix of retail and service commercial, household supply and furnishings, eating and drinking establishments, food sales, drug stores, professional offices, art and cultural facilities, overnight accommodations, and similar uses which serve local and regional residents on parcels designated as “C-4” (II.1).

1.17.2 Accommodate professional, finance, insurance, real estate, and other offices at the rear or on the second level of structures whose ground floor frontage is occupied by pedestrian-active retail or similar uses on parcels designated as “C-2-PD” (II.1).

**Artesia Boulevard Corridor**

**Sub-Area 3: Mixed-Use Corridor-Blossom to West of Flagler Lane**

This is one of several areas within the City that has been designated for "mixed use." The mixed use designation permits commercial development by itself (and is therefore a commercial designation), but also permits the option of constructing residential units on the upper floors of a development with commercial uses on the ground floor. To complement the incorporated residential units, an emphasis is placed on a "pedestrian-oriented" character of the commercial component as described under the preceding sub-area.

The concept of mixing commercial and residential uses has been gaining in popularity in many cities. Traditional planning practice has dictated that residential uses should be physically separated and buffered from other types of "conflicting" uses. More recent experience, however, has shown that when properly planned and designed, mixed use developments can create a unique and positive environment for residents and businesses alike.

In mixed use developments, residential units are located and designed to provide sufficient privacy and security, while commercial uses are located and designed to provide easy accessibility and good visibility to the public.

While separated in this manner, the two types of uses also enjoy the benefits of their mutual proximity. For residents, they have the convenience and added dimension of having desirable retail businesses within a short walk. For businesses, they can draw vitality from having a "round-the-clock" source of patronage. This adds a type of "energy" to a development that would not exist if it were strictly commercial.

There are also several other potential benefits of mixed use development. These include (1) enhancing the opportunities for redevelopment of an area that may be currently lacking in vitality; (2) introducing a new and interesting form of development into the city; (3) increasing affordable housing opportunities and providing an alternative type of housing; and (4) helping to curb traffic congestion by decreasing the need for automobile trips.

**From:** Catherine M Carlisle  
**Sent:** Thursday, December 17, 2015 9:51 AM  
**To:** Aaron Jones <Aaron.Jones@redondo.org>  
**Subject:** High-density mixed use developments



To the Redondo Beach Planning Commission:

My parents built their home in the Hollywood Riviera in 1948. My mother still resides there, along with my sister and her family, who help care for her. My other sister has lived in the Hollywood Riviera for many years as well. After growing up here I knew the area was special and was thrilled in 1986 when my husband and I found a house to buy, one where we could raise our two sons. My husband and I still reside at the same property today.

Our home is located very near the intersection of Palos Verdes Blvd and Calle Mayor. We drive on Palos Verdes Blvd many times a day. The street, and particularly where it intersects PCH has already become overcrowded. The proposed mixed-use development (Legado) at that intersection will completely overwhelm it and severely impact the quality of life for residents in the area. Please urge the City Council to deny that project.

Mixed-use developments in general have no place in our area of South Redondo Beach/Hollywood Riviera. Previously built mixed-use developments built very close to the above proposed project (on PCH between Palos Verdes Blvd and Prospect) have been complete failures. Please rezone the area such that no more mixed-use developments can be allowed. Such developments are completely out of character with the surrounding area, overburden the infrastructure, the water and sewer systems, and cause nightmarish traffic congestion. These are more appropriate for areas like downtown Los Angeles.

I urge you to keep our area's character, charm, and livability by prohibiting further mixed-use developments. They make no sense here.

Thank you,

Catherine Carlisle  
102 Via Pasqual  
Redondo Beach, CA 90277

**From:** Carol Perry  
**Sent:** Thursday, December 17, 2015 9:58 AM  
**To:** Aaron Jones <Aaron.Jones@redondo.org>  
**Subject:** Planning Commission -December 17th



Members of Planning Commission,

I cannot be present at the meeting tonight due to illness, but I ask the commission to change mixed-use zoning in Redondo to avoid large projects, incompatible with our beach community, such as Legado. I note continually the number of vacant commercial properties in Riviera Village and along Pacific Coast Highway.

I urge you to reduce the maximum number of units per square acre to 30 from 35., and change buildable area from gross to net buildable area.

Thank you for your consideration.

Carol Perry  
413 Avenue G, Unit 4  
Redondo Beach, CA 90277

**AGENDA – REGULAR MEETING  
PLANNING COMMISSION  
CITY OF REDONDO BEACH  
THURSDAY DECEMBER 17, 2015 – 7:00 P.M.  
CITY COUNCIL CHAMBERS  
415 DIAMOND STREET**

**I. OPENING SESSION**

1. Call Meeting to Order
2. Roll Call
3. Salute to the Flag

**II. APPROVAL OF ORDER OF AGENDA**

**III. CONSENT CALENDAR**

*Routine business items, except those formally noticed for public hearing (agendized as either a “Routine Public Hearing” or “Public Hearing”), or those items agendized as “Old Business” or “New Business” are assigned to the Consent Calendar. The Commission Members may request that any Consent Calendar item(s) be removed, discussed, and acted upon separately. Items removed from the Consent Calendar will be taken up immediately following approval of remaining Consent Calendar items. Remaining Consent Calendar items will be approved in one motion.*

4. Approval of Affidavit of Posting for the Planning Commission meeting of December 17, 2015.
5. Approval of the following minutes: Regular Meeting of November 19, 2015.
6. Receive and file the Strategic Plan Update of November 17, 2015
7. Receive and file written communications.

**IV. AUDIENCE OATH**

**V. EX PARTE COMMUNICATIONS**

*This section is intended to allow all officials the opportunity to reveal any disclosure or ex parte communication about the following public hearings.*

**VI. EXCLUDED CONSENT CALENDAR ITEMS**

**VII. PUBLIC HEARINGS**

**VIII. OLD BUSINESS**

*Items continued from previous agendas.*

**IX. NEW BUSINESS**

*Items for discussion prior to action.*

8. Update on Community Growth and Development  
Recommendation: Receive and file memorandum and presentation slides
9. Overview of Mixed-Use Zoning  
Recommendation: Receive and file report

**X. PUBLIC PARTICIPATION ON NON-AGENDA ITEMS**

*This section is intended to provide members of the public with the opportunity to comment on any subject that does not appear on this agenda for action. This section is limited to 30 minutes. Each speaker will be afforded three minutes to address the Commission. Each speaker will be permitted to speak only once. Written requests, if any, will be considered first under this section.*

**XI. COMMISSION ITEMS AND REFERRALS TO STAFF**

*Referrals to staff are service requests that will be entered in the City's Customer Service Center for action.*

**XII. ITEMS FROM STAFF**

**XIII. COUNCIL ACTION ON PLANNING COMMISSION MATTERS**

**XIV. ADJOURNMENT**

The next meeting of the Planning Commission of the City of Redondo Beach will be a Regular Meeting to be held at 7:00 p.m. on Thursday, January 21, 2016 in the Redondo Beach City Council Chambers, 415 Diamond Street, Redondo Beach, California.

Any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda will be made available for public inspection at the City Clerk's Counter at City Hall located at 415 Diamond Street, Door C, Redondo Beach, Ca. during normal business hours. In addition, such writings and documents will be posted, time permitting, on the City's website at [www.redondo.org](http://www.redondo.org).

It is the intention of the City of Redondo Beach to comply with the Americans with Disabilities Act (ADA) in all respects. If, as an attendee or a participant at this meeting you will need special assistance beyond what is normally provided, the City will attempt to accommodate you in every reasonable manner. Please contact the City Clerk's Office at (310) 318-0656 at least forty-eight (48) hours prior to the meeting to inform us of your particular needs and to determine if accommodation is feasible. Please advise us at that time if you will need accommodations to attend or participate in meetings on a regular basis.

An agenda packet is available 24 hours at [www.redondo.org](http://www.redondo.org) under the City Clerk and during City Hall hours, agenda items are also available for review in the Planning Department.

**CONSENT CALENDAR**

The Planning Commission has placed cases, which have been recommended for approval by the Planning Department staff, and which have no anticipated opposition, on the Consent Calendar section of the agenda. Any member of the Planning Commission may request that any item on the Consent Calendar be removed and heard, subject to a formal public hearing procedure, following the procedures adopted by the Planning Commission.

All cases remaining on the Consent Calendar will be approved by the Planning Commission by adopting the findings and conclusions in the staff report, adopting the Exemption Declaration or certifying the Negative Declaration, if applicable to that case, and granting the permit or entitlement requested, subject to the conditions contained within the staff report.

Cases which have been removed from the Consent Calendar will be heard immediately following approval of the remaining Consent items, in the ascending order of case number.

**RULES PERTAINING TO ALL PUBLIC TESTIMONY**

(Section 6.1, Article 6, Rules of Conduct)

1. No person shall address the Commission without first securing the permission of the Chairperson; provided, however, that permission shall not be refused except for a good cause.

2. Speakers may be sworn in by the Chairperson.
3. After a motion is passed or a hearing closed, no person shall address the Commission on the matter without first securing permission of the Chairperson.
4. Each person addressing the Commission shall step up to the lectern and clearly state his/her name and city for the record, the subject he/she wishes to discuss, and proceed with his/her remarks.
5. Unless otherwise designated, remarks shall be limited to three (3) minutes on any one agenda item. The time may be extended for a speaker(s) by the majority vote of the Commission.
6. In situations where an unusual number of people wish to speak on an item, the Chairperson may reasonably limit the aggregate time of hearing or discussion, and/or time for each individual speaker, and/or the number of speakers. Such time limits shall allow for full discussion of the item by interested parties or their representative(s). Groups are encouraged to designate a spokesperson who may be granted additional time to speak.
7. No person shall speak twice on the same agenda item unless permission is granted by a majority of the Commission.
8. Speakers are encouraged to present new evidence and points of view not previously considered, and avoid repetition of statements made by previous speakers.
9. All remarks shall be addressed to the Planning Commission as a whole and not to any member thereof. No questions shall be directed to a member of the Planning Commission or the City staff except through, and with the permission of, the Chairperson.
10. Speakers shall confine their remarks to those which are relevant to the subject of the hearing. Attacks against the character or motives of any person shall be out of order. The Chairperson, subject to appeal to the Commission, shall be the judge of relevancy and whether character or motives are being impugned.
11. The public participation portion of the agenda shall be reserved for the public to address the Planning Commission regarding problems, question, or complaints within the jurisdiction of the Planning Commission.
12. Any person making personal, impertinent, or slanderous remarks, or who shall become boisterous while addressing the Commission, shall be forthwith barred from future audience before the Commission, unless permission to continue be granted by the Chairperson.
13. The Chairperson, or majority of the members present, may at any time request that a police officer be present to enforce order and decorum. The Chairperson or such majority may request that the police officer eject from the place of meeting or place under arrest, any person who violates the order and decorum of the meeting.
14. In the event that any meeting is willfully interrupted so as to render the orderly conduct of such meeting unfeasible and order cannot be restored by the removal of individuals willfully interrupting the meeting, the Commission may order the meeting room cleared and continue its session in accordance with the provisions of Government Code subsection 54957.9 and any amendments.

### **APPEALS OF PLANNING COMMISSION DECISIONS:**

All decisions of the Planning Commission may be appealed to the City Council. Appeals must be filed, in writing, with the City Clerk's Office within ten (10) days following the date of action of the Planning Commission. The appeal period commences on the day following the Commission's action and concludes on the tenth calendar day following that date. If the closing date for appeals falls on a weekend or holiday, the closing date shall be the following business day. All appeals must be accompanied by an appeal fee

of 25% of original application fee up to a maximum of \$500.00 and must be received by the City Clerk's Office by 5:00 p.m. on the closing date.

Planning Commission decisions on applications which do not automatically require City Council review (e.g. Zoning Map Amendments and General Plan Amendments), become final following conclusion of the appeal period, if a written appeal has not been filed in accordance with the appeal procedure outline above.

No appeal fee shall be required for an appeal of a decision on a Coastal Development Permit application.

December 10, 2015

STATE OF CALIFORNIA        )  
COUNTY OF LOS ANGELES    )        ss  
CITY OF REDONDO BEACH     )

**AFFIDAVIT OF POSTING**

Pursuant to the requirements of Government Code Section 54955, agendas for a regular commission meeting must be posted at least seventy-two (72) hours in advance and in a location that is freely accessible to members of the public. As Planning Analyst of the City of Redondo Beach, I declare, under penalty of perjury, that in compliance with the requirements of Government Code Section 54955, I caused to have posted on Thursday December 10, 2015, the agenda for the December 17, 2015 Regular Meeting of the City of Redondo Beach Planning Commission in the following locations:

City Hall, Door "A", 415 Diamond Street, Redondo Beach  
City Clerk's Counter, Door "C", 415 Diamond Street, Redondo Beach



Lina Portolese  
Planning Analyst



I, Lina Portolese, hereby declare, under penalty of perjury, that I am over the age of 18 years and am employed by the City of Redondo Beach, and that the following document: Planning Commission Regular Meeting Agenda of December 17, 2015 was posted by me at the following location(s) on the date and hour noted below:

Posted on: 12/10/2015 at 4:30 pm  
(date) (time)

Posted at: City Hall, Door "A", 415 Diamond Street, Redondo Beach

City Clerk's Counter, Door "C", 415 Diamond Street, Redondo Beach

|  
  
Signature

12/10/15  
Date

**CALL TO ORDER**

The Regular Meeting of the Planning Commission was called to order by Chairperson Rodriguez at 7:00 p.m. in the City Hall Council Chambers, 415 Diamond Street.

**ROLL CALL**

Commissioners Present:      Biro, Gaian, Goodman, Ung, Rodriguez  
Commissioners Absent:       Mitchell, Sanchez  
Officials Present:            Aaron Jones, Community Development Director  
                                      Sean Scully, Planning Manager  
                                      Cheryl Park, Assistant City Attorney  
                                      Genny Ochoa, Recording Secretary

**SALUTE TO THE FLAG**

Commissioner Ung led the Commissioners and audience in a Salute to the Flag.

**APPROVAL OF ORDER OF AGENDA**

Motion by Commissioner Goodman, seconded by Commissioner Ung, to approve the order of agenda as presented. Motion carried unanimously.

**CONSENT CALENDAR ITEMS**

Motion by Commissioner Biro, seconded by Commissioner Ung, to approve the following Consent Calendar Items, and by its concurrence, the Commission:

4. Approved Affidavit of Posting for the Planning Commission meeting of November 19, 2015.
5. Approved the following minutes: Regular Meeting of October 15, 2015.
6. Received and filed the Strategic Plan Update: No update since last month
7. Received and filed written communications.

**AUDIENCE OATH**

Chairperson Rodriguez asked that those people in the audience who wish to address the Commission on any of the hearing issues stand and take the following oath:

Do each of you swear or affirm that the testimony you shall give shall be the truth, the whole truth, and nothing but the truth?

Audience members stood and answered, "I do."

**EX PARTE COMMUNICATIONS**

Chairperson Rodriguez disclosed speaking with residents regarding the Legado project.

**EXCLUDED CONSENT CALENDAR**

None.

## **PUBLIC HEARINGS**

### **8. APPROVE CONSTRUCTION OF A 4-UNIT RESIDENTIAL CONDOMINIUM DEVELOPMENT AND VARIANCE TO ALLOW TANDEM CONFIGURATION OF GUEST PROPERTY 111 VISTA DEL MAR CASE NO. 2015-11-PC-012**

Community Development Director Aaron Jones recommended that the Commission receive and file revised plans that eliminate the request for a Variance for the tandem configuration of guest parking.

Motion by Commissioner Goodman, seconded by Commissioner Biro, to open the Public Hearing and receive and file all documents regarding Case No. 2015-11-PC-012, as revised, the applicant being Bagnard Co. LLC, to consider approval of an Exemption Declaration, Conditional Use Permit, Planning Commission Design Review, Coastal Development Permit, Vesting Tentative Parcel Map No. 73613 for the construction of a 4-unit residential condominium development, on property located within a Medium-Density Multiple-Family Residential (RMD) zone, in the Coastal Zone.

Planning Manager Sean Scully presented the staff report and described the project through a PowerPoint presentation which included review of the existing property site, and the following:

#### **BACKGROUND AND REQUEST**

- Zoned RMD – Medium Density Multiple-Family Residential Zone
- Approximately 10,625 square feet in area
- Project meets all RMD Development Standards
  - Original application included a Variance for the reduction in guest parking spaces
  - The Architect was able to re-design the project
- Design considerations
  - Maximize on-site parking
  - High quality aesthetic architecture with varied setbacks and relief
  - Significant varied setbacks between project and western property line to best ensure compatibility with existing adjacent development to the west
    - Side yard setbacks vary from 7' to 27'
- Significant and enhanced landscaping
- Conditional Use Permit – 4 Unit Condominium Project
- Planning Commission Design Review – Mediterranean Style of Architecture
- Vesting Tentative Tract Map
- Coastal Development Permit

Planning Manager Scully emphasized that on-site parking was maximized by providing four parking spaces per unit plus a guest parking space for a total of 17 parking spaces. Mr. Scully added that consideration was given to high quality aesthetics and architecture. Mr. Scully noted that units 1 and 2 access onto Vista del Mar and units 3 and 4 access Vista del Mar but that the front doors face to the west. Mr. Scully reviewed the floor plans and landscape plans for the proposed project.

Planning Manager Scully concluded his presentation by recommending approval of the Conditional Use Permit and Planning Commission Design Review, Coastal Development Permit, Vesting Tentative Parcel Map. No. 73613 and exemption Declaration for the 4-unit condominium project subject to the plans, staff report, and conditions of approval.

Chairperson Rodriguez invited the project applicant to speak.

Mr. Jan Trobaugh, project architect, stated that street parking was limited and spoke on their effort to provide parking spaces on site and not impact the neighboring properties. Mr. Trobaugh added that each owner would get four parking spaces that they would control. Mr. Trobaugh described the 2-story building design and stated that there were no basements or mezzanines, and that the stairways to the roof decks were not visible to the public. Mr. Trobaugh added that the common open space on the ground was on decks and was part of the roof deck on unit 2.

In response to Commissioner Gaian regarding the fourth garage, Mr. Trobaugh explained that the garage had to be set back per code requirements as it did not allow side yard exterior setback. Mr. Trobaugh added that the project would prevent people from parking on the sidewalk, and added that the project had no vehicle access on Vista del Mar, and the sidewalk would be usable in front of the lot.

In response to Commissioner Ung, Mr. Trobaugh stated that enhanced visibility for vehicles backing up was provided as allowed by the Building Code. Planning Manager Scully added that staff would work with the Engineering Department and the applicant on a solution to increase line of sight and visibility for vehicles backing up.

In response to Commissioner Biro, Mr. Trobaugh stated that if the Engineering Department allows, the sidewalk would be pulled out and have a shorter curb cut.

In response to Chairperson Rodriguez regarding the letter from concerned citizens, Planning Manager Scully stated that there was no specific requirement in the zoning regulations to provide air and light; however, the issue was considered in the project design and the structure was pulled back to increase distance and height from the western property line.

In response to Chairperson Rodriguez, Community Development Director Aaron Jones explained that the City had requirements pertaining to construction nuisances and dust control.

Chairperson Rodriguez called for anyone wishing to speak for or against the proposed project. No one came forward.

Motion by Commissioner Ung, seconded by Commissioner Biro, to close the Public Hearing. Motion carried unanimously.

In response to Commissioner Biro, Community Development Director Jones stated that the project was applied for prior to adding standards to the pre-project checklist, however, a project rendering and landscape plan was provided. Mr. Jones added that the project qualified for the public art ordinance and the applicant would probably select to pay the required in-lieu public art fee. Mr. Jones further added that the project met

the City's landscaping standards by providing water-wise drought tolerant landscaping and a low water-drip irrigation system.

In response to Commissioner Goodman, Community Development Director Jones stated that the conditions of approval would be modified by adding Condition No. 36 requiring the applicant work with staff to maximize the depth of the guest parking to avoid encroachment onto the sidewalk.

Motion by Commissioner Biro, seconded by Commissioner Goodman, to approve an Exemption Declaration, Conditional Use Permit, Planning Commission Design Review, Coastal Development Permit, Vesting Tentative Parcel Map No. 73613 for the construction of a 4-unit residential condominium development, on property located within a Medium-Density Multiple-Family Residential (RMD) zone, in the Coastal Zone, Case No. 2015-11-PC-12, the applicant being Bagnard Co. LLC, subject to the eight findings and 35 conditions in the staff report, with Condition No. 36 added that applicant work with staff to adjust the driveway design to allow for maximum depth possible for the guest parking. Motion carried unanimously.

## **OLD BUSINESS**

### **9. APPROVE CONSTRUCTION OF A MIXED-USE DEVELOPMENT 1700 SOUTH PACIFIC COAST HIGHWAY CASE NO. 2015-03-PC-005**

Motion by Commissioner Biro, seconded by Commissioner Goodman, to open the continued Public Hearing to consider approval/certification of a (Revised) Mitigated Negative Declaration/Initial Environmental Study (IS-MND), and Mitigation Monitoring and Reporting Program (including Modified Mitigation Measures), a Conditional Use Permit, Design Review, Landscape and Irrigation Plan, Sign Review, and a Minor Subdivision (Vesting Tentative Tract Map No. 72662) to permit the construction of a mixed-use project with 149 residential apartment units, and approximately 37,000 square feet of neighborhood-serving commercial development with a total of 614 parking spaces at a maximum height of three stories and 45 feet above existing grade, and the renovation of an existing 110-room hotel, on property located within a Mixed Use (MU-3A) zone.

Community Development Director Jones presented the item and displayed a PowerPoint presentation which included a summary of the following:

#### **BACKGROUND**

- Last considered by Planning Commission – August 20, 2015
- 149-unit mixed use development
- Direction to significantly revise project mass, scale, bulk, and design
- Continued matter to November 19, 2015

#### **SINCE AUGUST**

- Staff actively worked with applicant to obtain revised plans
- New plans received November 5, 2015 (146 units)
- Only 4 working days to review, analyze, and write detailed report
- Letter to City Clerk on November 5, 2015 – requesting final vote

#### **MINUTES**

**PLANNING COMMISSION**

**NOVEMBER 19, 2015**

**PAGE 4**

- Applicant notified on November 6, 2015 – submittal incomplete
- More time and materials necessary to properly analyze revised 146-unit project

#### STAFF RECOMMENDATION

Accept further public testimony and either:

- Deny 149-unit mixed use development, after considering Study of Effects of Denial, and adopt Resolution of Denial  
OR
- Continue the public hearing to a date certain (min. 60 days) to allow applicant to complete the submittal of the revised project and perform further outreach.
- Deadlines: January 4<sup>th</sup> for January 21, 2016 Planning Commission hearing  
February 1<sup>st</sup> for February 18, 2016 Planning Commission hearing

Chairperson Rodriguez invited the project applicant to speak.

Edward Czucker, President and CEO of Legado Companies (Legado), stated that Legado has worked hard to respond to the City's directives. Mr. Czucker thanked those who were in the audience to support the project and stated that he wanted to clarify the misunderstanding on the revised application deadlines. Mr. Czucker noted that this was not a new application and that Legado submitted revised plans and results of outreach on time. Mr. Czucker displayed a PowerPoint presentation which included a thorough summary of the following:

- Background on Legado Redondo
- Project Background
- Outreach Efforts -  
Since August 2015: more than 50 meetings with residents, organizations, and community groups; multiple presentations to Save the Riviera, Riviera BID, Chamber of Commerce; and NRBBA; email project summary community members  
Before August 2015: community outreach began in 2012; communications with residents, HOAs, community groups, city staff and officials; 3 District Councilmember meetings in 2013; 3 open house meeting in 2015

Mr. Czucker submitted a binder to each Commissioner containing information on extensive outreach efforts for the past four years, emails, and correspondence.

Motion by Commissioner Goodman, seconded by Commissioner Biro, to receive and file the information binder. Motion carried unanimously.

Mr. Czucker continued his summary:

- History of Design Changes –  
Mr. Czucker stated that at the community's request the original design was changed from Mediterranean to Modern, which was reflected in the March 2015 application. Mr. Czucker added that based on continuation of meetings with the community, Legado was asked to change the design back to a Mediterranean design.
- Comparison Table of major changes made –

- Building Design: from Modern to Mediterranean
- Building Mass: from one 3-story building to five 1-, 2- 3-story smaller buildings
- Commercial: 36% reduction
- Residential: decreased from 149 to 146 units
- Relocation of restaurant and public open spaces to provide useful community gathering space as requested by community
- Building Height: reduced 5 feet

Mr. Czucker stated that Legado had received many positive comments to the changes made, which were included in the information binder submitted to the Commission.

Mr. Czucker stated that traffic would be reduced due to reduction in retail space. Mr. Czucker added that Legado hoped that the Planning Commission recognized Legado's good faith efforts to work with the City and community, and approve the project as presented. Mr. Czucker asked that the Planning Commission make the decision tonight.

Commissioner Gaian noted that he thought it was Mr. Czucker who had asked for an extension of the hearing at the last meeting.

Mr. Czucker responded that City staff had suggested that Legado request the extension to work with the community further, and the letter requesting the extension to this time was based on collaboration with staff.

Commissioner Ung asked about the deadlines for submission of the plans as well as emails from staff to Mr. Ki Ryu expressing concern for not having received plans. Commissioner Ung added that there was a disconnect from what Mr. Czucker was told and the email correspondence.

Mr. Czucker responded that there was a meeting between Fernando Villa, Ki Ryu and (Community Development Director) Aaron Jones to review timetables and discuss flexibility on times that gave them to the first week of November.

Fernando Villa (Allen Matkins Leck Gamble Mallory & Natsis LLP, Attorneys at Law), representing the applicant, came forward and stated that Mr. Czucker was not present at the September meeting with Mr. Aaron Jones, Ki Ryu and himself. Mr. Villa stated that Mr. Jones had conveyed that he would need project renderings, floor plans and site elevations, and if materials were submitted by early November it would be enough time because he had the other materials needed for (tonight's) meeting. Mr. Villa stated that Legado clearly understood the materials were due in early November and that in his letter of November 5<sup>th</sup> to the Commissioners he indicated they were submitting materials based on their understanding from staff. He added that if there was miscommunication, it was honest miscommunication and that they had worked hard to submit the materials. He further added that they had submitted more than what they were asked to provide.

Commissioner Ung stated that four working days was not enough time for staff to make a good analysis. He stated that he was perplexed as to the lack of communication.

Mr. Fernando Villa stated that Legado emailed staff in mid-October requesting meetings as soon as possible to discuss the materials required and they did not receive a

response. He stated that they wanted and asked for the meeting to make sure they were on the right path, however it was not until early November that they received a response from staff.

In response to Commissioner Gaian, Community Development Director Jones stated that staff did not recommend Planning Commission action on a project that staff had not reviewed.

Commissioner Gaian stated that tonight's hearing was to grant or not grant an extension or approve or not approve the 149-unit project. Mr. Gaian added that it was not practical to spend time discussing a project which was deemed incomplete.

Commissioner Goodman commended Legado for working with the community and making requested changes; however, he could not approve a project that had not been reviewed by City staff and lacked information. He added that it was reasonable to defer a decision until they had the required information to vote on the project.

Commissioner Biro stated that he liked Legado's level of work and presentation analyzing the project, but asked why they City was left out from the outreach efforts to complete the application.

Mr. Villa stated that this was the same application submitted for the 180-unit project submitted in February for the March 2015 meeting. He stated that when they met with the (Planning) Director they asked what was needed to make sure that the City had enough material for Commission action, and they understood that the City only needed renderings, floor plans and a site plan because the City already had all the supporting data. Mr. Villa stated they had included a comparison table and a revised traffic study and they had submitted material beyond what the City needed. Mr. Villa further stated that they did not believe they had an incomplete packet. Mr. Villa added that the Commission had the power and discretion to consider the 146-unit project and that it was staff who takes direction from the Commission. He stated that they asked Mr. Jones to advise the Commission on the merits of the project and added that they had changed the architectural design and downscaled the project, and had delivered what the City had asked for. Mr. Villa stated that the project was an excellent project.

Commissioner Biro stated that he understood the Commission's authority and that they needed to understand the project in order to make a decision. He further stated that the project presented had not been incorporated into staff's report.

Mr. Villa stated that they had submitted all project materials asked by the City which were forwarded to the Commission and were on tonight's agenda. Mr. Villa added that they disagreed with staff's report and stated that the Commission had the power to refine the conditions of approval and approve the project.

Chairperson Rodriguez commented on emails from City staff to Legado regarding deadlines.

Mr. Villa stated that they had responded and made efforts to communicate with staff and did not receive a response from staff.

Commissioner Ung stated that the Commission had a responsibility to do what was right for the developer, citizens, and all stakeholders, and that it would be a disservice to make a decision not supported by staff in haste. Mr. Ung stated he wanted to give every application time and respect on its merits and identify any potential shortcomings before making a decision. He stated he wanted to make a decision based on facts.

Mr. Villa stated that consideration of this application was a result of eight hearings and revisions to the project. He further stated that they have worked with staff, the Commission, and the community, and that a decision (at tonight's meeting) would not be in haste.

In response to Commissioner Gaian, Assistant City Attorney Park identified the Commission's options in making a decision on the project:

- 1) Denial of the 149-unit project if the Commission finds that there is substantial evidence to support the findings in the resolution attached to staff's report (pursuant to direction given to staff by the Commission in July 2015); or
- 2) Continue the Public Hearing to consider the revised plan for the 146-unit project, based on staff's inability to review the project (staff has deemed the revised plan as incomplete); or
- 3) Approve the 149-unit project, although there is no resolution for approval of the project before the Commission, however, a previously prepared resolution could be revived; or
- 4) Consider staff's comments that they have not had time to consider the revised plan that they deem as incomplete, and based on what is before the Commission, the Commission can vote to deny or approve the 146-unit proposal. The Commission must find that there is substantial evidence to support the decision to approve or deny the proposal.

Commissioner Goodman stated that he did not believe the Commission had enough information to make a decision "on the project which was not really officially proposed." He added that he would not feel comfortable making a decision on a project this size and recommended giving staff time to do a thorough review of a complete packet.

Mr. Villa stated that they wanted a decision tonight on the 146-unit project, but if it was not approved, he asked for approval of the 180-unit project since that application was still pending. If approval was not made on either one, Mr. Villa asked for approval of the 149-unit project.

Community Development Director Jones stated that the applicant had refiled the 149-unit project application, making it the only application before the Commission.

Chairperson Rodriguez called for those in the audience wishing to speak in favor of the 149-unit project.

Reuben Chan, Redondo Beach, supported the project and stated that more housing was needed for young families.

Chris Ray, 15300 Ventura Blvd., supported the project and stated that mixed-use projects offer great live/work opportunities. He commented on the City's housing and

affordability crisis and added that the project would create jobs and local businesses would benefit as well.

Anthony (last name inaudible) supported the project and stated that the Legado team was willing to listen and work with the community. He added that if a decision was not made tonight perhaps it could be considered at the next meeting.

Lash Cameron supported the project and commented on the limited housing opportunities for the young people of the community. Mr. Cameron stated that the Legado project would help bring people back to their home community.

Steve Sleck, 221 Via Alameda, supported the project and stated that he appreciates both City staff and Legado. He stated that Legado has reached out to the community and that the proposed project is beautiful. Mr. Sleck commented that everyone needs to be heard in a professional manner, and thanked the Commission.

Nelu Ardeljan, 342 Camino de las Colinas, spoke in favor of the project and stated that it was a beautiful project.

Gordon Joseph, Redondo Beach, supported the project and spoke of the lack of housing in the City. Mr. Joseph stated that the project would give opportunity for many to live in and enjoy the City.

Scott Tregarthen, 521 S. Juanita Ave., stated that he was a life-long resident of the City and that he was excited and in favor of the project. Mr. Tregarthen stated that the project would provide affordable housing opportunities for people who want to stay in the City.

Sheila Kutkus, 2304 Curtis Ave., supported the project and stated that affordable housing apartments were needed for the City's young people. Ms. Kutkus stated the Legado's project improvements were commendable and added that she was excited for the 146-unit project and that it would be a beautiful project.

Cathy Caplener, 718-B N. Juanita Ave., spoke in favor of the project and stated that the project would help the community and environment and would be good for tourism and businesses in the City. She further stated that the purpose of the project would benefit all parties, businesses, the community and environment, and make the City a better place.

Griselda Morales, 1700 S. Pacific Coast Highway, supported the project and stated that she has worked in the City for eight years and that she would love to live in the City. She stated that the project would provide housing opportunities for people like herself to live in the City.

Shawn Merritt, 6202 S. Pacific Coast Highway, #1, Torrance, stated that he was in favor of the 146-unit project and that he hoped the project moved forward. Mr. Merritt stated that the intersection was "broken" and the development was an opportunity for the City to make necessary repairs to the decaying infrastructure.

Brian Belten, 825 Bard, Hermosa Beach, supported the project and stated that it was worth looking into because of the opportunities it could provide. He added that the project could potentially revitalize the area.

Don Szerlip, Redondo Beach, discussed the number of City residents and residential units and stated that 146 additional dwellings would not be a major addition to the City. Mr. Szerlip stated that 52% of City residents were renters, yet no rental properties have been built since the '70s. Mr. Szerlip stated that the housing project was needed and read excerpts of an UCLA economic forecast regarding affordable housing. Mr. Szerlip shared his personal experience in his current search for housing. Mr. Szerlip suggested that the Commission postpone its decision and consider the 146-unit project as appropriate.

Michael Donovan, 2021 Curtis Ave., supported the project and stated he wanted an opportunity to raise his family in a quality project like this one. He stated that Legado had made changes to the project, reduced its size, scope, and architectural design, and asked that they be given more time to complete what is necessary. Mr. Donovan further stated that the community needs modernization and asked the Commission to do the right thing.

Kim Schaeffer, 1800 S. Pacific Coast Highway, Unit 98, stated that she appreciated the project going forth and that the development was welcome and she was in favor, but at the appropriate scale.

(Commissioner Biro stepped out of the meeting at 8:52 p.m.)

Bertin Guillory, 408 Ave G #32, opposed the project and stated the project was huge and that it called for tearing up the median on PCH, tearing grass and trees, shortening sidewalks, and changing parking on PCH. Mr. Guillory added that the (developer) wanted the City reconfigured for their project, and asked that the Commission deny the project.

Jane Abrams, 416 Avenue G, Unit 1, opposed the project and stated that the project was too big and bulky, and that (the community) wanted lower density residential on the site. She expressed concern with the proposed parking structure, and asked the Commission to deny the project.

Jeff Abrams, 416 Avenue G, #1, opposed the project and stated that in a year and a half no one had spoken in favor of the project before tonight's meeting. He stated he was in favor of sensible development which would accent and enhance the City and the Legado project was not one of those developments. Mr. Abrams added that continuance of the hearing would be counterproductive and that he agreed with staff's findings and recommendation that the Planning Commission deny the project.

Dwight Garbe, 220 Camino de las Colinas, opposed the project and stated that traffic and congestion had increased to overcrowded conditions. He stated that the property needs to be developed correctly and added that the project was too big. Mr. Garbe requested that the Planning Commission deny the project in its present form and consider allowing the developer to come back with something more acceptable for the community.

Motion by Commissioner Goodman, seconded by Commissioner Biro, to receive and file information submitted by a participant. Motion passed unanimously.

(Commissioner Biro returned to his seat at 8:55 p.m.)

Jeralyn Kirby, 143 Camino de las Colinas, opposed the project and expressed concern with increase in traffic congestion and the inability to find parking in the area. Ms. Kirby stated that (they) did not want to lose the “village” atmosphere nor the beautiful trees. Ms. Kirby commented that without sidewalks, people walk on the streets and that they would be endangered by the speeding and congested traffic. Ms. Kirby asked the Commission to consider the community and citizens’ welfare and requested that the project be denied.

An “active realtor” (name not given) came forward and spoke in favor of the project. He stated that it was important to have residential units for young people and that it was a good project for the community.

Tim Smith, 1700 S. Pacific Coast Highway, Vice President of Operations of Broughton Hotels (hotel company that manages the Palos Verdes Inn), stated that he was in favor of the hotel renovation which would bring premiere clientele and would benefit local business owners.

Bruce Caukin, 163 Via La Circula, spoke against the project and stated that he would like the density reduced from 45 to 35 people per acre. Mr. Caukin expressed concern with not seeing Caltrans representatives and asked how residents and cars would be moved safely. He stated that the direction the City was going might be counterproductive to the residential area in the Village.

Robert Rycroft, Broughton Hotels, 2400 E. Katella Ave., #300, Anaheim, spoke in favor of the project and stated he has worked with Mr. Szucker for 10 years. Mr. Rycroft requested approval of the project.

Carol Perry, 413 Avenue G, Unit 4, opposed the project and requested that the project density be substantially reduced. She stated she supported the resolution denying the project.

Joe Oliveri, 1401 S. Pacific Coast Highway, stated that he has attended meetings and that at the last Planning Commission meeting, the Commission decided that Legado’s presentation was not what the Commission wanted; and tonight the Commission said the presentation was incomplete and that the project should be denied. Mr. Oliveri stated that the City has exposure to litigation if the wrong decision is made and asked that the Commission consider starting over.

Rolf Strutzenberg, 431 Avenue F, spoke against the project, He commended City staff on their work and condemned the threatening letter from the (Allen Matkins Leck Gamble Mallory & Natsis LLP, Attorneys at Law) law firm. He stated that the project presented was massive and that missing from the package was the south view from Palos Verdes Boulevard. Mr. Strutzenberg asked the Commission to deny the 149-unit project.

Albert Ramos, 1600 Vine St., Apt. 706, Los Angeles, stated he agreed with the project and with additional restaurants and hotel renovation, and requested that the Commission approve the project.

Mary Trainor, 215 Paseo de Granada, stated she appreciated the developer's effort; however, the project was too massive and asked that the Commission deny the project.

Rhonda Cress, 412 Avenue G, #10, stated that she was representing 41 homeowners and spoke in opposition of the project. She stated that she had been to all meetings regarding the project and that people who had spoken in favor of the project at tonight's meeting were employees or worked for companies associated with Legado. Ms. Cress asked for denial of the project.

Patricia Williams, 412 Avenue G, #11, thanked everyone for their work and efforts. She stated that the monstrous development would affect her quality of life and cause horrific traffic and safety issues at a dangerous intersection. Ms. Williams stated that although she applauded Legado's efforts, she urged the Planning Commission to deny the project.

Michael Dube, 259 Paseo de Granada, spoke in opposition of the project and stated that there was no substantial reduction in size of the project, and asked for denial of the project. He requested that if an extension was given, the Commission mandate that the project be scaled to the actual buildable land.

Susan Renick, Redondo Beach, opposed the project and talked about apartment residential rates and Legado's rental income on the proposed project. Ms. Renick discussed the impacts from the project including pollution, lack of water and open space, noise, crime, and safety. She commented on the outreach made by Legado to NRBBA and Chamber of Commerce. Ms. Renick stated that as proposed, the project was too big, and asked for denial of the project.

Nils Nehrenheim (member of Save the Riviera) stated Legado was playing by its rules, they missed deadlines, and there was lack of clarity. Mr. Nehrenheim urged the Planning Commission to deny the project.

Joyce Neu, Redondo Beach, thanked the Commissioners for their dedication and recognized Mr. Czucker and his team for their outreach to the community. She questioned why Legado would give a presentation on a project that was not on the agenda. Ms. Neu stated that she believed that most speakers were referring to a revised project not being considered tonight. Ms. Neu read an article by SCAG regarding densification policies and closed by urging the Commission to deny the project.

Konni Tanaka, Redondo Beach, supported the project and stated that her research showed that Legado was a very responsible company and that it was open and responsive to the community. Ms. Tanaka stated that Legado would be a benefit to the Riviera and that she was in favor of the project.

Suzanne McCune, S. Gertruda Ave., opposed the project and stated she would like to see the Bristol Farms building preserved. She stated that the project was too big and the

hotel would have no ocean view. Ms. McCune stated that 149 to 146 units was not a (significant) reduction, and stated the community did not want to see beautiful buildings demolished. She asked for preservation and renovation of Bristol Farms and that the hotel be opened. Ms. McCune asked that the project be denied.

David Keeling, Avenue G, stated he was agnostic on the project but that something needed to be done. He added that he had not heard of viable alternatives to the project and he didn't think the apartments would be for average young people.

Peter Verenkoff, 533 Via La Selva, 40-year resident of Redondo Beach, stated he agreed with staff's findings and was in favor of denying the project. Mr. Verenkoff spoke of residential density and stated that net acreage was an issue for this project. Mr. Verenkoff asked that the 149-unit project be denied.

Don Moore, 416 Avenue G, #5, stated that he appreciated everyone's efforts and added that the community was not anti-development, but wanted a good development on the property. He stated that he opposed the project density and the additional traffic generated by the size of the project. Mr. Moore added that this was a massive development and more cooperation from the community would be gained if the density was reduced. Mr. Moore requested denial of the project.

Sheila Garcia, 530 Avenue G, commended the Commission for doing its due diligence, and stated that she didn't appreciate Legado's approach in their presentation to the Commission. She stated that citizens were not against development but wanted logical development. Ms. Garcia added that many of the people who spoke in favor the project were not City residents.

Pam Combar, 22502 Gaycrest Avenue, Torrance, thanked the Commissioners for their hard work and for sending her notice of the meeting. She spoke on unaffordability of rental units and the project's elimination of open space at Palos Verdes Boulevard and PCH.

Bruce Szeles commented that based on staff's report (we) were back to where we were in August. He stated that maybe it was time to deny the project and come back with more input from the community and a more acceptable (project) for the developer and residents.

Julie Moore, 416 Avenue G, Suite 5, thanked the Commission and Director of Community Development Aaron Jones for their work. She stated that (they) were not opposed to apartments at the project location and would like to see a thriving development, but the proposed project was too massive as proposed and asked the Commission to deny the project.

Robin Crevelt, 414 Avenue G, expressed concern with the additional cars generated by the project at the intersection of Palos Verdes and PCH and the project's impacts to traffic, pedestrians, and children. She stated that the project was too dense and asked that the Commission consider the safety, health and welfare of the residents and reject the 149-unit project.

Walt Howells, 619 N. Irena Avenue, District 2, commended Legado for changing the project design to Mediterranean, however urged the Commission to deny the proposed project and have Legado reduce the project density.

Andy Shelby, 1800 S. Pacific Coast Highway, stated that the 149-unit project was still too big and dense. He expressed concern with traffic and safety and commented on the hotel use. Mr. Shelby asked the Commission to deny the project.

Veronica Davidson, 40-year resident of the Riviera, stated she appreciated what the Legado team presented, but the project was too big, too high, and too dense for that corner. She stated that 110 units felt "doable" and that she opposed mixed use. Ms. Davidson stated that Legado was missing an opportunity to reach the community and suggested that they use signage to connect with the community. Ms. Davidson said that they wanted to keep the character and integrity of the Village.

Anita Shelby, Redondo Beach, expressed her concern for misrepresentation from Legado. She stated that staff had made substantial findings to support denial of the project, which was too dense, bulky, and massive. Ms. Shelby further stated that Legado had offered no description of the hotel redesign and added that she was willing to continue discussions with Legado to make the project workable for all stakeholders. Ms. Shelby urged the Commission to deny the 149-unit project.

Marci Guillermo, District 1, Redondo Beach, expressed concern with traffic in the City and that she would like to see a report from Caltrans. She stated that the removal of medians, trees, and parking spaces was unacceptable. She added that the hotel was a major factor for the City and urged the Commission to deny the 149-unit project.

Egor Nastaska, Hollywood Riviera, opposed the project and stated that this intersection was the most important intersection in the area, and asked that the Commission make the right decision. He stated that he appreciated the project's change of style from Modern to Mediterranean, but that density was too high and property values in the area would be affected. Mr. Nastaska added that the email he received was for 146 units, but the project was for 149 units.

In response to Mr. Fernando Villa, Chairperson Rodriguez stated that the Commission would be deciding on the 149-unit application or continuance of the item. Mr. Rodriguez stated that Legado could complete the 146-unit application.

Mr. Fernanda Villa stated that they have the same application but with new version, but if the City considered the 146-unit project as a new application, they would have to withdraw and ask for a vote on the 149-unit project.

Motion by Commissioner Goodman, seconded by Commissioner Biro, to close the Public Hearing. Motion carried unanimously.

Motion by Commissioner Goodman, to **DENY Case No. 2015-03-PC-005 as presented and APPROVE Resolution No. 2015-11-PCR-021** denying revised applications for Conditional Use Permit, Planning Commission Design Review, Vesting Tentative Tract Map No. 72662, Environmental Review (Mitigated Negative Declaration) and Sign Review for the development of One Hundred Forty Nine (149) residential apartment

units and approximately 37,000 square feet of commercial use located in a mixed-use project within a Mixed Use (MU-3A) Zone at 1700 S. Pacific Coast Highway.

In response to Commissioner Ung, Director of Community Development Jones stated that the application for 146 units was incomplete and that the only completed application was for 149 units. Mr. Jones further stated that it did not have a Tract Map for a 146-unit project.

Commissioner Goodman stated that the applicant was specific about not wanting to continue to complete the 146-unit application. He added that there was no alternative but to approve or deny the 149-unit project and that his motion was to deny the project.

In response to Commissioner Biro, Assistant City Attorney Cheryl Park stated that if the Planning Commission denies the 149-unit project, the applicant could appeal the decision to the City Council, and depending on the Council's decision, anyone could appeal that decision and would go to a court case.

Commissioner Biro expressed disappointment that the applicant wanted a denial of the project so they can appeal.

In response to Commissioner Gaian, Assistant City Attorney Cheryl Park stated that if the Planning Commission denies the 149-unit project and if there was an appeal, it would go before the City Council; and that if applicant would submit for 146 units it could theoretically go before Planning Commission.

In response to Chairperson Rodriguez, Assistant City Attorney Cheryl Park stated if the Commission denies the request, the applicant can come back with new plan for 180 units.

Re-motion by Commissioner Goodman, seconded by Commissioner Gaian, to **DENY Case No. 2015-03-PC-005 as presented and APPROVE Resolution No. 2015-11-PCR-021 denying** revised applications for Conditional Use Permit, Planning Commission Design Review, Vesting Tentative Tract Map No. 72662, Environmental Review (Mitigated Negative Declaration) and Sign Review for the development of One Hundred Forty Nine (149) residential apartment units and approximately 37,000 square feet of commercial use located in a mixed-use project within a Mixed Use (MU-3A) Zone at 1700 S. Pacific Coast Highway. Motion carried unanimously.

Commissioner Goodman stated that he was disappointed with Legado's desire to put the Commission in this position and that he thought they would have moved forward with the application as presented.

**RECESS: 10:23 PM**

The Commission recessed at 10:23 p.m.

**RECONVENE: 10:29 PM**

## **ROLL CALL**

MINUTES  
PLANNING COMMISSION  
NOVEMBER 19, 2015  
PAGE 15

Commissioners Present: Biro, Gaian, Goodman, Ung, Rodriguez  
Commissioners Absent: Mitchell, Sanchez  
Officials Present: Aaron Jones, Community Development Director  
Sean Scully, Planning Manager  
Cheryl Park, Assistant City Attorney  
Genny Ochoa, Recording Secretary

### **NEW BUSINESS**

None.

### **PUBLIC PARTICIPATION ON NON-AGENDA ITEMS**

Marci Guillermo, Redondo Beach, requested that the City require homeowners to maintain their properties.

Sheila Garcia stated that the homeowners were responsible for the City's beautification and that the City should inspect properties and enforce property maintenance to require upgrades to dilapidated properties. Ms. Garcia added that renters were hesitant to report maintenance issues for fear of eviction or rent increases.

Community Development Director Jones responded to the comments and stated that property maintenance was a code enforcement issue which could be included in the General Plan update discussions and added that increasing code enforcement resources would be discussed in the next budget cycle. Mr. Jones stated that code enforcement concerns could be addressed to his office.

### **COMMISSION ITEMS AND REFERRALS TO STAFF**

In response to Commissioner Biro, Community Development Director Jones stated that further discussion on the Legado application process would be appropriate at another open session.

In response to Commissioner Gaian, Community Development Director Jones stated that a summary of the City's growth and development would be presented at the next Planning Commission meeting in December 2015.

In response to Chairperson Rodriguez, Community Development Director Jones briefly explained development standards and stated that whatever is developed on a site counts towards overall maximum floor area ratio and added that allowable development would include the land area on which the units are constructed. Mr. Jones stated that further conversation could be held on the matter.

In response to Commissioner Goodman, Community Development Director Jones stated that development discussion to include the Legado project could be held until after the appeal period.

### **ITEMS FROM STAFF**

Community Development Director Jones announced that the first of three public workshops regarding the Waterfront DEIR would be held on Saturday, November 21, 9 a.m. to 1 p.m. at the Crown Plaza Hotel. Subsequent meetings will be on Wednesday, December 9, 2015, 6 p.m. to 9 p.m., at the City's Main Library, and on Saturday, January 9, 2016, 9 a.m. to 1 p.m., at the Crown Plaza Hotel.

**COUNCIL ACTION ON PLANNING COMMISSION MATTERS**

Community Development Director Jones announced that at the regular City Council meeting of November 17, the City Council adopted the massage and tattoo ordinances which would become effective 30 days from the ordinance adoption date. Mr. Jones reported that the City Council approved the Strategic Plan, the next six-month work program, and added that staff would present to the Commission discussion on mixed use amendments in January 2016, and present to City Council at the second meeting in February.

In response to Commissioner Gaian, Community Development Director Jones stated it would be six to seven months before public hearings are held on the Waterfront EIR.

**ADJOURNMENT: 10:40 PM**

There being no further business to come before the Commission, a motion was made by Commissioner Ung, seconded by Commissioner Goodman, to adjourn at 10:40 p.m. to a regular meeting to be held at 7:00 p.m. on Thursday, December 17, 2015 in the Redondo Beach City Council Chambers, 415 Diamond Street, Redondo Beach, California. Motion carried unanimously.

Respectfully submitted,

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Aaron Jones  
Community Development Director



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# Memorandum

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**Action Date: November 17, 2015**

**To: CITY COUNCIL**

**From: STEVE ASPEL, MAYOR**

**Subject: ADOPTION OF STRATEGIC PLAN**

## **RECOMMENDATION**

1. Adopt the 2013-2016 City of Redondo Beach Strategic Plan six-month objectives established at the October 14, 2015 Strategic Planning Workshop
2. Set March 29, 2016 for the next Strategic Planning Workshop.

## **EXECUTIVE SUMMARY**

At its October 14, 2015 workshop, the City Council reviewed the city's Strategic Plan goals for 2013-2016 (not in priority order):

- Vitalize the Waterfront, Artesia Corridor, Riviera Village and North Redondo Industrial Complex
- Improve public infrastructure and facilities in an environmentally responsible manner
- Increase organizational effectiveness and efficiency
- Build an economically vital and financially sustainable city
- Maintain a high level of public safety with public engagement
- Review and identify a process for updating the City's General Plan

The City Council established six-month objectives attached to this report and scheduled the next workshop for March 29, 2016.

## **BACKGROUND**

The Strategic Planning workshop was facilitated by Marilyn Snider and Associates, and attended by the Mayor, City Council Members, executive staff, and members of the public. It featured City Council review of the 2013-2016 Strategic Plan goals; a listing of strengths and weaknesses; and development of the new six-month objectives for each of the Strategic Plan goals. The process involved less small group interaction and more interaction by the City Council and attendees as a whole.

Councilmembers will recall that certain items were described in general terms with the understanding that the City Manager and staff would follow up and include the needed detail and milestones for the new objectives. The attached document incorporates the

staff input as coordinated by the City Manager. The City Council should review the work plan and the listed milestones which are recommended for inclusion in the Plan. Additionally, staff has reviewed the target dates for other objectives and made suggested modifications to help ensure that they can be realistically achieved in the stated time frames.

Should the City Council adopt the updated Strategic Plan, the City Manager will provide monthly progress reports to the City Council on the six-month objectives and staff liaisons will provide similar reports to City Commissions as part of their ongoing work. A date was set for the next Strategic Planning workshop (March 29, 2016) at which time the City Council will establish new three year goals covering the 2016-2019 time period.

### **COORDINATION**

Each department responsible for specific objectives within the Strategic Plan has reviewed the document and provided support for this recommendation.

### **FISCAL IMPACT**

Funds for activities related to Strategic Planning are included in the Mayor and City Council FY 2015-16 Budget.

Attachment:

- 2013-2016 Strategic Plan and Six Month Objectives, October 14, 2015

**CITY OF REDONDO BEACH**        **SIX-MONTH STRATEGIC OBJECTIVES**  
**October 14, 2015 – March 15, 2016**

ACM=Assistant City Mgr    CD=Community Development    PW=Public Works    WED=Waterfront and Economic Development    CS=Community Services

<i>THREE-YEAR GOAL: VITALIZE THE WATERFRONT, ARTESIA CORRIDOR, RIVIERA VILLAGE AND NORTH REDONDO INDUSTRIAL COMPLEX</i>						
WHEN	WHO	WHAT	STATUS			COMMENTS
			DONE	ON TARGET	REVISED	
1. By March 15, 2016	ACM and WED Director	Conduct Public Outreach meetings (Feb – March 2016) regarding alternative locations for installation of a new boat ramp including a meeting with Harbor Commission and present the results to the City Council.				
2. At the March 1, 2016 City Council meeting	WED and PW Director, working with regional agencies	Report on the status of the analysis of sea level rise and its potential impact on the Redondo Beach waterfront.				
3. At the February 16, 2016 City Council Meeting	PW Director	Present to the City Council for review the cost of fully implementing the Riviera Village sidewalk landscaping improvement plan along Catalina Avenue from Palos Verdes Boulevard to Avenue I.				
4. At the March 1, 2016 City Council Meeting	CD Director working with WED Director	Present to the City Council for consideration options for further modification of parking requirements Citywide to help encourage economic development.				
5. At the November 17, 2015 Council Meeting	City Manager	Agendize a report on the appointment of a Mayor/City Council Subcommittee to work with staff on issues that may arise during the time that AES is marketing the AES site for non-industrial uses.				
5.a. FUTURE OBJECTIVE between April 4, 2016 and June 15, 2016	City Manager with the CD Director and City Attorney	City and AES representatives to meet and confer as necessary and discuss implementation of the AES Task Force, its purpose, organization, and membership, and other details relevant to the formation of the AES Task Force prior to a City Council Report on July 5, 2016 for appointment of the Task Force				
5.b. FUTURE OBJECTIVE	City Manager with City Attorney and CD Director	City Council to select consulting services firms needed to support the Task Force following the RFP Process.				

5.c. FUTURE OBJECTIVE	Task Force, working with Consultants	Task Force/Consultants present findings and recommendations to the City Council.				
6. FUTURE OBJECTIVE	WED working with CD Director	Explore the feasibility and recommend to the City Council whether or not to create a Storefront Improvement Program in key business areas.				

**THREE-YEAR GOAL: *IMPROVE PUBLIC INFRASTRUCTURE AND FACILITIES IN AN ENVIRONMENTALLY RESPONSIBLE MANNER***

WHEN	WHO	WHAT	STATUS			COMMENTS
			DONE	ON TARGET	REVISED	
1. By the January 19, 2016 City Council meeting	WED Director working with PW Director	Present to the City Council for review the fiscal impact for financing the construction of a replacement pier parking structure and other Harbor Area public infrastructure.				
2. By the March 15, 2016 City Council meeting	PW Director	Present to the City Council a report on the status of the Tri-City Aviation Boulevard Bikeability Plan Grant.				
3. At the March 1, 2016 City Council meeting	ACM working with PW Director, Finance Director, Police Chief and Fire Chief	Report on the status of the Major Facilities Repair Fund and the City's long-term major facilities needs list.				
4. At the February 2, 2016 City Council meeting	PW Director	Present to the City Council for consideration a Conceptual Plan for improvement of Anderson Park restrooms and the demolition of the Park's vacant Annex Building.				
5. By March 15, 2016	CS Director working with PW, Finance and CD Directors	Review and report to the City Council the City's park and open space inventory and funding sources for acquisition and rehabilitation of parks and open space.				

**THREE-YEAR GOAL: *INCREASE ORGANIZATIONAL EFFECTIVENESS AND EFFICIENCY***

WHEN	WHO	WHAT	STATUS			COMMENTS
			DONE	ON TARGET	REVISED	
1. By March 15, 2016	City Attorney, working with the CD Director	Present to the City Council for direction options for the restructuring of the Redondo Beach Sister City Committee as a separate non-profit 501(c)(3) and/or an official city committee or commission.				
2. By March 15, 2016	IT Director working with City Manager, City Attorney and other Department Heads	Report the results to the City Council of a social media pilot project that explores additional methods of public outreach (social media e.g., Facebook, Twitter) through launch of the City's new webpage.				
3. At the January 5, 2016 City Council meeting	PW Director	Review the Administrative Policy and Procedure (APP) regarding purchase and replacement of zero emission vehicles and present the results to the City Council.				
4. By March 15, 2016	PW Director working with IT Director	Explore and recommend to the City Council for consideration the potential use of technology to better regulate pedestrian and bicycle traffic crossing where the beach bike path meets the south end of the pier for improved safety and traffic flow.				
5. Prior to March 1, 2016	City Manager, HR Director and all City Departments	Implement a Customer Service Training Program for applicable City employees on a city-wide basis.				

THREE-YEAR GOAL: *BUILD AN ECONOMICALLY VITAL AND FINANCIALLY SUSTAINABLE CITY*

WHEN	WHO	WHAT	STATUS			COMMENTS
			DONE	ON TARGET	REVISED	
1. By the January 19, 2016 City Council meeting	WED Director	Research and report to the City Council on the new State tax increment financing law to fund public infrastructure and other projects.				
2. By the February 1, 2016 City Council meeting	CD Director working with Finance Director	Present a report to the City Council on current regulation of short-term rental activity and obtain direction, if any, from the City Council.				
3. At the March 15 2016 City Council meeting	Finance Director working with HR Director and all involved Departments	As part of the Mid-Year Budget Review, recommend to the City Council for consideration a budget modification to be able to hire or contract with a Grant Specialist to identify and apply for grants and coordinate with departments to facilitate implementation.				

**THREE-YEAR GOAL: *MAINTAIN A HIGH LEVEL OF PUBLIC SAFETY WITH PUBLIC ENGAGEMENT***

WHEN	WHO	WHAT	STATUS			COMMENTS
			DONE	ON TARGET	REVISED	
1. By February 16, 2016	PW Director and Police Chief	Develop plans and specifications for security fencing around the police station.				
2. By March 15, 2016	Police Chief, working with the PW Director and CS Director	Research and present to the City Council for direction options for construction of a canine training facility on an existing unused city parcel.				
3. By January 1, 2016	HR Director working with Police and Fire Chiefs	Create hiring and promotional lists to fill all vacancies as they arise within the Fire and Police Departments.				
4. At the March 15, 2016 City Council meeting	City Manager working with Fire Chief, ACM and Finance Director	As part of the Mid-Year Budget Review, explore and make a recommendation to the City Council for consideration the possible restoration of two Fire Prevention Inspectors and one Fire Training Officer in the Fire Department.				
5. By March 15, 2016	Police Chief	Increase Neighborhood Watch participation by 30%, and improve assistance from homeowners associations.				

**THREE-YEAR GOAL: *REVIEW AND IDENTIFY A PROCESS FOR UPDATING THE CITY'S GENERAL PLAN***

WHEN	WHO	WHAT	STATUS			COMMENTS
			DONE	ON TARGET	REVISED	
1. At the February 2, 2016 City Council meeting	CD Director working with ACM and City Manager	Present to the City Council and community a General Plan 101 workshop(s) overview on the General Plan's current status and content..				
2. At the March 1, 2016 City Council meeting	CD Director working with ACM and City Manager	Present to the City Council a budget process and timeline for either a comprehensive or "living" (incremental) update of the General Plan.				
3. Consider in context of FY 2016-17 Budget Process	CD Director working with Finance Director, ACM and City Manager	Present to the City Council for action a budget appropriation to conduct a community assessment and engagement process for updating the General Plan.				



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# Administrative Report

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**Planning Commission Hearing Date:**

**December 17, 2015**

**AGENDA ITEM:** 8 (New Business)

**SUBJECT:** Update on Community Growth and Development

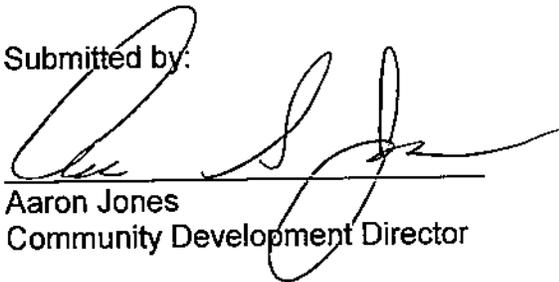
**RECOMMENDATION:**

The Community Development Department recommends that the Planning Commission receive and file this memorandum and presentation slides on Community Growth and Development.

**SUMMARY:**

Attached is the presentation on Community Growth and Development for the Planning Commission to receive and file.

Submitted by:



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Aaron Jones  
Community Development Director

# 2015 Community Growth and Development Report

CITY OF REDONDO BEACH



# General Plan Framework



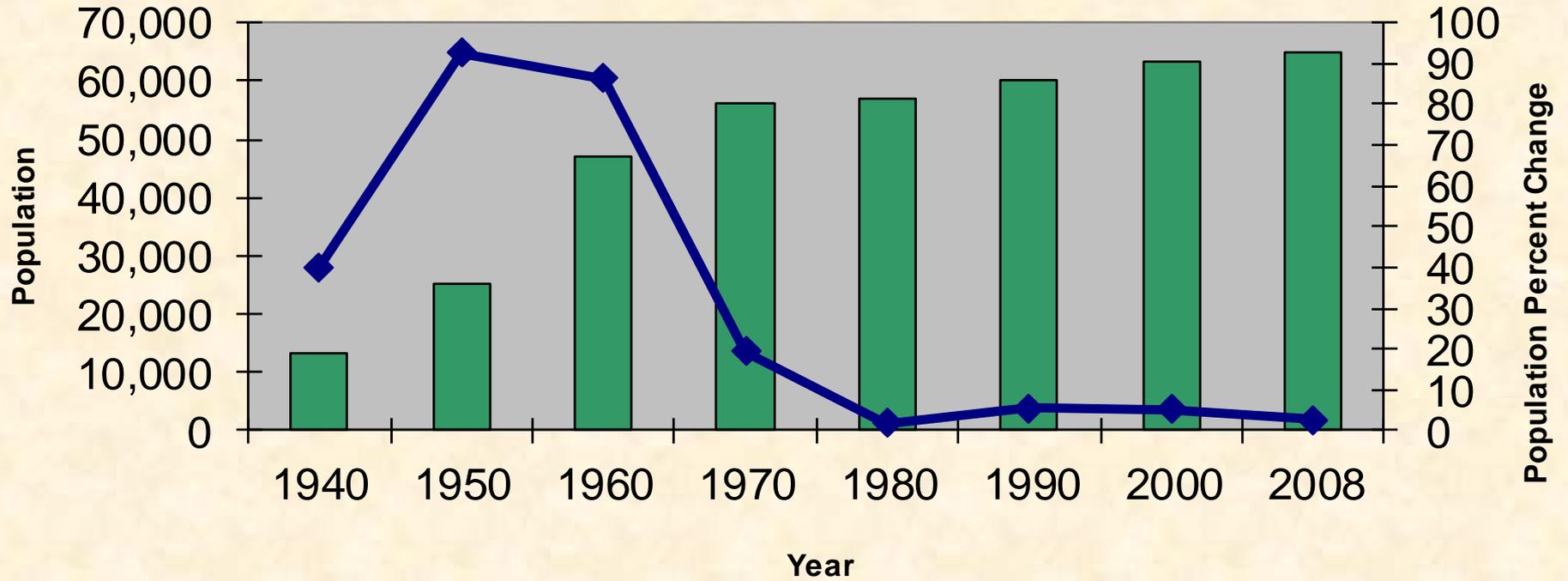
- ▶ 1992 General Plan considered 2010 development capacities.
- ▶ Residential development will not reach expected 2010 levels until 2025 or later.
- ▶ Commercial and Industrial Development will not approach these levels until after 2030.

# 2015 Community Growth Summary

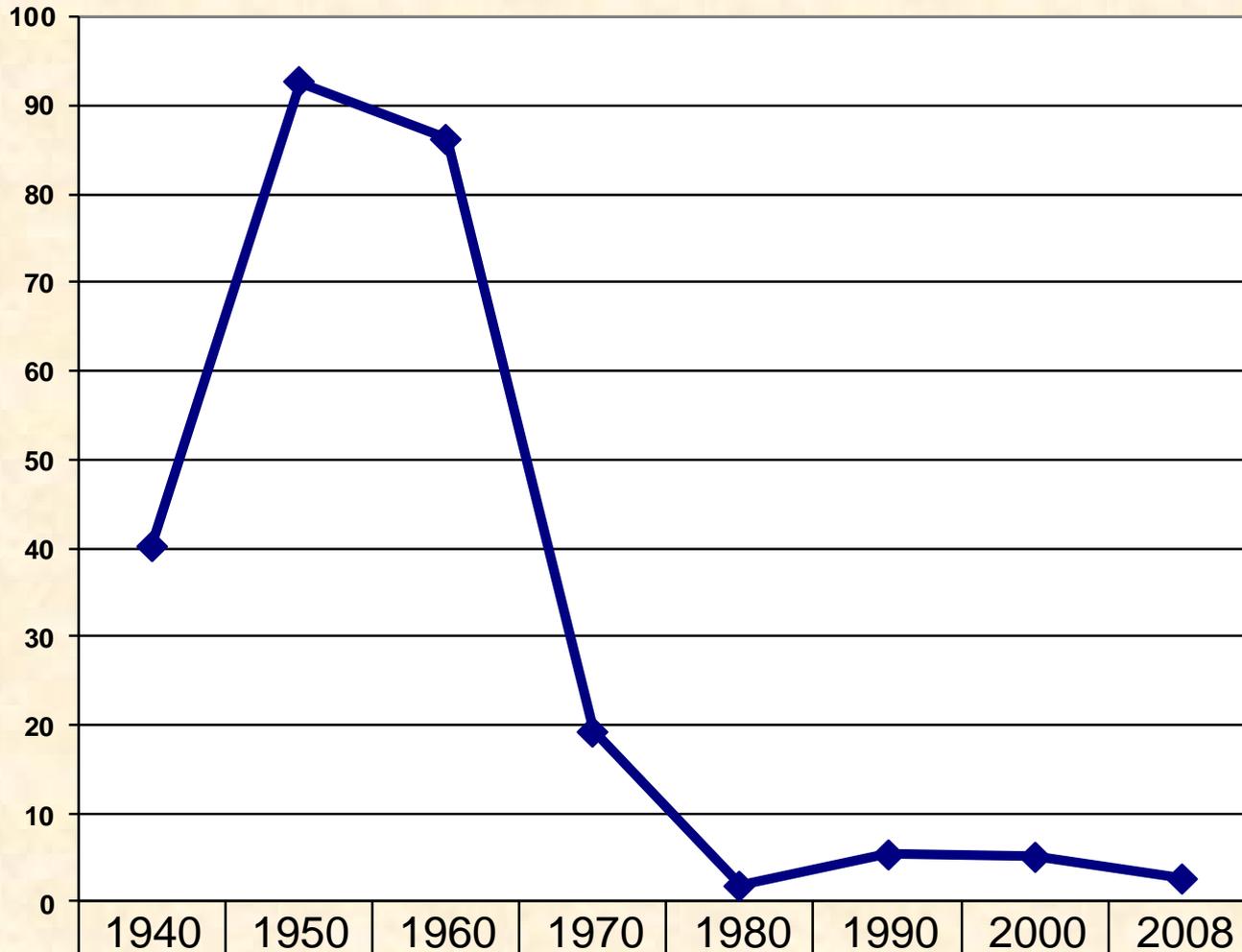
- ▶ 31 Single Family Permits
- ▶ 53 Multi-Family Permits
- ▶ Demolition
  - ▶ 59 Single Family
  - ▶ 1 Multi-Family
- ▶ 24 Net Housing Unit Gain
- ▶ 1,475 Sq. Ft. Commercial

# Population and Housing

# Redondo Beach Population Trends



# Redondo Beach Population Percent Change 1940-2008



	1940	1950	1960	1970	1980	1990	2000	2008
◆ Percent Change	40.1	92.7	86.3	19.3	1.8	5.4	5.1	2.7

Year

# Population Growth

Year	Population	% Change
1940	13,092	-----
1950	25,226	92.7%
1960	46,986	86.3%
1970	56,075	19.3%
1980	57,102	1.8%
1990	60,167	5.4%
2000	63,261	5.1%
2010	66,748	5.2%
2013	67,815	1.57%

Source: Bureau of the Census (2010), American Community Survey Estimate (2013).

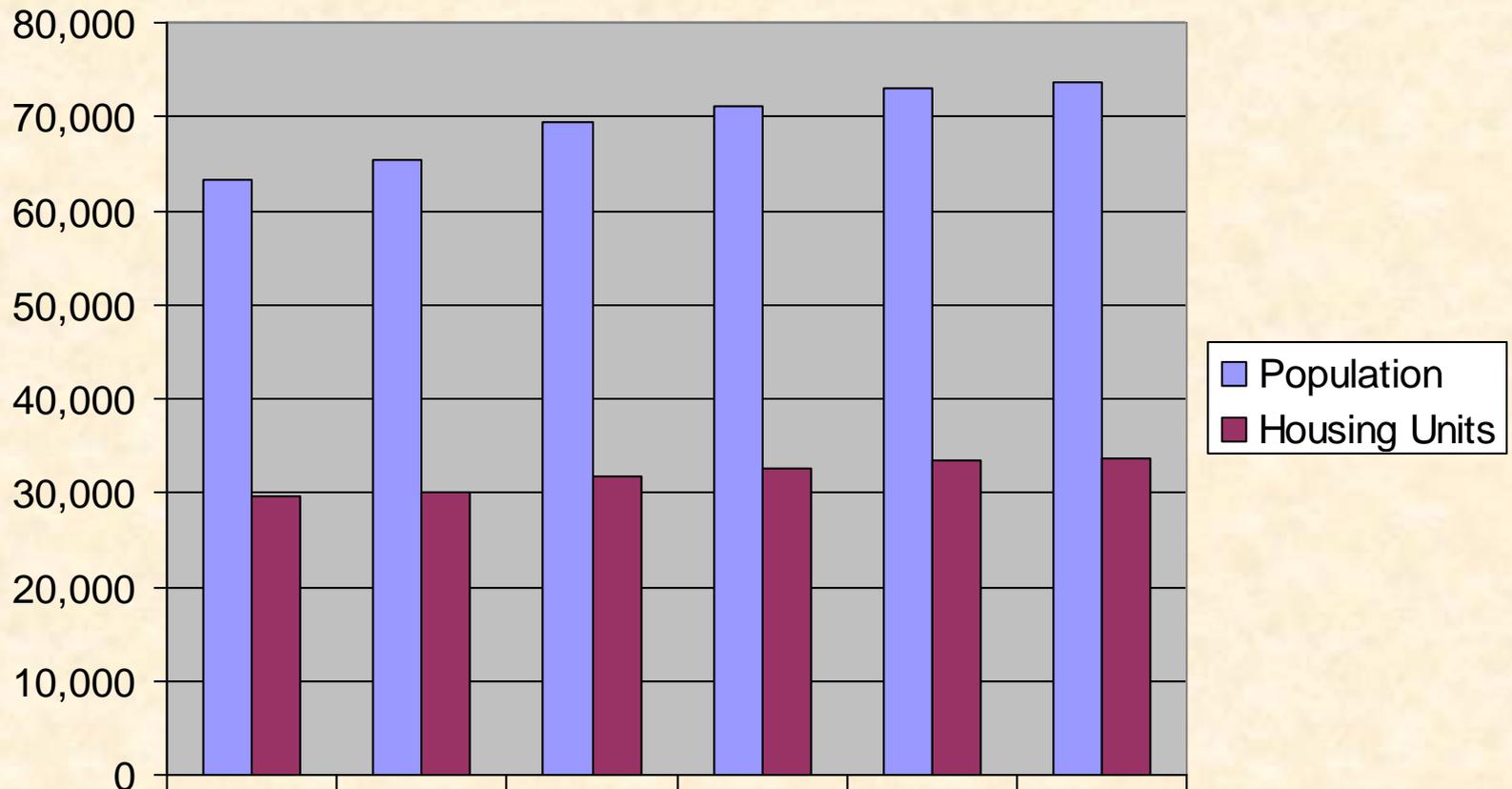
# Housing Unit Growth 2009-2015

Year	Demolitions	New Units	Net Gain (Units)	Existing Units	Total Units	Net Gain (%)
2009-2010	36	44	8	30,601	30,609	
2010-2011	37	41	4	30,609	30,613	0.013%
2011-2012	46	74	28	30,613	30,641	0.091%
2012-2013	44	51	7	30,641	30,648	0.023%
2013-2014	69	70	1	30,648	30,649	0.003%
2014-2015	60	84	24	30,649	30,673	0.078%
<b>Total</b>	<b>292</b>	<b>364</b>	<b>72</b>	<b>X</b>	<b>30,673</b>	<b>0.23%</b>
<b>Annual Average</b>	<b>48.6</b>	<b>60.6</b>	<b>12</b>			<b>0.039%</b>



What should  
we  
anticipate?

## Redondo Beach Housing and Population Trends 2000-2030



	2000 Census	2007	2014	2021	2028	2030
Population	63,261	65,374	69,381	71,212	73,043	73,566
Housing Units	29,543	29,988	31,826	32,666	33,506	33,746

Assumptions: 1) Avg. HH size 2.18 2) Unit increase 263 per year 2007-2014, 120 per year thereafter

# Residential Growth Estimates from 2007

Citizen's Growth Management and Traffic Committee

Infill Development:

▶ 2007-2014

- ▶ annual increase eight-tenths of one percent (263 units/year; 1,838 units total) **Actual change 18.71 units/year (131 units total)**

▶ 2014-2030

- ▶ increase at 10-year average rate of four-tenths of one percent (approx. 120 units per year; 1,920 units total)  
**This estimate is highly conservative. In the last 6 years only 72 net new units have been constructed (12 units per year). This is an growth rate of .039 percent**

# Residential 2030

- ▶ 3,758 net new housing units, and
- ▶ 8,192 resident population increase
  - ▶ Overall increase averages 0.5% per year; 12.5% above 2007
- ▶ Adopted General Plan
  - ▶ Anticipated 18% housing unit gain 1991-2010
  - ▶ Predicted 0.9% gain per year
  - ▶ Projected numbers not realized until 2025



# Commercial, Industrial and Institutional Development

# Commercial/Industrial/ Institutional Growth Estimate

The following estimate was used in comprehensively updating the General Plan Circulation Element

By 2030:

- ▶ 1 Million sq. ft. Commercial/industrial development outside Harbor/Pier area
- ▶ 400,000 sq. ft. within Harbor/Pier area
- ▶ 1.4 Million net new square feet

# Actual Commercial Industrial Growth

<b>Year</b>	<b>Demolition (Sq. Ft.)</b>	<b>New (Sq. Ft.)</b>	<b>Net Gain (Sq. Ft.)</b>
2009	1,242	9,450	8,208
2010	0	1,150	1,150
2011	76,110	127,604	51,494
2012	0	1,000	1,000
2013	41,000	76,000	35,000
2014	7,000	67,000	60,000
2015	6,784	8,239	1,475
<b>Total</b>	<b>125,836</b>	<b>290,443</b>	<b>164,607</b>
<b>Annual Average</b>	<b>17,976.5</b>	<b>41,491.8</b>	<b>23,515.3</b>

# Annual Commercial/Industrial Change Estimate for Growth Planning

- ▶ As presented to CGMT and used to plan traffic and circulation needs
- ▶ 50,000 sq. ft. per year (four-tenths of one percent) increase (excludes Harbor/Pier) **Actual annual change has been approximately 23,500 sq. ft.- less than one-half the estimate**
- ▶ 400,000 sq. ft. net new Harbor/Pier development (allocated over 20 years) **Current Waterfront plan proposes 304,000 sq. ft. for CEQA analysis**

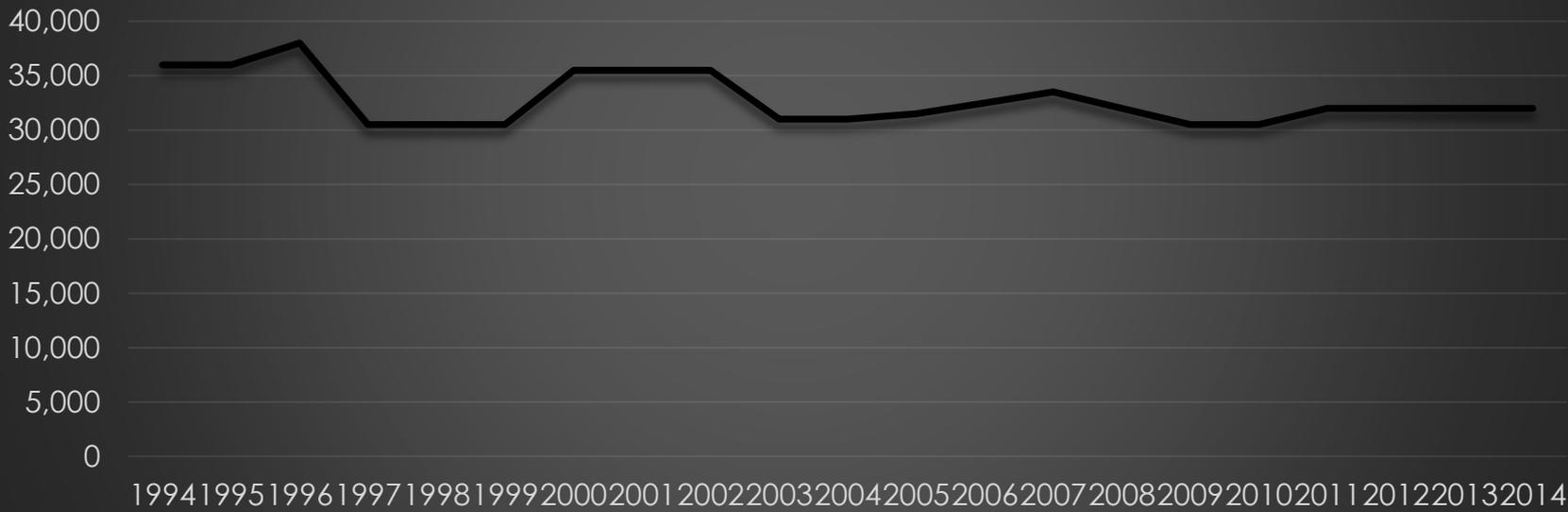


# What about Traffic?

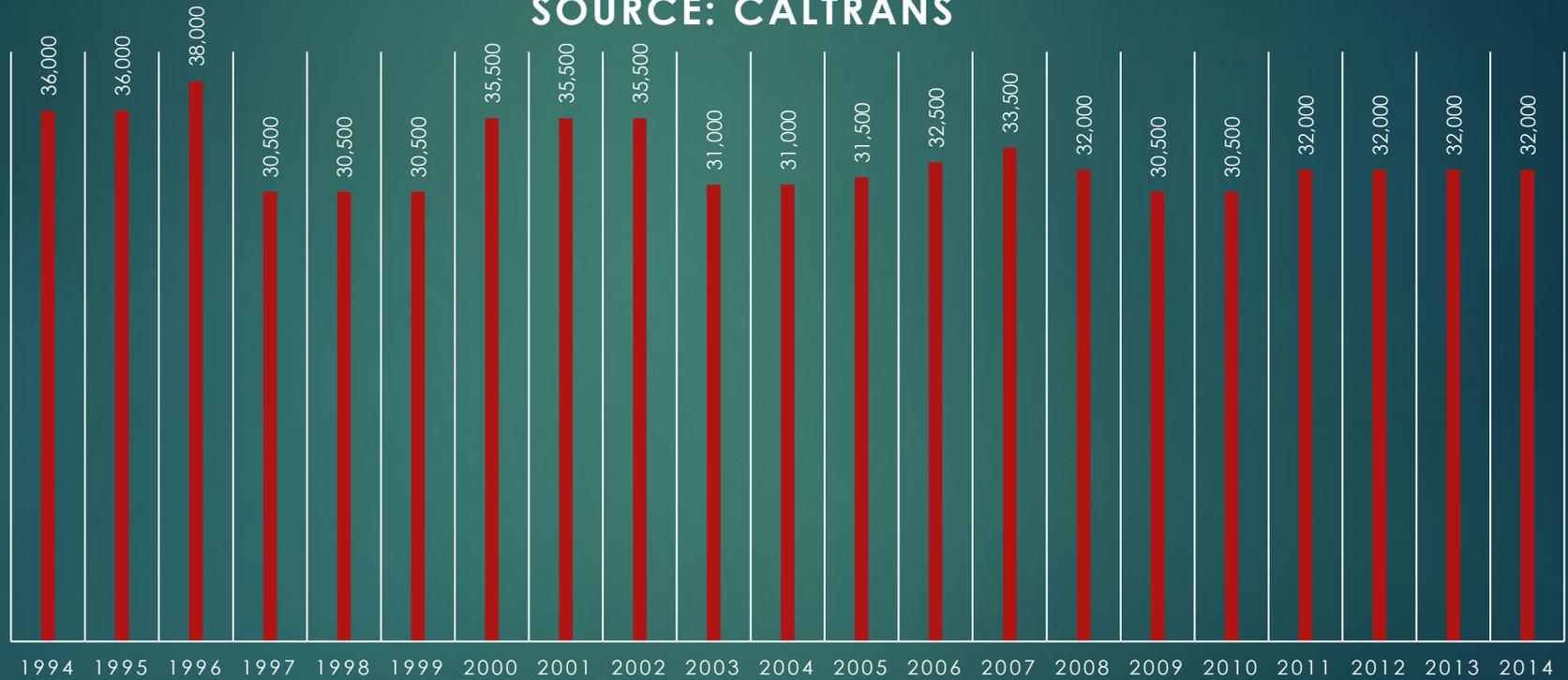
HOW HAS TRAFFIC CHANGED IN THE LAST 20 YEARS?



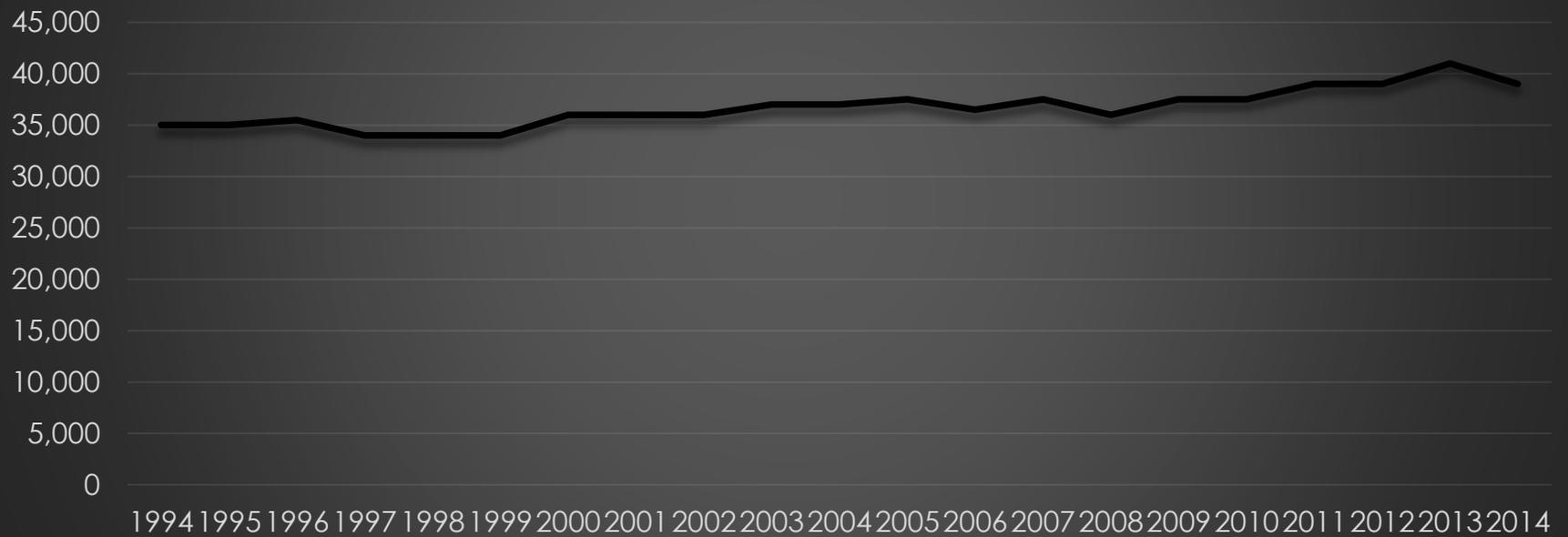
# PCH/PALOS VERDES TRAFFIC 1994-2014



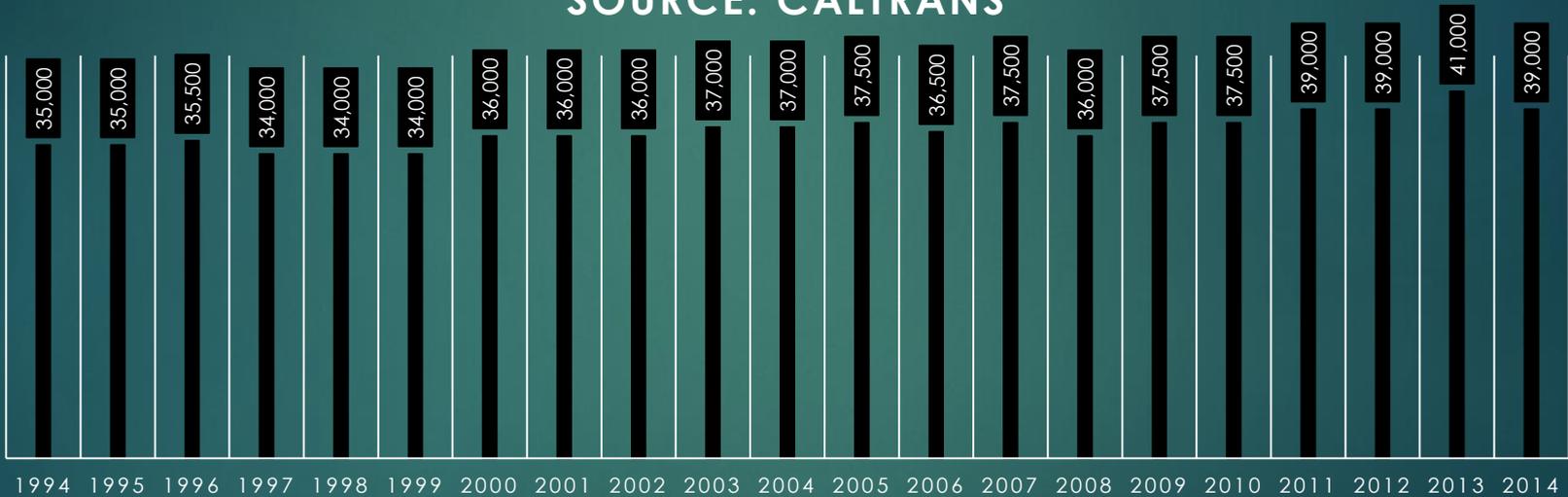
# PCH/PALOS VERDES TRAFFIC 1994-2014 SOURCE: CALTRANS



# PCH/ TORRANCE TRAFFIC 1994-2014



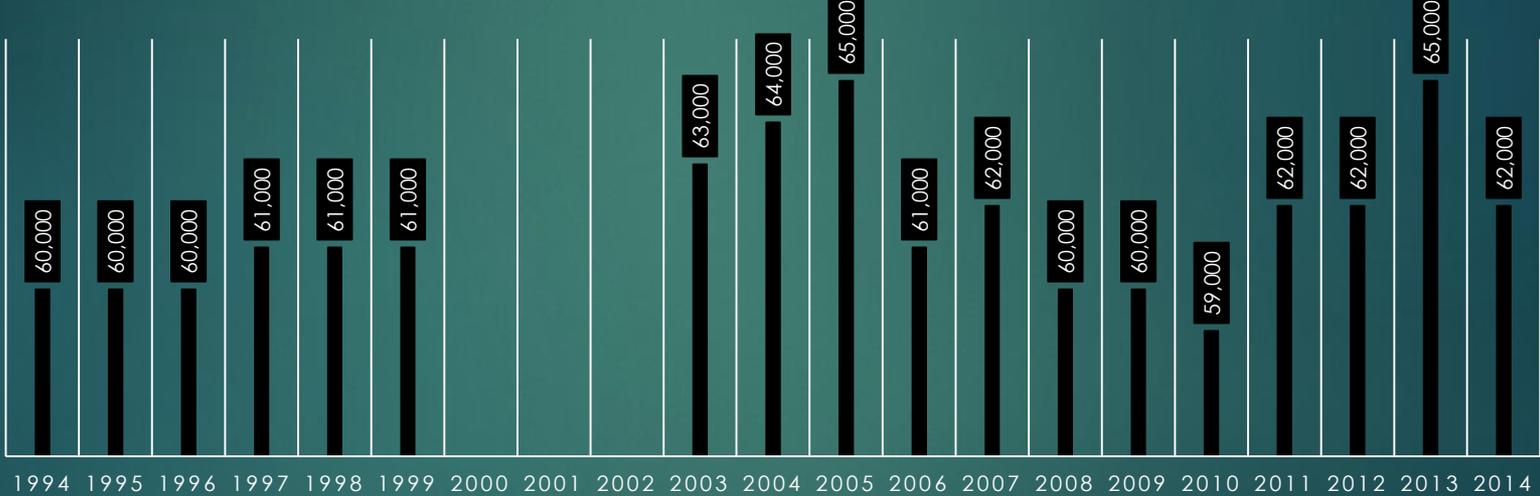
**PCH/TORRANCE BLVD.  
TRAFFIC 1994-2014  
SOURCE: CALTRANS**



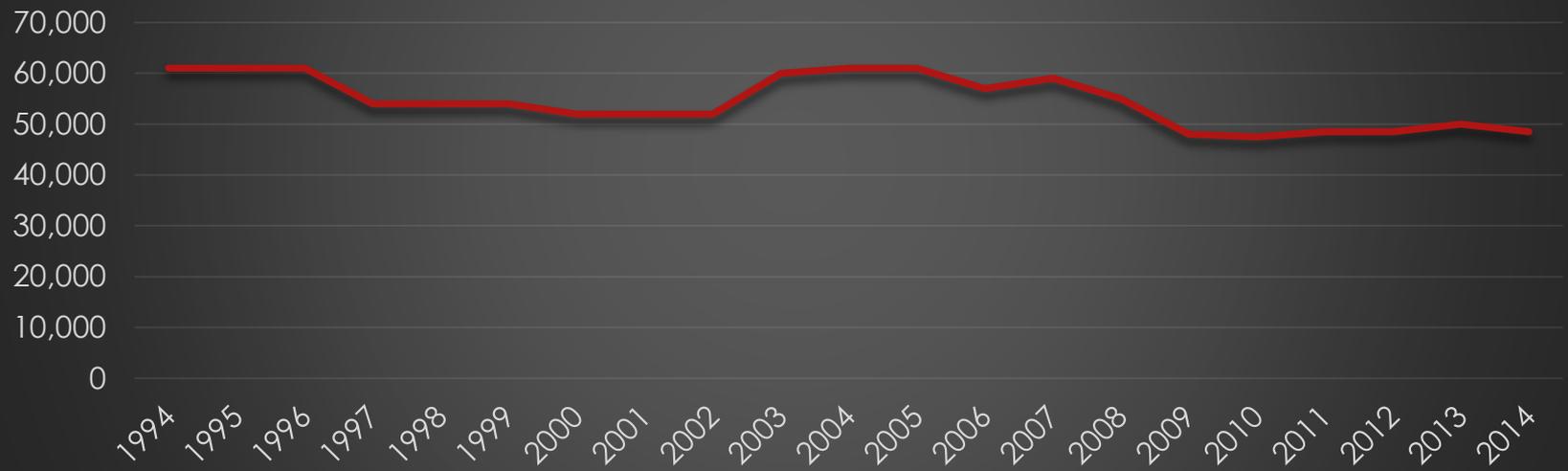
# PCH/ AVIATION TRAFFIC 1994-2014



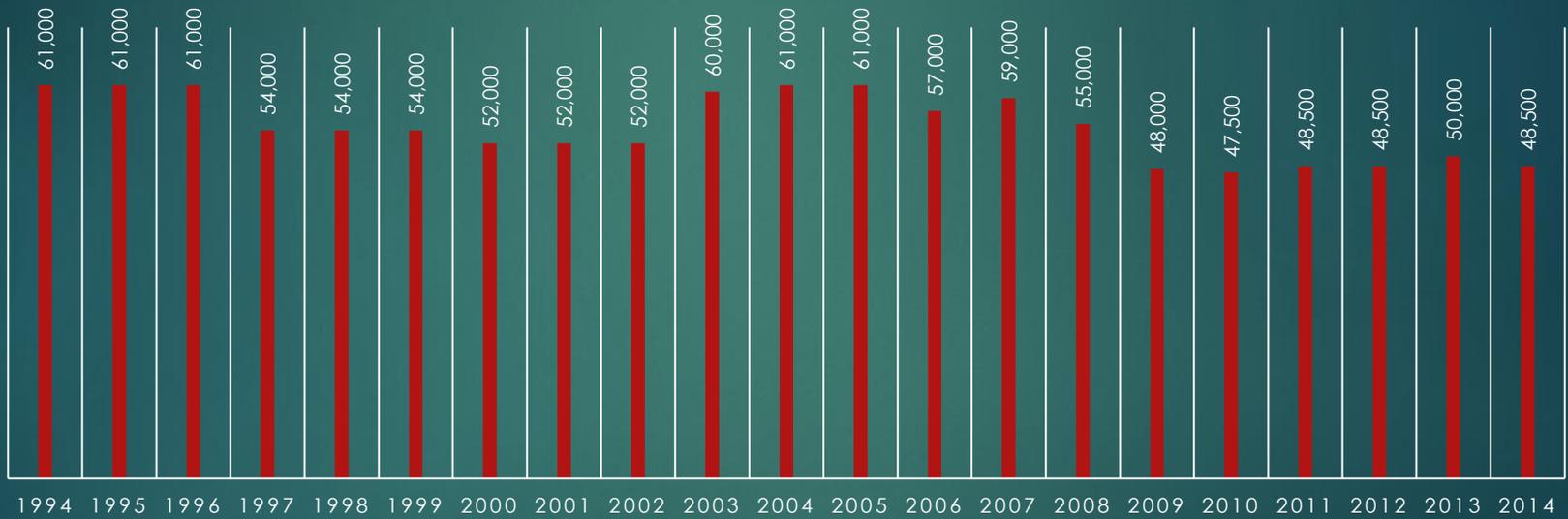
# PCH/ AVIATION TRAFFIC 1994-2014



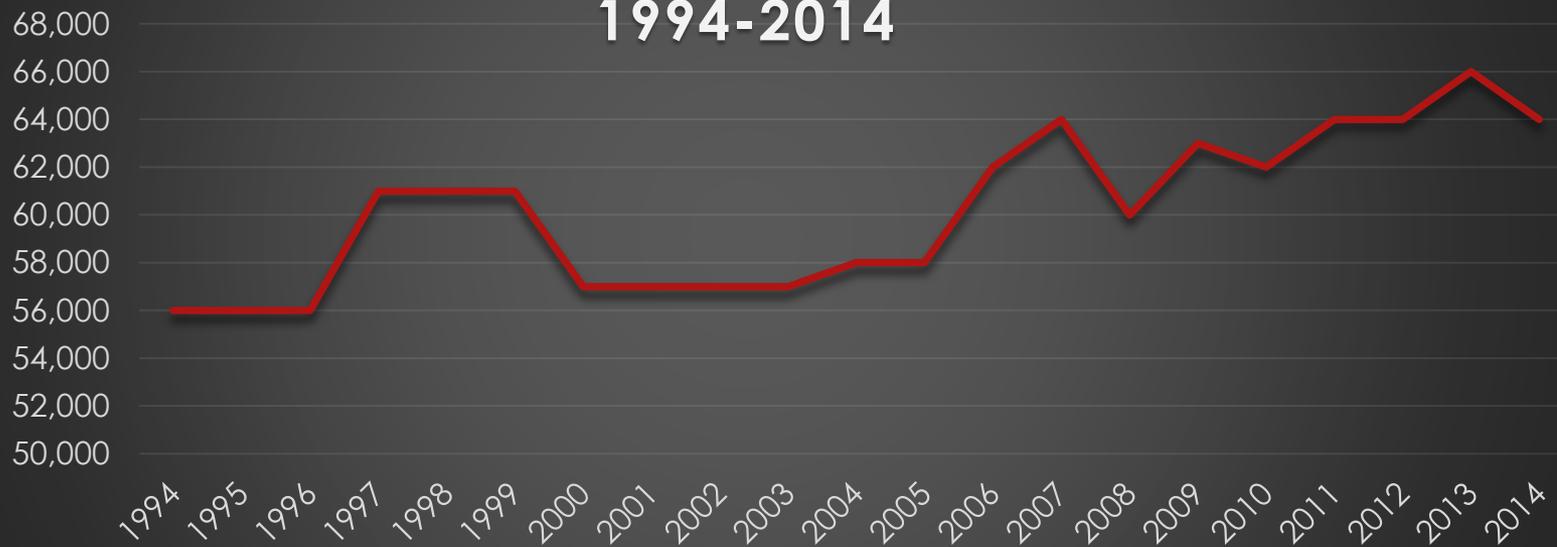
## PCH/ MBBL TRAFFIC 1994-2014



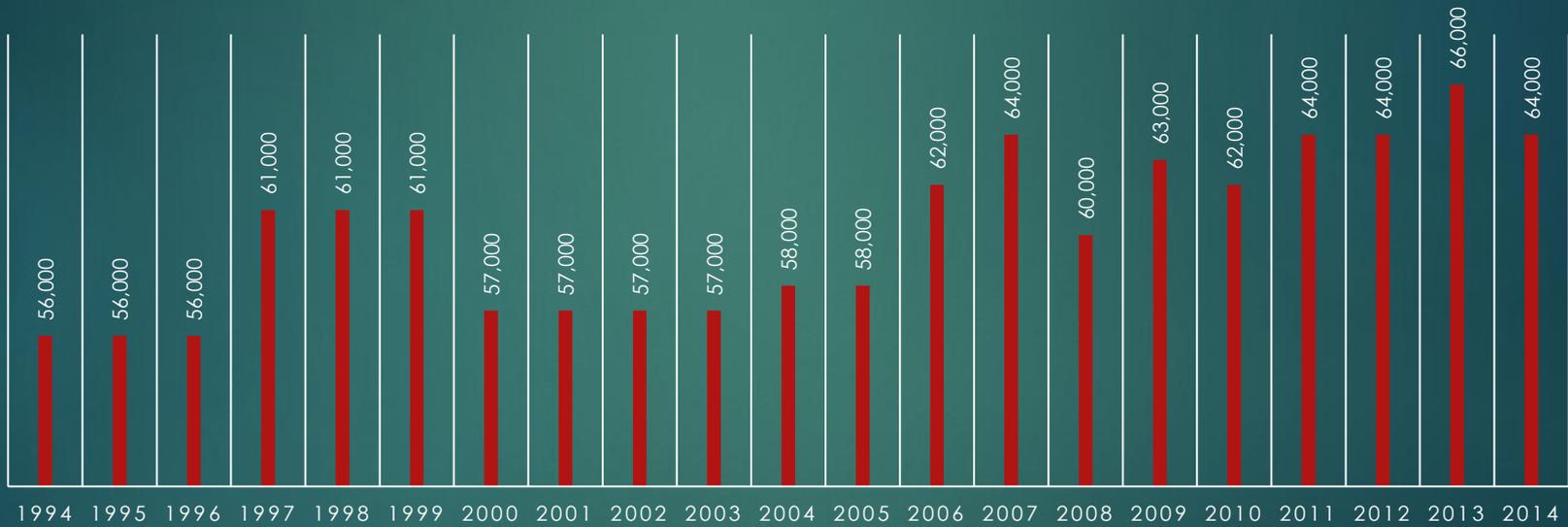
## PCH/ MBL TRAFFIC SOURCE: CALTRANS



## PCH/ROSECRANS TRAFFIC 1994-2014



## PCH/ ROSECRANS TRAFFIC SOURCE: CALTRANS





# Growth Impacts

RESIDENTIAL

COMMERCIAL

INDUSTRIAL

HARBOR



# Impact Factors and Measurements

	<b>Single Family</b>	<b>Multi-family</b>	<b>Difference</b>	<b>Percent</b>
<b>Daily Trips per Unit</b>	9.57	5.86	(3.71)	(38.7%)
<b>Peak Hr. Trips per Unit</b>	1.02	.52	(.5)	(49.0%)
<b>Water Use per Day per Unit (Gallons)</b>	247	174	(73)	(29.5%)
<b>Sewer Discharge per Day per Unit (Gallons)</b>	185	130	(55)	(29.7%)
<b>Student Generation per Unit*</b>	.471	.258	(.213)	(45.2%)

\*Source RBUSD Fee Study

# Commercial/ Industrial- Development Estimates and Traffic

- ▶ 1 Million Sq. Ft. Commercial/Industrial
  - ▶ 250 ksf Retail (ITE 820)
    - ▶ 10,735 ADT 938 Peak Hr (weekday).
  - ▶ 250 ksf Office (ITE 710)
    - ▶ ADT 2,775 388 Peak Hr.
  - ▶ 500 ksf Indust.(ITE 130)
    - ▶ 3,480 ADT 430 Peak Hr.

# Harbor Development Impact Factor vs Actual

- ▶ Planned 400,000 sq. ft, Considered All Retail (ITE 820)\*
- ▶ Result- 17,176 ADT 1,501 Peak Hr.
- ▶ Actual approximately 304,000 sq. ft
  - ▶ Restaurant 35%, , Retail 20%, Hotel 24%, Office 12%, Cinema 9%
- ▶ Result- 12,550 ADT 782 Peak Hr.

\*Trip Generation substantially overestimated. Development anticipated to include substantial hotel, office and public use components. e.g. Office ADT is only 25.8 percent of retail

# Cumulative Impact Estimates used for 2030 analysis

- ▶ Resident Population- increases 8,192 to 73,566 (six-tenths of one percent per year).
- ▶ Housing Units- increase 3,758 to approx. 33,746 (six-tenths of one percent per year).
- ▶ Industrial land uses- increase 500k (7.9 percent) to approx. 6.8 Million sq. ft. (four-tenths of one percent per year).
- ▶ Commercial land uses- increase 900k (14.5 percent) to approx. 7.1 Million sq. ft. (seven-tenths of one percent per year).

# Comparative and Cumulative Impacts

Use	Number	ADT	Peak Hour
Single Family	96	919	98
Multi-family	3,662	21,460	1,904
<b>Total Residential</b>	<b>3,758</b>	<b>22,379</b>	<b>2,002</b>
Retail	250,000 sq. ft.	10,735	938
Office	250,000 sq. ft.	2,775	388
Industrial	500,000 sq. ft.	3,480	430
Harbor	400,000 sq. ft.	17,176	1,501
<b>Total Com./Indust.</b>	<b>1.4 Million sq. ft.</b>	<b>34,166</b>	<b>3,257</b>

# Conclusions

- ▶ Community growth rates well under General Plan projections.
- ▶ City mature with moderating growth rates.
- ▶ Infrastructure exists, but requires improvement as necessary to support new development.
- ▶ Sustainable growth (infill and recycling) can be accommodated with adequate community investment and improvement.

# Recommendation

- ▶ Receive and file this presentation



# Profile of the City of Redondo Beach

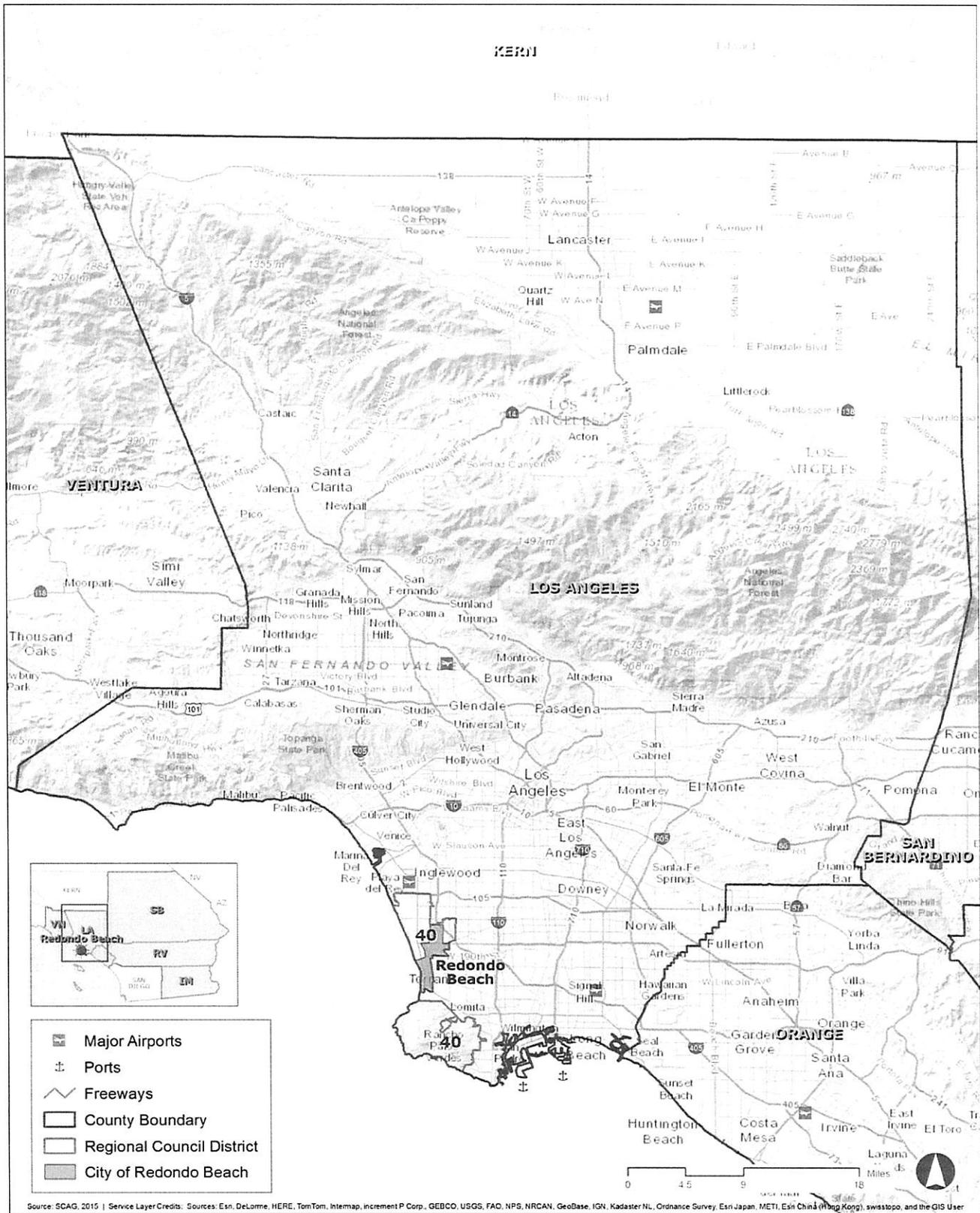
Southern California Association of Governments' (SCAG) Regional Council includes 69 districts which represent 191 cities in the SCAG region.

SCAG Regional Council District 40 includes El Segundo, Hermosa Beach, Lawndale, Manhattan Beach, Palos Verdes Estates, Rancho Palos Verdes, Redondo Beach, Rolling Hills, and Rolling Hills Estates  
Represented by: Hon. Judy Mitchell



## LOCAL PROFILES REPORT 2015

This profile report was prepared by the Southern California Association of Governments and shared with the City of Redondo Beach. SCAG provides local governments with a variety of benefits and services including, for example, data and information, GIS training, planning and technical assistance, and sustainability planning grants.



Source: 2012 SCAG city boundary data, which is provided by the county Local Agency Formation Commissions.

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## I. Introduction

The purpose of this report is to provide current information and data for the City of Redondo Beach for planning and outreach efforts. Information on population, housing, transportation, employment, retail sales, and education can be utilized by the city to make informed planning decisions. The profile provides a portrait of the city and its changes since 2000, using average figures for Los Angeles County as a comparative baseline. In addition, the most current data available for the region is also included in the Statistical Summary (page 3). This profile demonstrates the current trends occurring in the City of Redondo Beach.

The Southern California Association of Governments (SCAG) is the largest Metropolitan Planning Organization (MPO) in the nation. The SCAG region includes six counties (Imperial, Los Angeles, Orange, Riverside, San Bernardino, and Ventura) and 191 cities. As the designated MPO, SCAG is mandated by federal law to research and develop a Regional Transportation Plan (RTP), which incorporates a Sustainable Communities Strategy (SCS) per California state law. SCAG is currently undertaking a variety of planning and policy initiatives to foster a more sustainable Southern California.

In 2008, SCAG initiated the Local Profiles Project as a part of a larger initiative to provide a variety of services to its member cities and counties. Through extensive input from member jurisdictions, the inaugural Local Profiles Reports were released at the General Assembly in May 2009. The Profiles have been updated every two years.

Local Profiles provide basic information about each member jurisdiction including, but not limited to, the following:

- How much growth in population has taken place since 2000?
- Has the local jurisdiction been growing faster or slower than the county or regional average?
- Have there been more or fewer school-age children?
- Have homeownership rates been increasing or decreasing?
- How and where do residents travel to work?
- How has the local economy been changing in terms of employment share by sectors?
- Have the local retail sales revenues recovered to pre-recession levels?

Answers to questions such as these provide a snapshot of the dynamic changes affecting each local jurisdiction.

### **Factors Affecting Local Changes Reflected in the 2015 Report**

Overall, member jurisdictions since 2000 were impacted by a variety of factors at the national, regional, and local levels. For example, the vast majority of member jurisdictions included in the 2015 Local Profiles reflect the national demographic trends toward an older and a more diverse population. Evidence of the slow process towards economic recovery is also apparent through gradual increases in employment, retail sales, building permits, and home prices. Work destinations and commute times correlate with regional development patterns and the geographical location of local jurisdictions, particularly in relation to the regional transportation system.

## **Uses of the Local Profiles**

Following release at the SCAG General Assembly, the Local Profiles are posted on the SCAG website and are used for a variety of purposes including, but not limited to, the following:

- Data and communication resources for elected officials, businesses, and residents
- Community planning and outreach
- Economic development
- Visioning initiatives
- Grant application support
- Performance monitoring

The primary user groups of the Profiles include member jurisdictions and state and federal legislative delegates of Southern California. This profile report is a SCAG member benefit and the use of the data contained within this report is voluntary.

## **Report Organization**

This profile report has three sections. The first section presents a Statistical Summary for the City of Redondo Beach. The second section provides detailed information organized by subject areas and includes brief highlights on the impacts of the recent economic recession and recovery at the regional level. The third section, Methodology, describes technical considerations related to data definitions, measurement, and data sources.

## 2014 STATISTICAL SUMMARY

<i>Category</i>	<i>Redondo Beach</i>	<i>Los Angeles County</i>	<i>Redondo Beach relative to Los Angeles County*</i>	<i>SCAG Region</i>
<b>2014 Total Population</b>	67,717	10,041,797	[0.67%]	18,645,437
<b>2014 Median Age (Years)</b>	40.8	35.8	5	35.5
<b>2014 Hispanic</b>	15.7%	49.0%	-33.3%	46.8%
<b>2014 Non-Hispanic White</b>	63.6%	26.6%	37.0%	31.8%
<b>2014 Non-Hispanic Asian</b>	12.4%	13.8%	-1.4%	12.4%
<b>2014 Non-Hispanic Black</b>	2.8%	7.9%	-5.1%	6.3%
<b>2014 Non-Hispanic American Indian</b>	.3%	.2%	.1%	.3%
<b>2014 All Other Non-Hispanic</b>	5.3%	2.4%	2.9%	2.5%
<b>2014 Number of Households</b>	29,023	3,268,347	[0.89%]	6,029,326
<b>2014 Average Household Size</b>	2.3	3.0	-0.7	3.1
<b>2014 Median Household Income (\$)</b>	86,119	53,125	32,994	56,737
<b>2014 Number of Housing Units</b>	30,622	3,474,152	[0.88%]	6,524,730
<b>2014 Homeownership Rate</b>	51.3%	54.5%	-3.2%	54.5%
<b>2014 Median Existing Home Sales Price (\$)</b>	770,000	450,000	320,000	426,000
<b>2013 - 2014 Median Home Sales Price Change</b>	2.8%	9.2%	-6.4%	10.1%
<b>2014 Drive Alone to Work</b>	86.6%	76.0%	10.6%	78.2%
<b>2014 Mean Travel Time to Work (minutes)</b>	30.9	32.4	-1.5	31.7
<b>2013 Number of Jobs</b>	24,894	4,372,376	[0.57%]	7,660,489
<b>2012 - 2013 Total Jobs Change</b>	851	137,233	[0.62%]	231,953
<b>2013 Average Salary per Job (\$)</b>	64,096	51,493	12,603	48,213
<b>2014 K-12 Public School Student Enrollment</b>	9,547	1,542,223	.6%	3,058,957

Sources: U.S. Census Bureau American Community Survey, 2014; Nielsen Co.; California Department of Finance E-5, May 2014; MDA Data Quick; California Department of Education; and SCAG

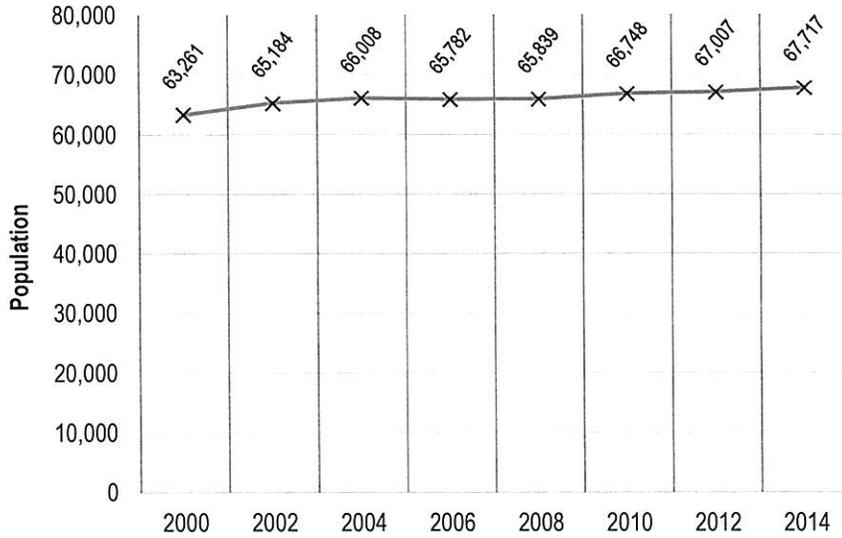
\* Numbers with [ ] represent Redondo Beach's share of Los Angeles County. The other numbers represent the difference between Redondo Beach and Los Angeles County.

Mapped jurisdictional boundaries are as of July 1, 2012 and are for visual purposes only. Report data, however, are updated according to their respective sources.

## II. Population

### Population Growth

Population: 2000 - 2014

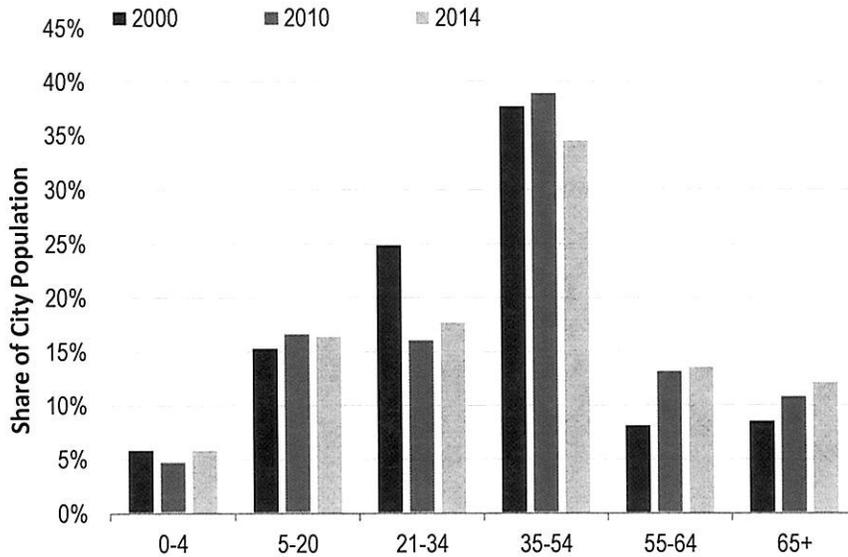


Source: California Department of Finance, E-5, 2014

- Between 2000 and 2014, the total population of the City of Redondo Beach increased by 4,456 to 67,717 in 2014.
- During this 14-year period, the city's population growth rate of 7 percent was higher than the Los Angeles County rate of 5.5 percent.
- 0.67% of the total population of Los Angeles County is in the City of Redondo Beach.

## Population by Age

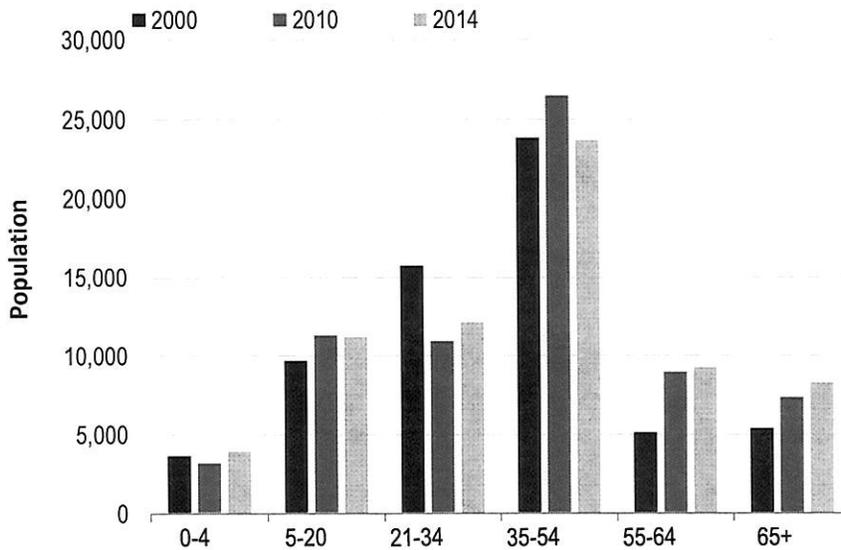
Population Share by Age: 2000, 2010, and 2014



Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- Between 2000 and 2014, the age group 55-64 experienced the largest increase in share, growing from 8.1 to 13.5 percent.
- The age group that experienced the greatest decline, by share, was age group 21-34, decreasing from 24.8 to 17.7 percent.

Population by Age: 2000, 2010, and 2014

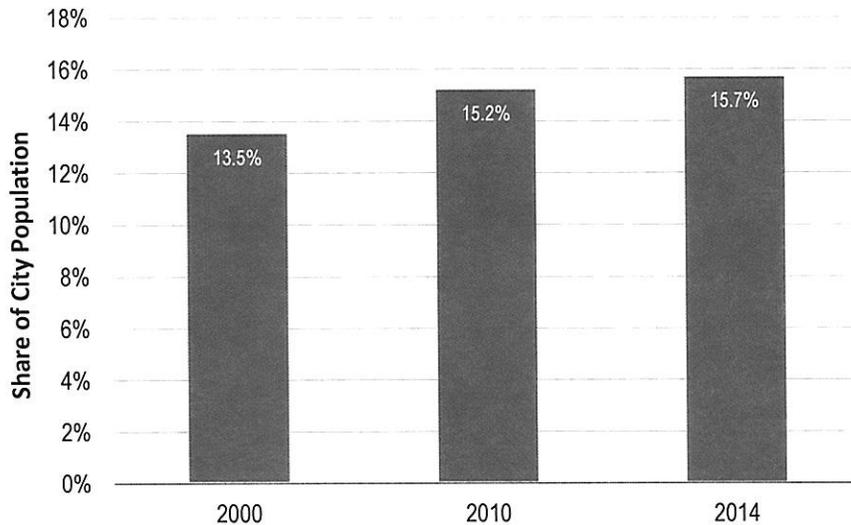


Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- The age group 55-64 added the most population, with an increase of 4,176 people between 2000 and 2014.

## Population by Race/Ethnicity

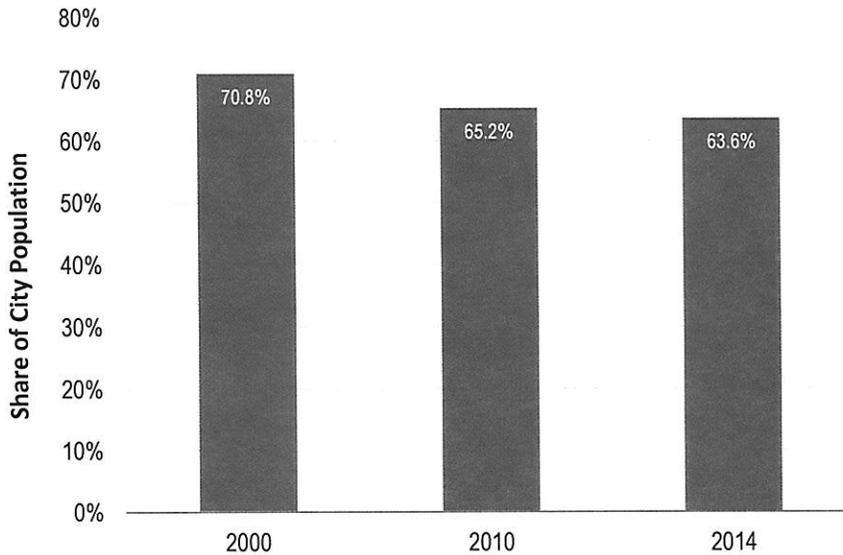
### Hispanic or Latino of Any Race: 2000, 2010, and 2014



Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- Between 2000 and 2014, the share of Hispanic population in the city increased from 13.5 percent to 15.7 percent.

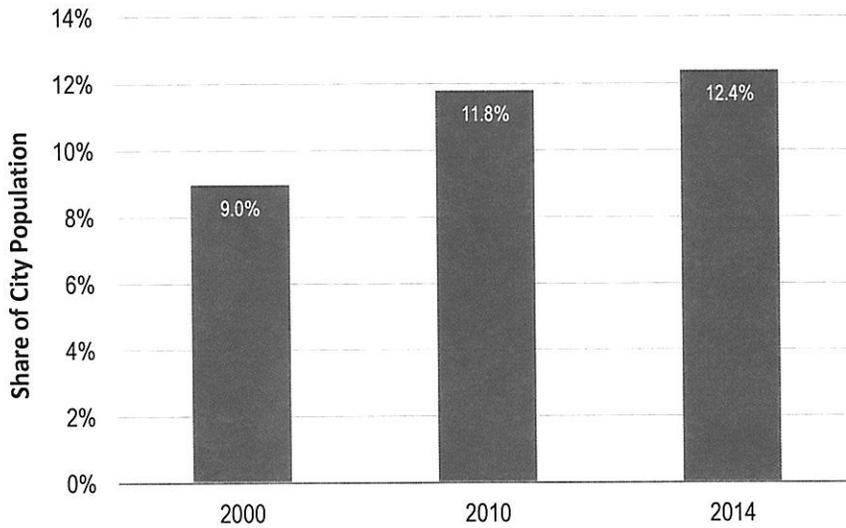
### Non-Hispanic White: 2000, 2010, and 2014



Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- Between 2000 and 2014, the share of Non-Hispanic White population in the city decreased from 70.8 percent to 63.6 percent.
- Please refer to the Methodology section for definitions of the racial/ethnic categories.

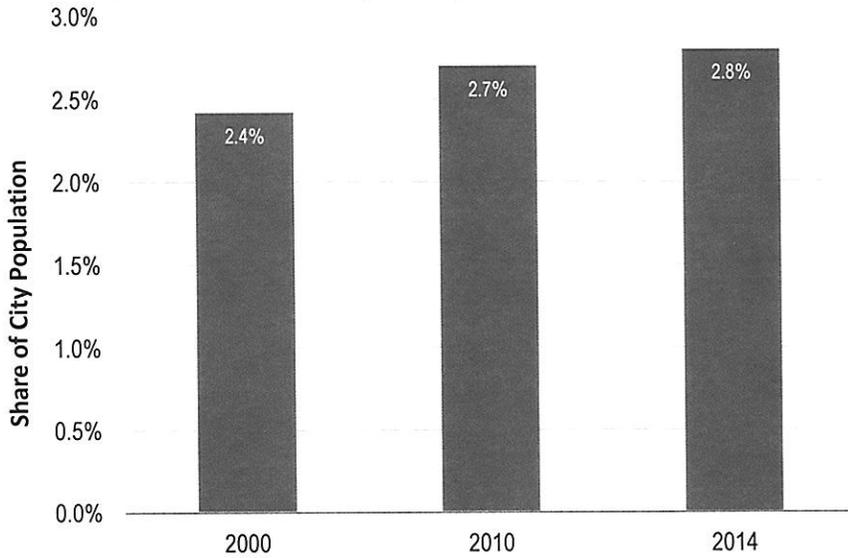
**Non-Hispanic Asian: 2000, 2010, and 2014**



Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- Between 2000 and 2014, the share of Non-Hispanic Asian population in the city increased from 9.0 percent to 12.4 percent.

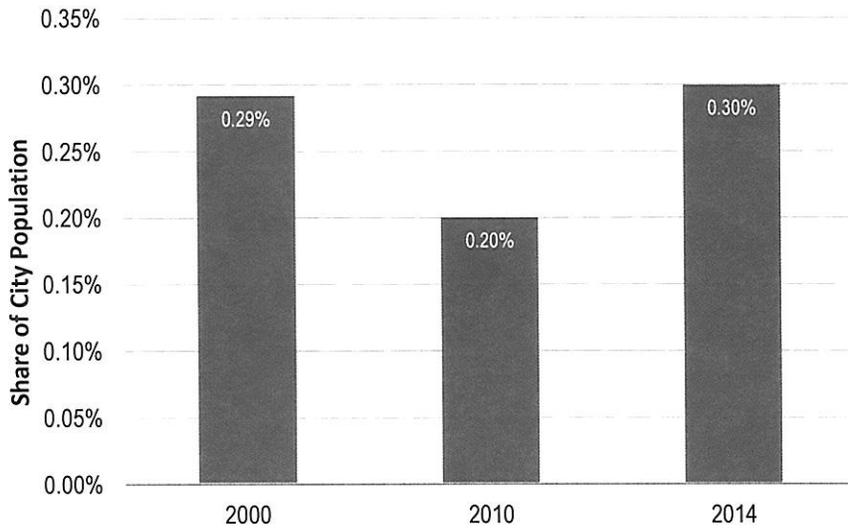
**Non-Hispanic Black: 2000, 2010, and 2014**



Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- Between 2000 and 2014, the share of Non-Hispanic Black population in the city increased from 2.4 percent to 2.8 percent.

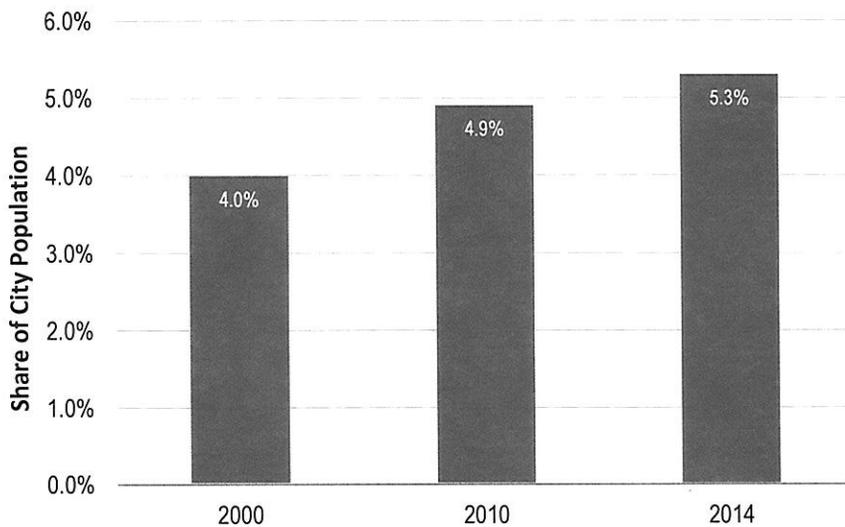
**Non-Hispanic American Indian: 2000, 2010, and 2014**



Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- Between 2000 and 2014, the share of Non-Hispanic American Indian population in the city remained at 0.3 percent.

**All Other Non-Hispanic: 2000, 2010, and 2014**



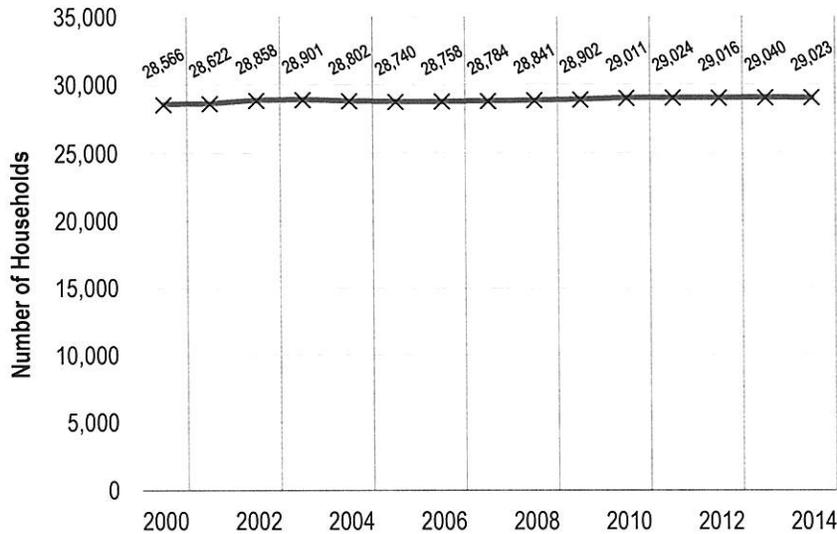
Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

- Between 2000 and 2014, the share of All Other Non-Hispanic population group in the city increased from 4.0 percent to 5.3 percent.

### III. Households

#### Number of Households (Occupied Housing Units)

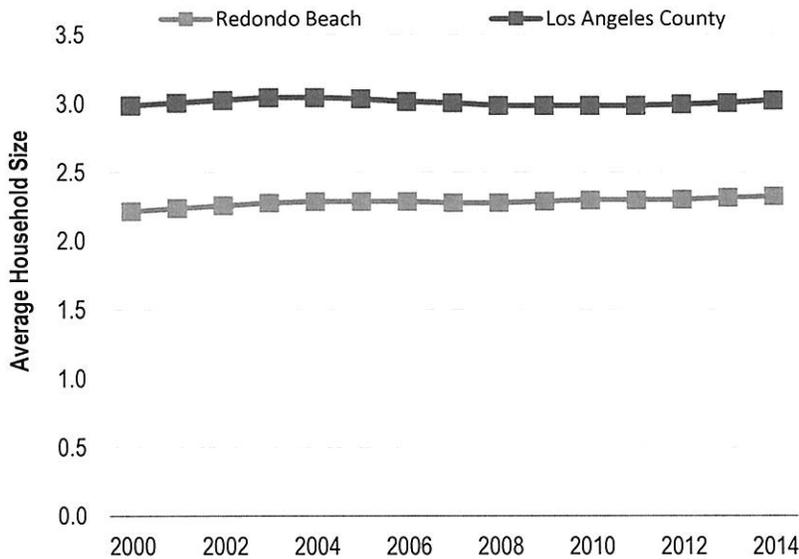
Number of Households: 2000 - 2014



Sources: 2000 and 2010 U.S. Decennial Census; California Department of Finance, E-5, 2014

- Between 2000 and 2014, the total number of households in the City of Redondo Beach increased by 457 units, or 1.6 percent.
- During this 14-year period, the city's household growth rate of 1.6 percent was lower than the county growth rate of 4.3 percent.
- 0.89 percent of Los Angeles County's total number of households are in the City of Redondo Beach.
- In 2014, the city's average household size was 2.3, lower than the county average of 3.0.

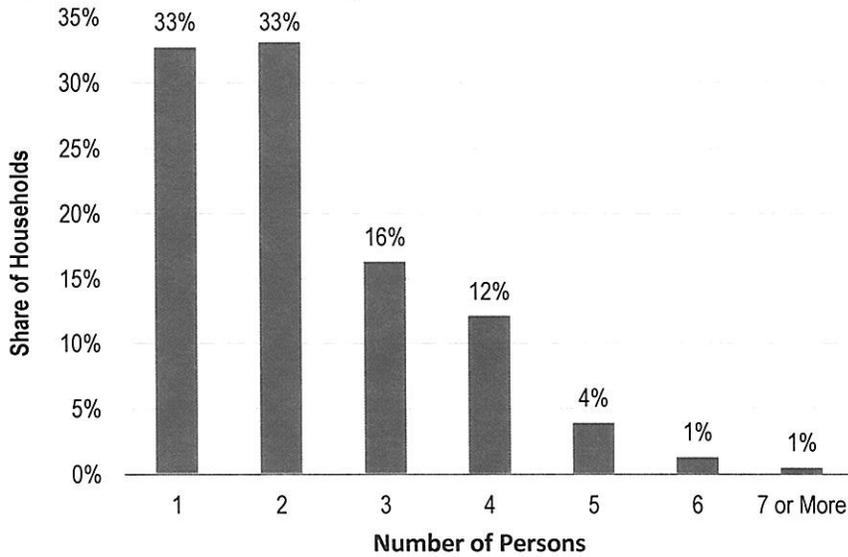
Average Household Size: 2000 - 2014



Source: California Department of Finance, E-5, 2014

## Households by Size

Percent of Households by Household Size: 2014

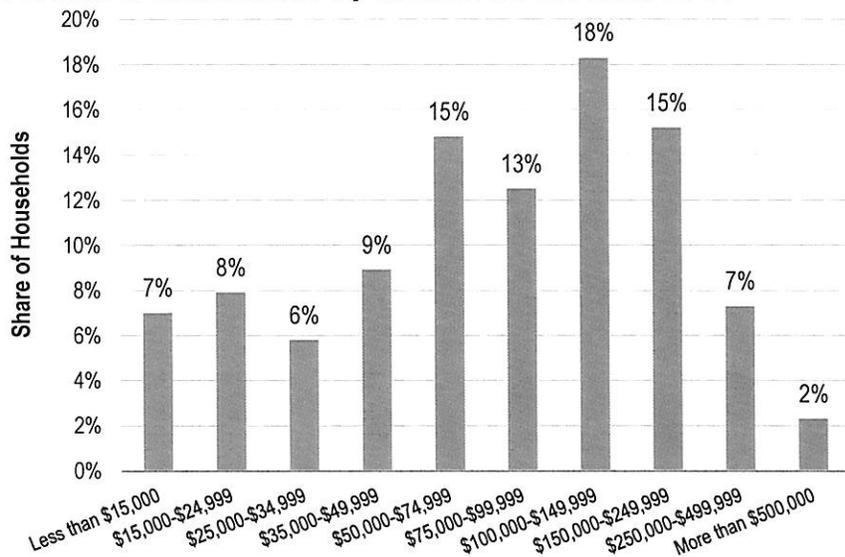


- In 2014, 82.1 percent of all city households had 3 people or fewer.
- About 33 percent of the households were single-person households.
- Approximately 6 percent of all households in the city had 5 people or more.

Source: Nielsen Co., 2014

## Households by Income

Percent of Households by Household Income: 2014

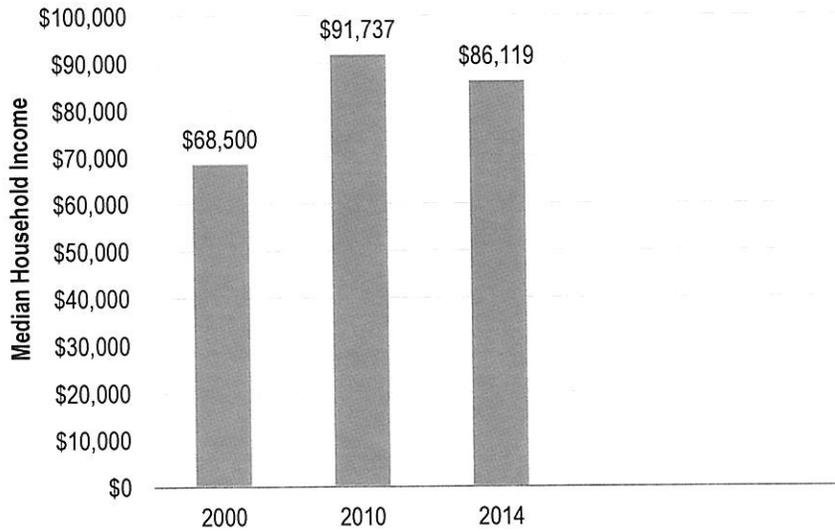


- In 2014, about 30 percent of households earned less than \$50,000 annually.
- Approximately 42 percent of households earned \$100,000 or more.

Source: Nielsen Co., 2014

## Household Income

### Median Household Income: 2000, 2010, & 2014

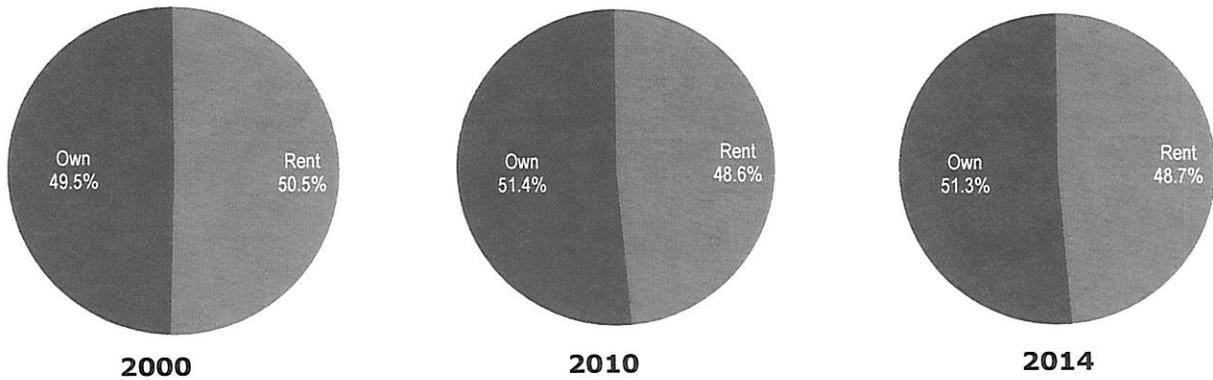


- From 2000 to 2014, median household income increased by \$17,619.
- Note: Dollars are not adjusted for annual inflation.

Source: Nielsen Co., 2014

## Renters and Homeowners

### Percentage of Renters and Homeowners: 2000, 2010, & 2014



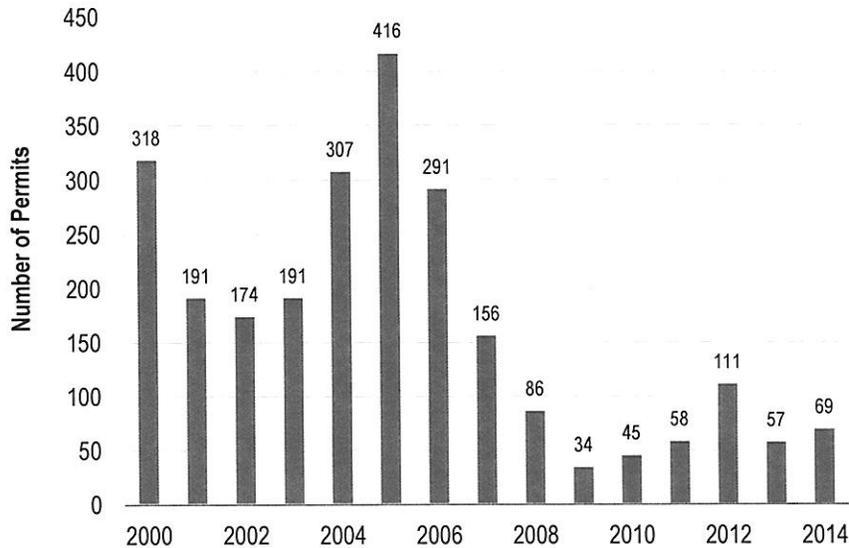
Sources: 2000 and 2010 U.S. Decennial Census; Nielsen Co., 2014

Between 2000 and 2014, homeownership rates increased and the share of renters decreased.

## IV. Housing

### Total Housing Production

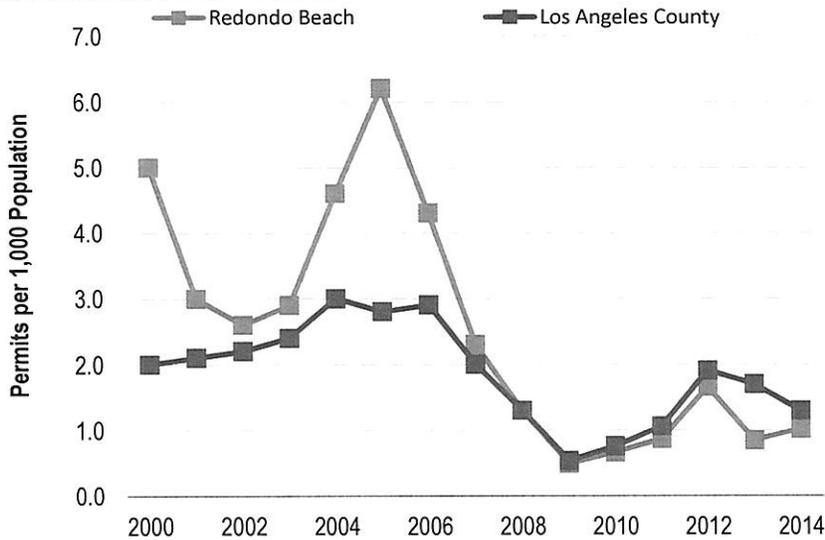
#### Total Permits Issued for all Residential Units: 2000 - 2014



Source: Construction Industry Research Board, 2000 - 2014

- Between 2000 and 2014, permits were issued for 2,504 residential units.

#### Permits Issued for all Residential Units per 1,000 Residents: 2000 - 2014

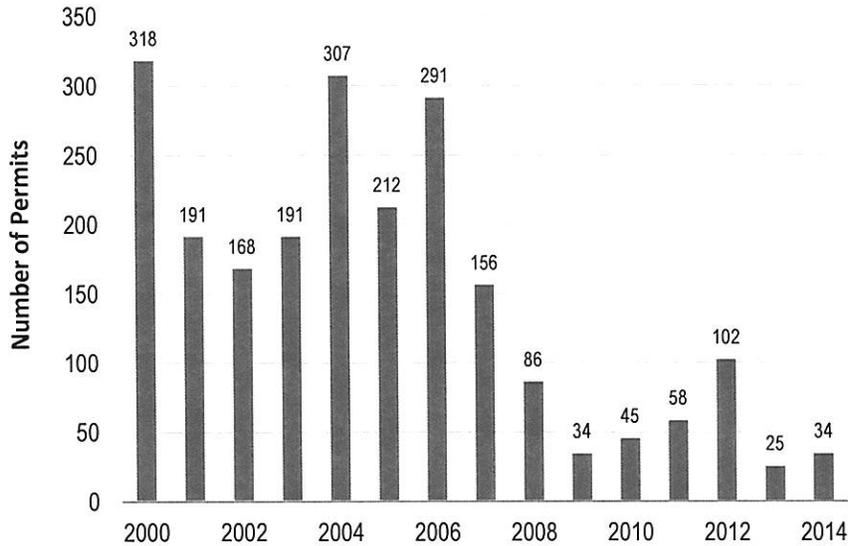


Source: Construction Industry Research Board, 2000 - 2014

- In 2000, the City of Redondo Beach had 5 permits per 1,000 residents compared to the overall county figure of 2 permits per 1,000 residents.
- For the city in 2014, the number of permits per 1,000 residents decreased to 1 permit. For the county overall, it decreased to 1.3 permits per 1,000 residents.

## Single-Family Housing Production

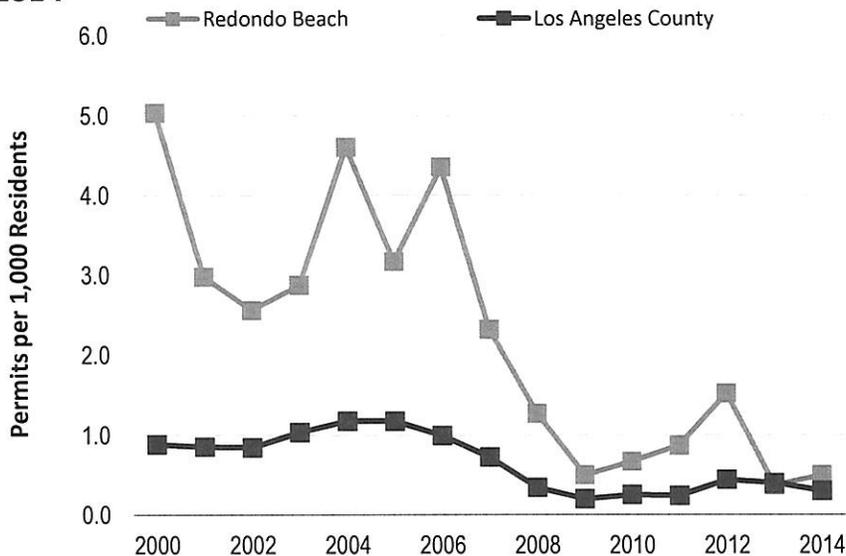
### Permits Issued for Single-Family Units: 2000 - 2014



Source: Construction Industry Research Board, 2000 - 2014

- Between 2000 and 2014, permits were issued for 2,218 single family homes.
- 7.3 percent of these were issued in the last 3 years.

### Single-Family Permits Issued per 1,000 Residents: 2000 - 2014

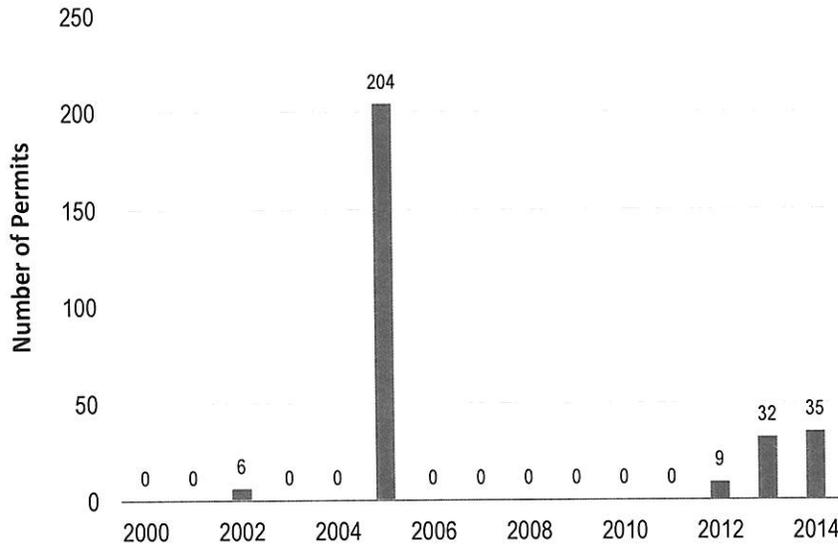


Source: Construction Industry Research Board, 2000 - 2014

- In 2000, the City of Redondo Beach issued 5 permits per 1,000 residents compared to the overall county figure of 0.9 permits per 1,000 residents.
- For the city in 2014, the number of permits issued per 1,000 residents decreased to 0.5 permits. For the county overall, it decreased to 0.3 permits per 1,000 residents.

## Multi-Family Housing Production

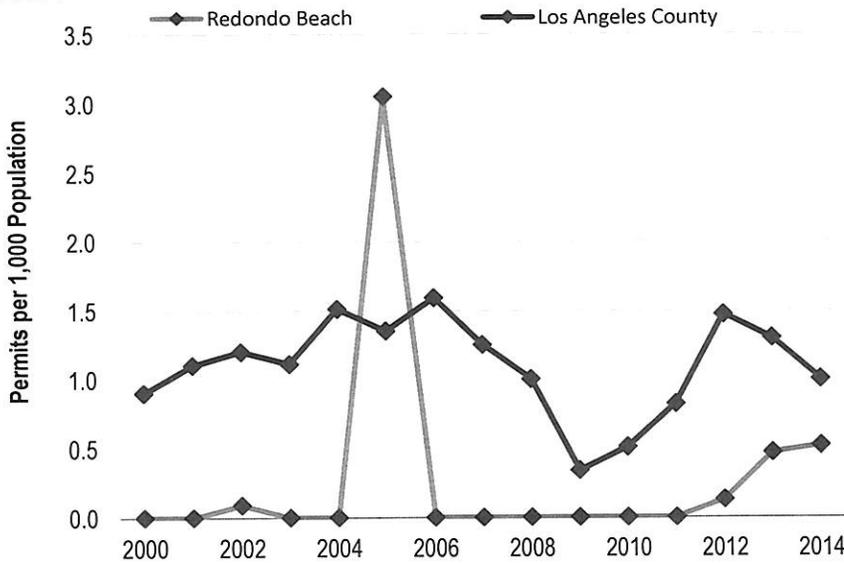
### Permits Issued for Multi-Family Units: 2000 - 2014



Source: Construction Industry Research Board, 2000-2014

- Between 2000 and 2014, there were permits issued for 286 multi-family residential units.

### Multi-Family Permits Issued per 1,000 Residents: 2000 - 2014

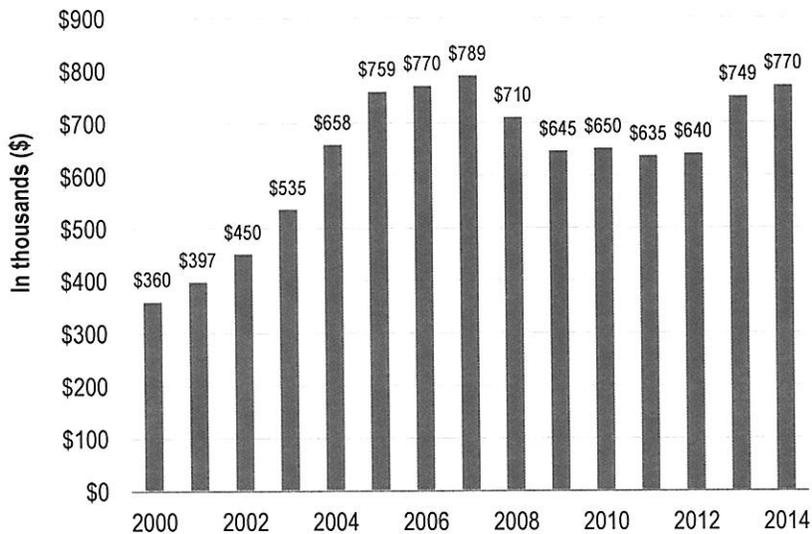


Source: Construction Industry Research Board, 2000-2014

- For the city in 2014, the number of permits per 1,000 residents increased to 0.5 permits. For the county overall, it increased to 1 permit per 1,000 residents.

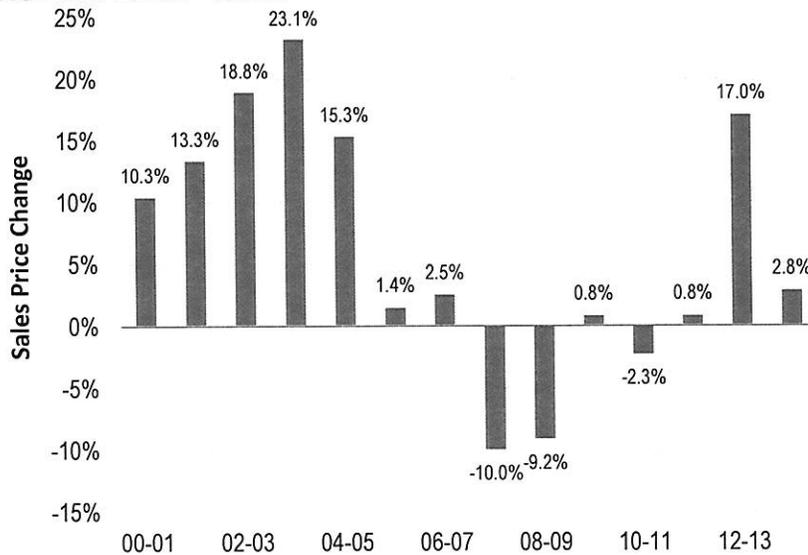
## Home Sales Prices

**Median Home Sales Price for Existing Homes: 2000 - 2014 (in \$ thousands)**



Source: MDA Data Quick, 2014

**Annual Median Home Sales Price Change for Existing Homes: 2000 - 2014**



Source: MDA Data Quick, 2014

- Between 2000 and 2014, the median home sales price increased 114 percent from \$360,000 to \$770,000.
- Median home sales price increased by 18.5 percent between 2010 and 2014.
- In 2014, the median home sales price in the city was \$770,000, \$320,000 higher than that in the county overall.
- Note: Median home sales price reflects resale of existing homes and provides guidance on the market values of homes sold.
- Between 2000 and 2014, the largest single year increase was 23.1 percent.

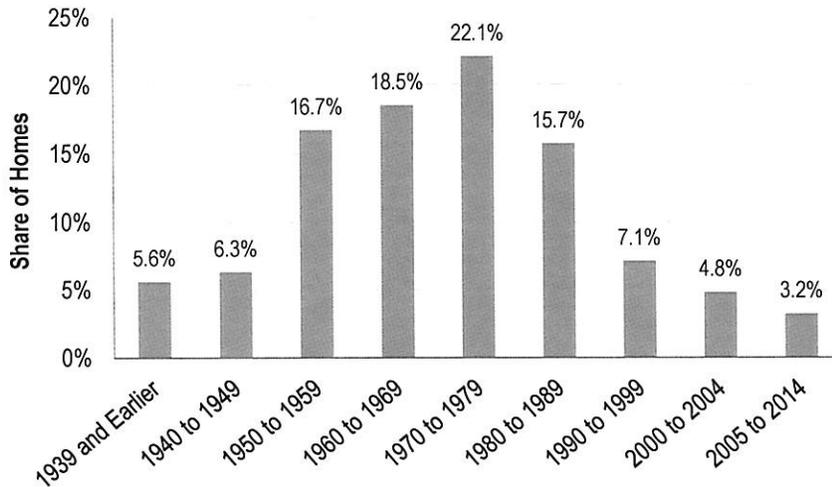
## Housing Units by Housing Type: 2014

Housing Type	Number of Units	Percent of Total Units
Single Family Detached	12,097	39.5 %
Single Family Attached	4,254	13.9 %
Multi-family: 2 to 4 units	4,630	15.1 %
Multi-family: 5 units plus	9,382	30.6 %
Mobile Home	259	0.85 %
<b>Total</b>	<b>30,622</b>	<b>100 %</b>

- The most common housing type is Single Family Detached.
- Approximately 53.4 percent were single family homes and 45.8 percent were multi-family homes.

Source: California Department of Finance, E-5, 2014

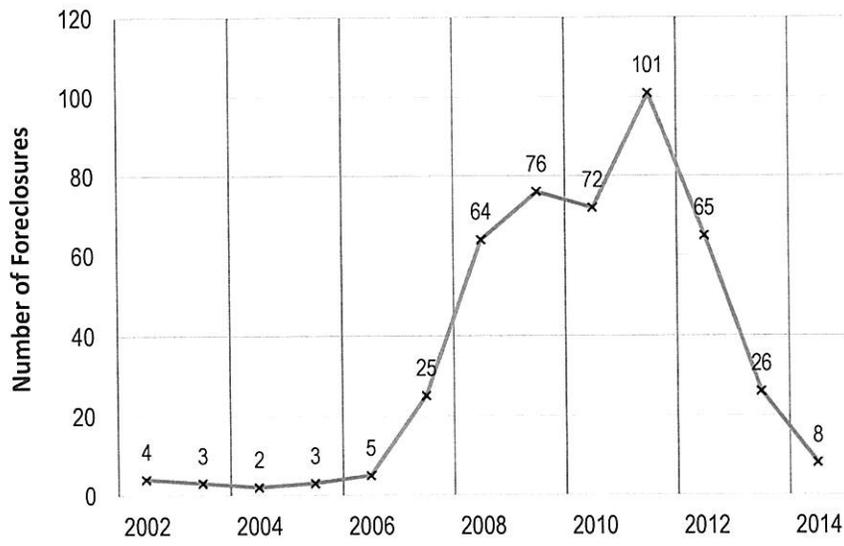
## Age of Housing Stock



- About 48 percent of the housing stock was built before 1970.
- Approximately 52 percent of the housing stock was built after 1970.

Source: Nielsen Co., 2014

## Foreclosures



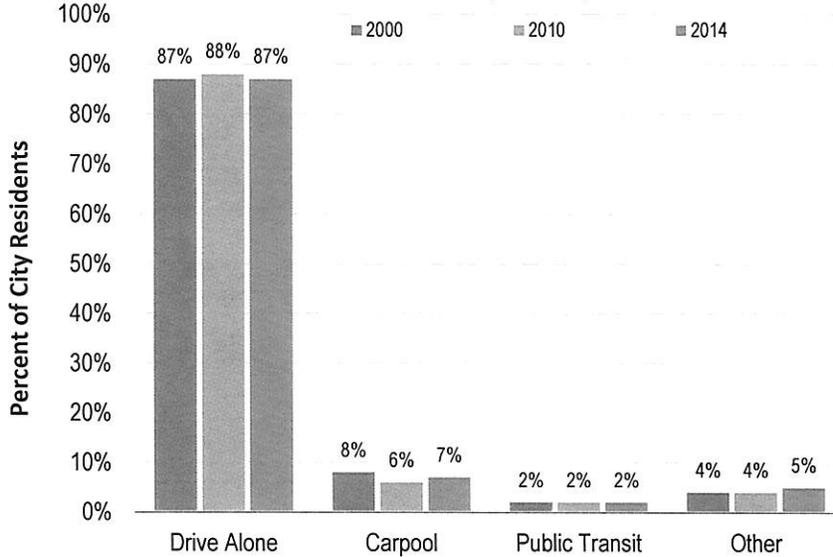
Source: MDA Data Quick, 2014

- There were a total of 8 foreclosures in 2014.
- Between 2007 and 2014, there were a total of 437 foreclosures.

## V. Transportation

### *Journey to Work for Residents*

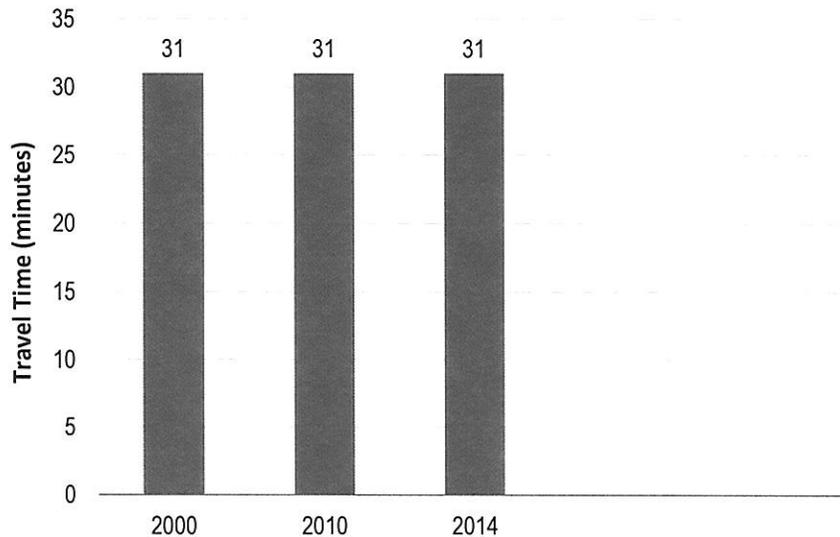
**Transportation Mode Choice: 2000, 2010, & 2014**



Sources: 2000 U.S. Decennial Census; 2010 U.S. Census American Community Survey; and Nielsen Co., 2014

- Between 2000 and 2014, the greatest change occurred in the percentage of individuals who traveled to work by other modes (e.g. work at home, walking or biking); this share increased by 1.2 percentage points.
- 'Other' refers to bicycle, pedestrian, and home-based employment.

**Average Travel Time (minutes): 2000, 2010, & 2014**



Sources: 2000 U.S. Decennial Census; 2010 U.S. Census American Community Survey; and Nielsen Co., 2014

- Between 2000 and 2014, the average travel time to work remained the same.

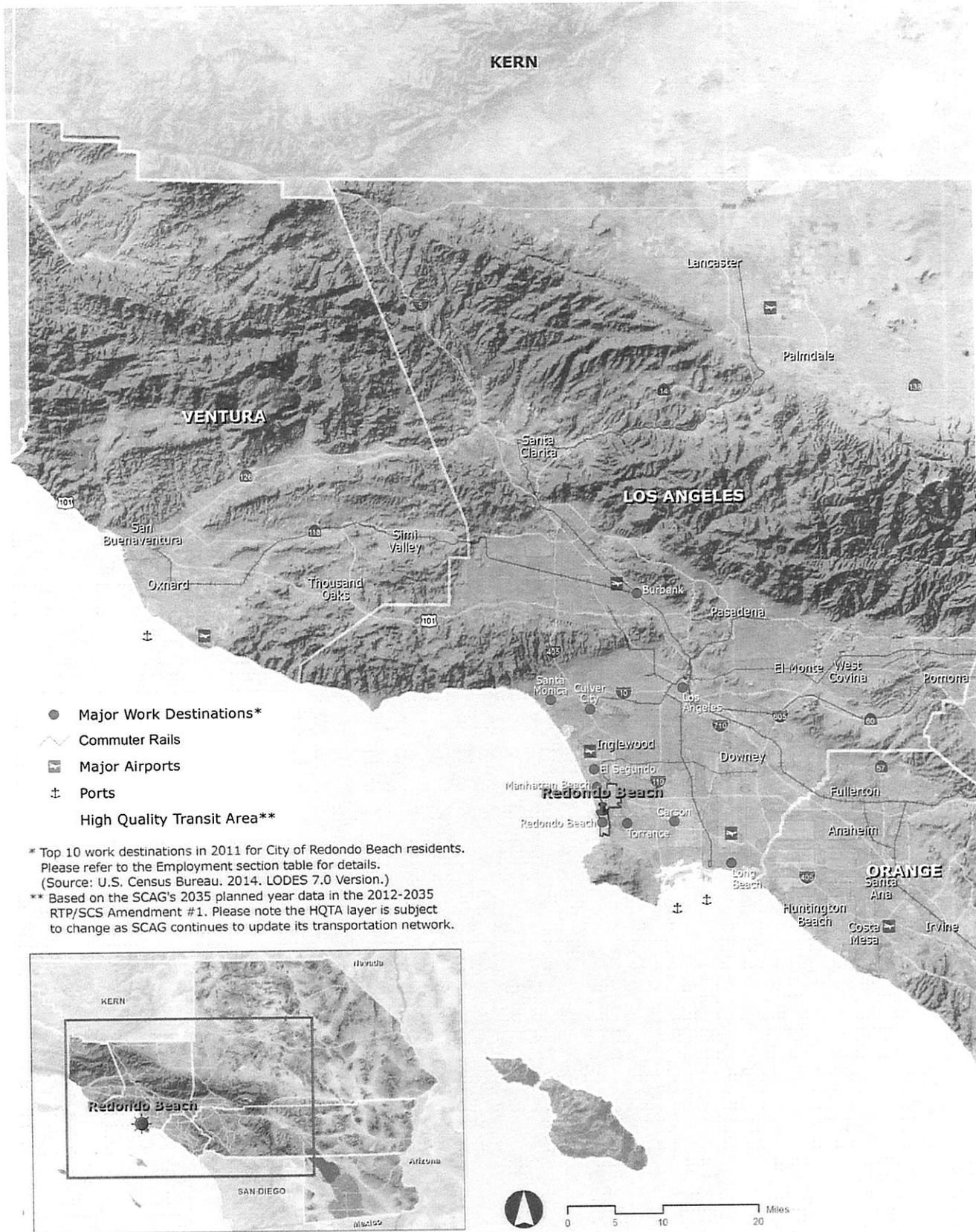
## VI. Employment

### Top 10 Places Where Residents Commute to Work

	<b>Local Jurisdiction</b>	<b>Number of Commuters</b>	<b>Percent of Total Commuters</b>
1.	Los Angeles	7,868	27.69 %
2.	Torrance	3,085	10.86 %
3.	El Segundo	2,803	9.86 %
4.	Redondo Beach	2,695	9.48 %
5.	Manhattan Beach	943	3.32 %
6.	Santa Monica	764	2.69 %
7.	Long Beach	620	2.18 %
8.	Burbank	575	2.02 %
9.	Carson	523	1.84 %
10.	Culver City	507	1.78 %
	All Other Destinations	8,035	28.27 %

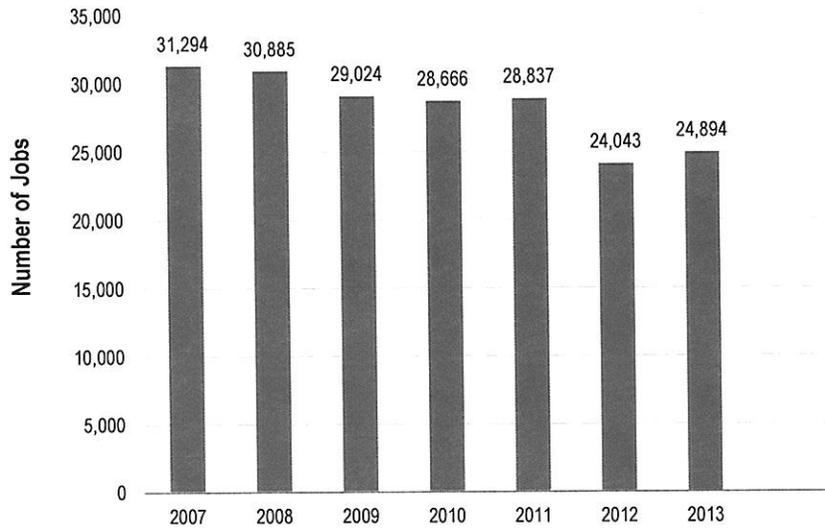
Source: U.S. Census Bureau, 2014, LODES Data; Longitudinal-Employer Household Dynamics Program, 2011

- This table identifies the top 10 locations where residents from the City of Redondo Beach commute to work.
- 9.5% work in the city where they live, while 90.5% commute to other places.





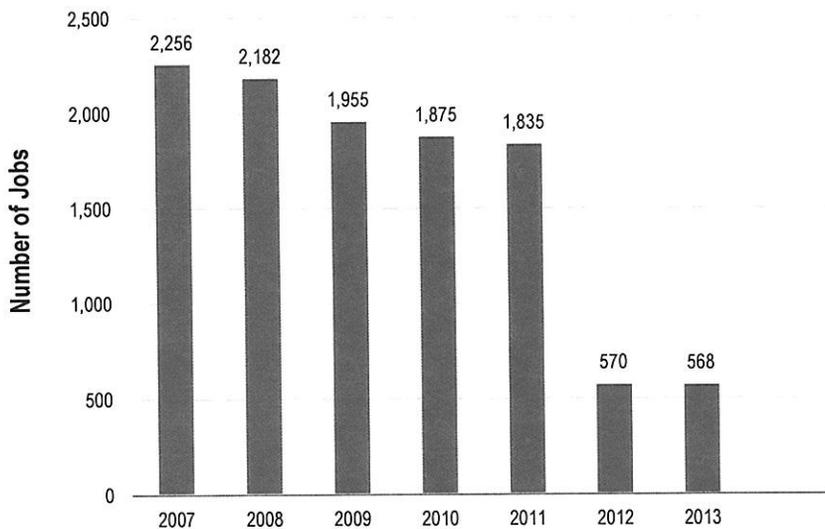
**Total Jobs: 2007 - 2013**



Sources: California Employment Development Department, 2007 - 2013; InfoGroup; and SCAG

- Total jobs include wage and salary jobs and jobs held by business owners and self-employed persons. The total job count does not include unpaid volunteers or family workers, and private household workers.
- In 2013, total jobs in the City of Redondo Beach numbered 24,894, a decrease of 20.5 percent from 2007.

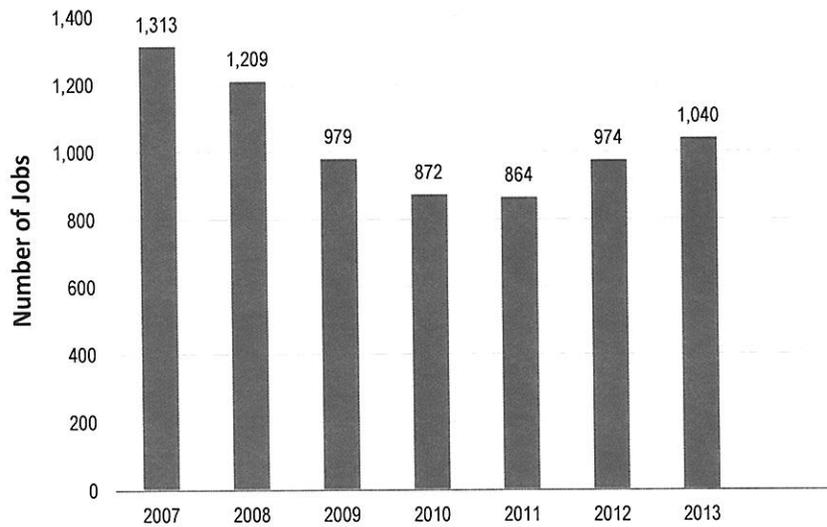
**Jobs in Manufacturing: 2007 - 2013**



Sources: California Employment Development Department, 2007 - 2013; InfoGroup; and SCAG

- Manufacturing jobs include those employed in various sectors including food; apparel; metal; petroleum and coal; machinery; computer and electronic products; and transportation equipment.
- In 2013, there were 568 manufacturing jobs in the city.

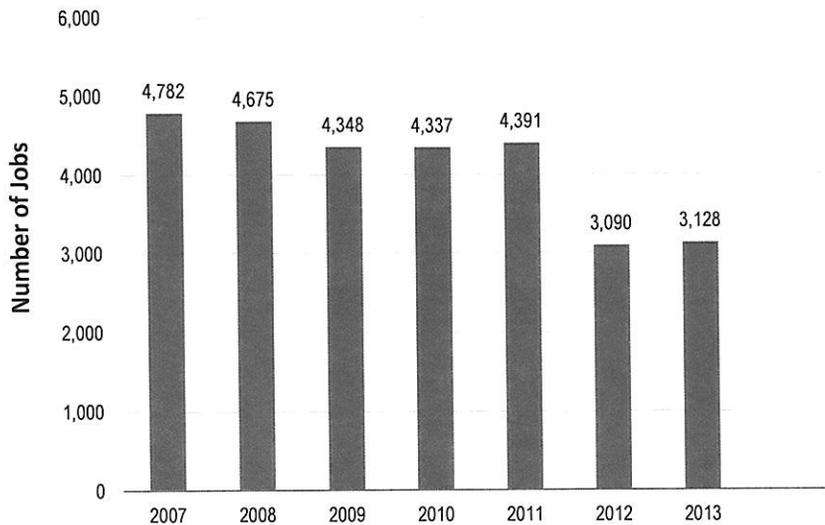
### Jobs in Construction: 2007 - 2013



Sources: California Employment Development Department, 2007 - 2013; InfoGroup; and SCAG

- Construction jobs include those engaged in both residential and non-residential construction.
- Between 2007 and 2013, construction jobs in the city decreased by 20.8 percent.

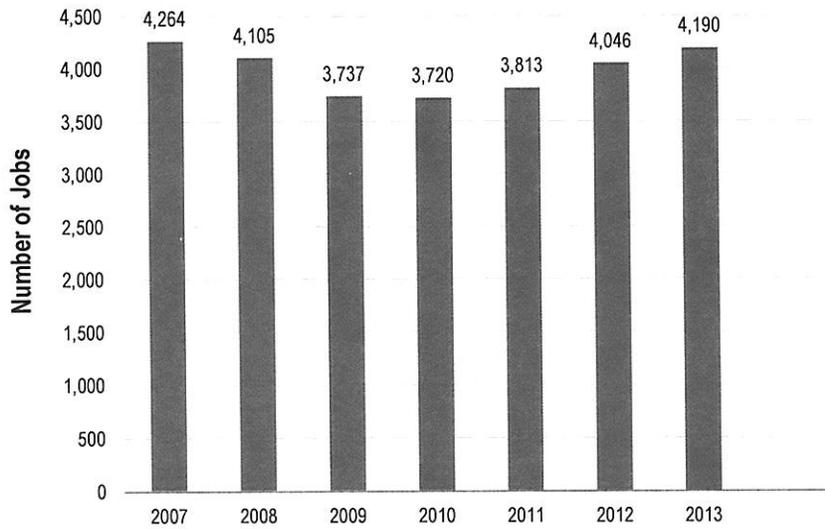
### Jobs in Retail Trade: 2007 - 2013



Sources: California Employment Development Department, 2007 - 2013; InfoGroup; and SCAG

- Retail trade jobs include those at various retailers including motor vehicle and parts dealers, furniture, electronics and appliances, building materials, food and beverage, clothing, sporting goods, books, and office supplies.
- In 2013, there were 3,128 retail trade jobs in the city.

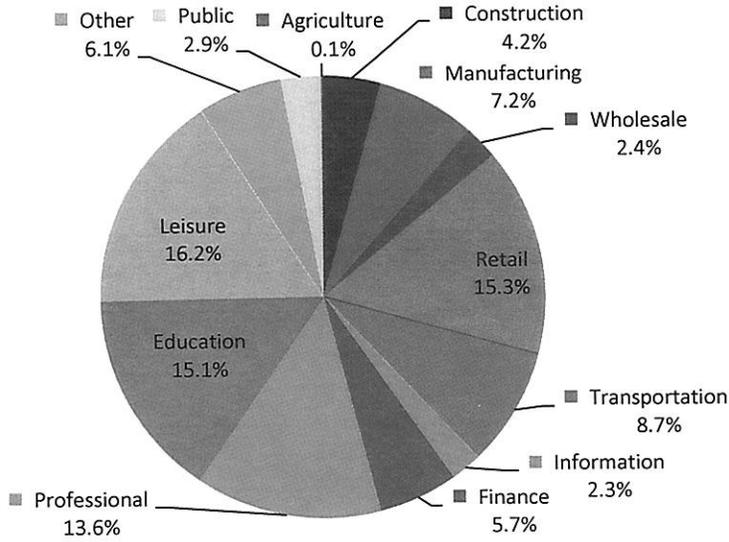
### Jobs in Professional and Management: 2007 - 2013



Sources: California Employment Development Department, 2007 - 2013; InfoGroup; and SCAG

- Jobs in the professional and management sector include those employed in professional and technical services, management of companies, and administration and support.
- Between 2007 and 2013, the number of professional and management jobs in the city decreased by 1.7 percent.

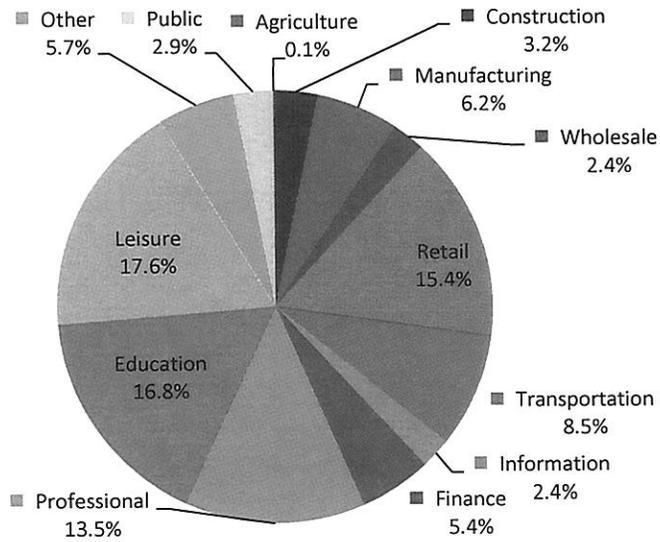
**Jobs by Sector: 2007**



Sources: California Employment Development Department, 2007; InfoGroup; and SCAG.

- From 2007 to 2013, the share of Education jobs increased from 15.1 percent to 16.8 percent.
- See Methodology Section for industry sector definitions.

**Jobs by Sector: 2013**

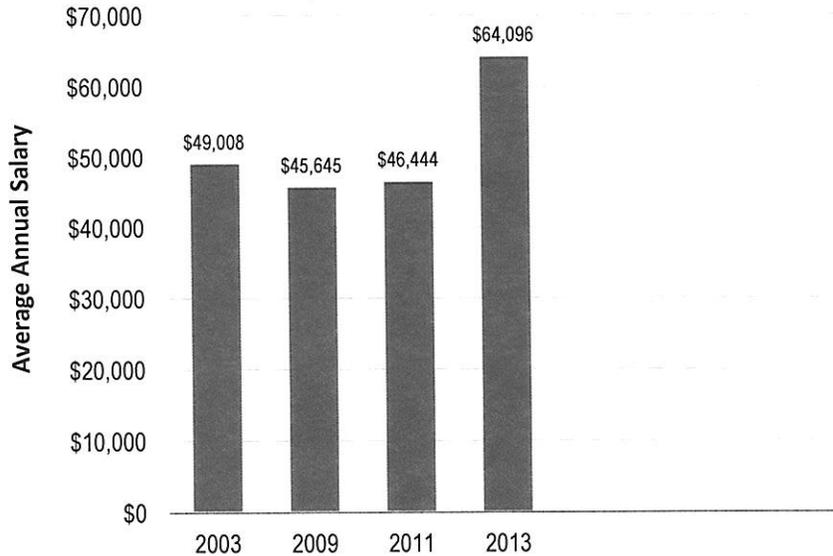


Sources: California Employment Development Department, 2014; InfoGroup; and SCAG.

- In 2013, the Leisure sector was the largest job sector, accounting for 17.6 percent of total jobs in the city.
- Other large sectors included Education (16.8 percent), Retail (15.4 percent), and Professional (13.5 percent).

## Average Salaries

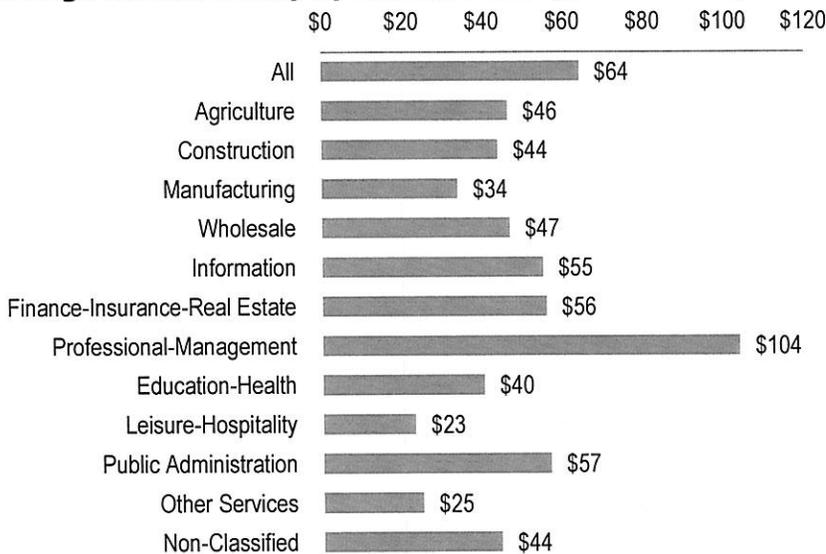
### Average Annual Salary: 2003, 2009, 2011, & 2013



Source: California Employment Development Department, 2003, 2009, 2011, 2013

- Average salaries for jobs located in the city increased from \$49,008 in 2003 to \$64,096 in 2013, a 30.8 percent change.
- Note: Dollars are not adjusted for annual inflation.

### Average Annual Salary by Sector: 2013 (\$ thousands)

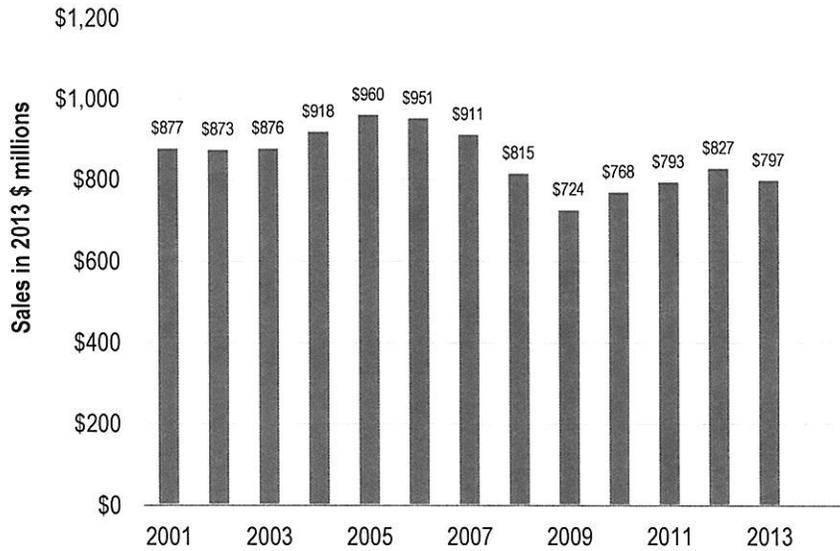


Source: California Employment Development Department, 2013

- In 2013, the employment sector providing the highest salary per job in the city was Professional-Management (\$103,693).
- The Leisure-Hospitality sector provided the lowest annual salary per job (\$22,877).

## VII. Retail Sales

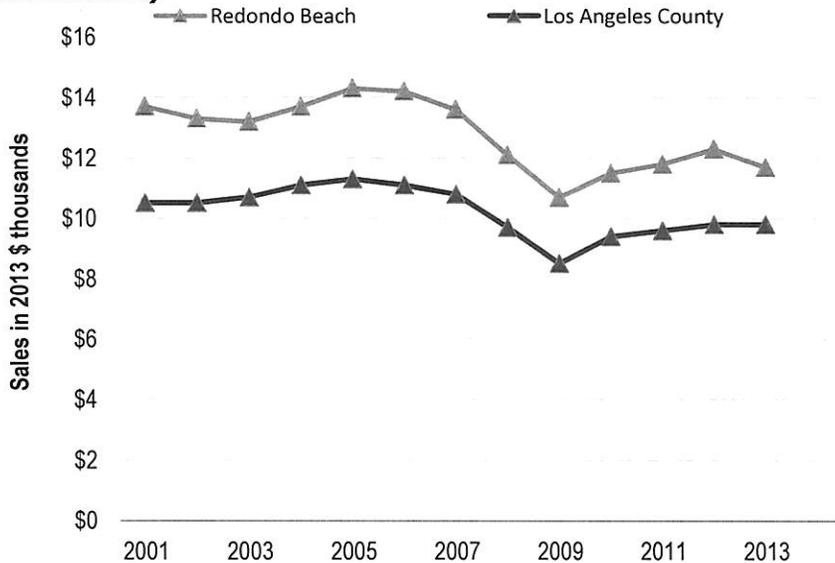
**Real Retail Sales: 2001 - 2013 (in 2013 \$ millions)**



Source: California Board of Equalization, 2001-2013

- Real (inflation adjusted) retail sales in the City of Redondo Beach increased by 4.5 percent between 2000 and 2005.
- Real retail sales decreased by 16.9 percent between 2005 and 2013.

**Real Retail Sales per Person: 2001 - 2013 (in 2013 \$ thousands)**

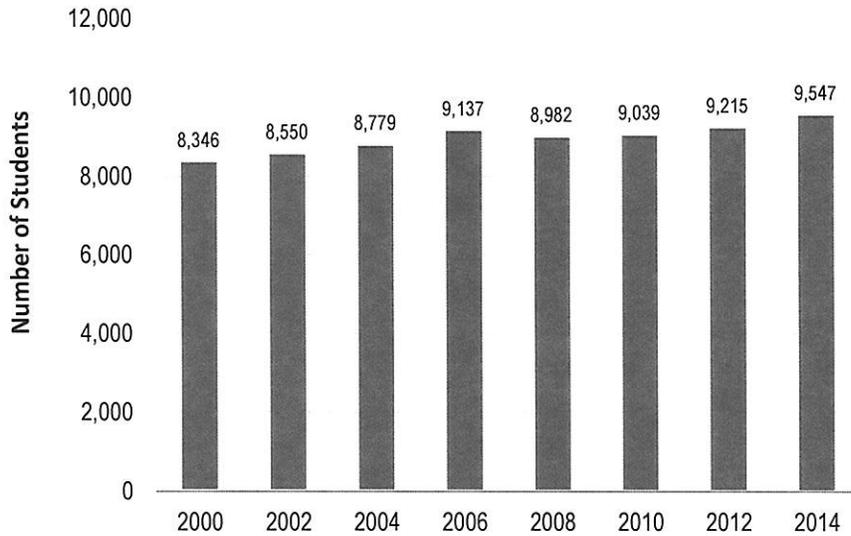


Source: California Board of Equalization, 2001-2013

- Between 2001 and 2013, retail sales per person for the city decreased from \$14,508 to \$11,819.

## VIII. Education

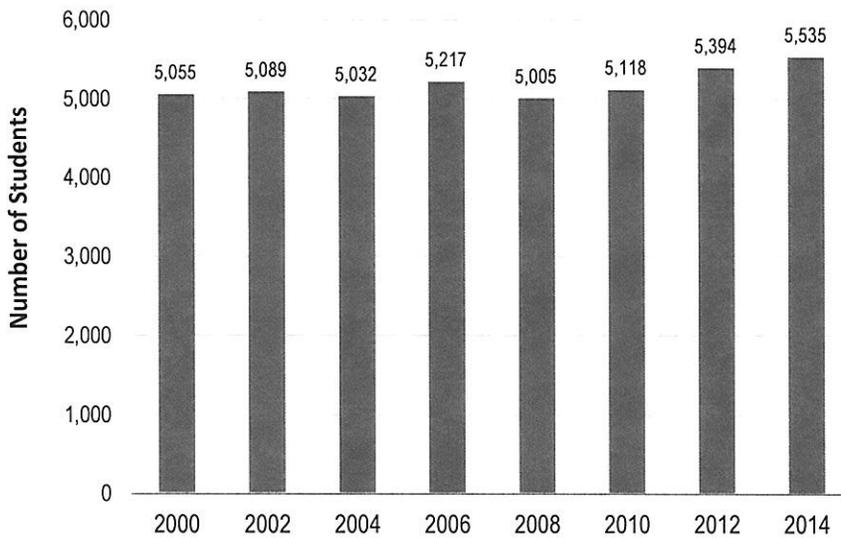
### K-12 Public School Student Enrollment: 2000 - 2014



Source: California Department of Education, 2000 - 2014

- Between 2000 and 2014, total K-12 public school enrollment for schools within the City of Redondo Beach increased by 1,201 students, or about 14.4 percent.

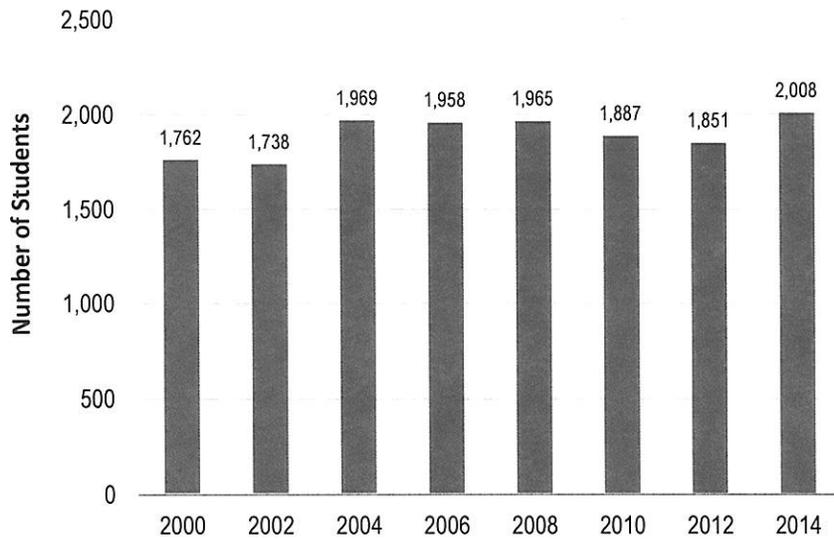
### K-6 Public School Student Enrollment: 2000 - 2014



Source: California Department of Education, 2000 - 2014

- Between 2000 and 2014, total public elementary school enrollment increased by 480 students or 9.5 percent.

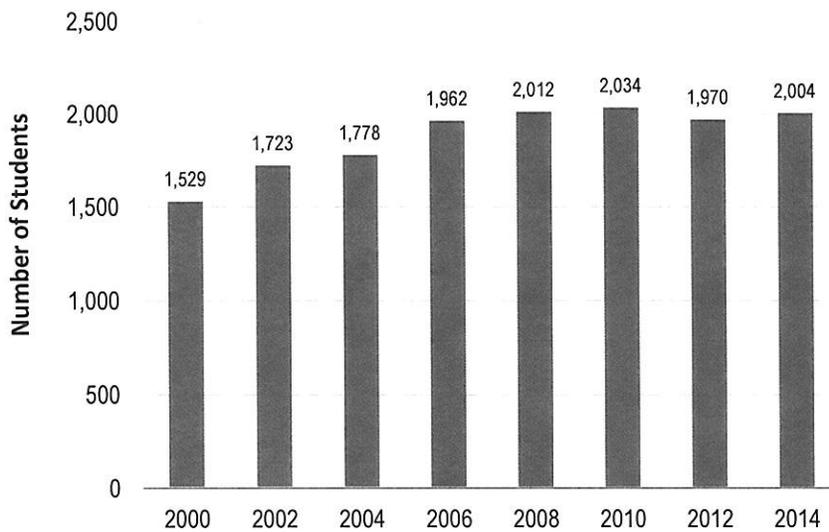
### Grades 7-9 Public School Student Enrollment: 2000 - 2014



Source: California Department of Education, 2000 - 2014

- Between 2000 and 2014, total public school enrollment for grades 7-9 increased by 246 students or 14 percent.

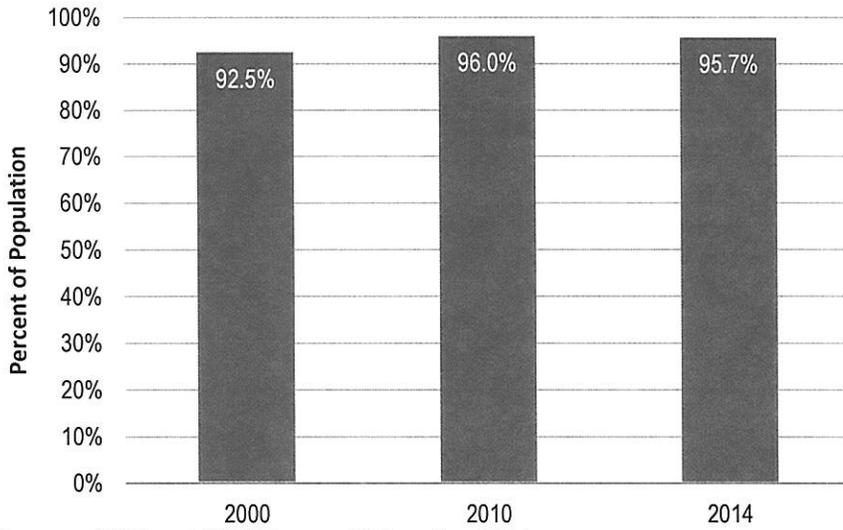
### Grades 10-12 Public School Student Enrollment: 2000 - 2014



Source: California Department of Education, 2000 - 2014

- Between 2000 and 2014, total public school enrollment for grades 10-12 increased by 475 students, about 31.1 percent.

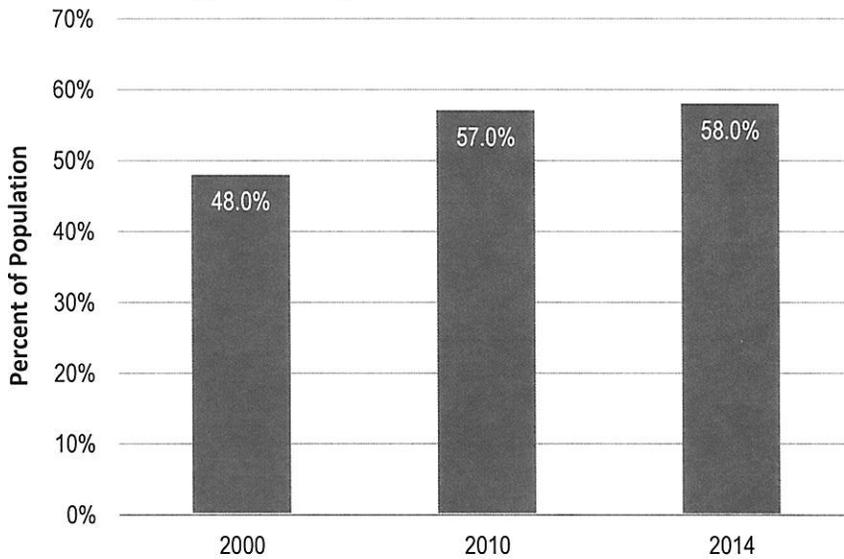
**Percent of City Population 25 Years & Over Completing High School or Higher**



Sources: 2000 and 2010 Census; Nielsen Co., 2014

- In 2014, 95.7 percent of the population 25 years and over completed high school or higher, which is higher than the 2000 level.

**Percent of City Population 25 Years & Over Completing a Bachelor's Degree or Higher**

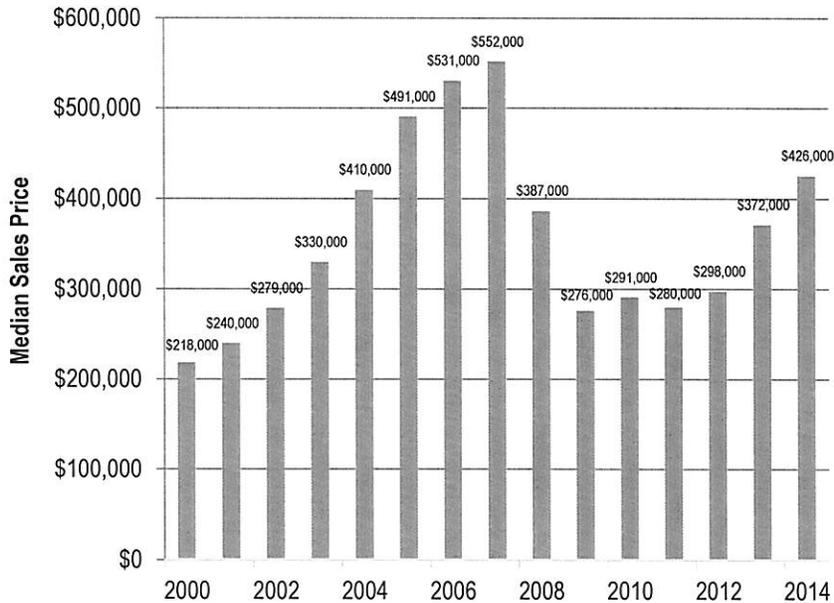


Sources: 2000 and 2010 Census; Nielsen Co., 2014

- In 2014, 58 percent of the population 25 years and over completed a Bachelor's degree or higher, which is higher than the 2000 level.

## IX. SCAG Regional Highlights

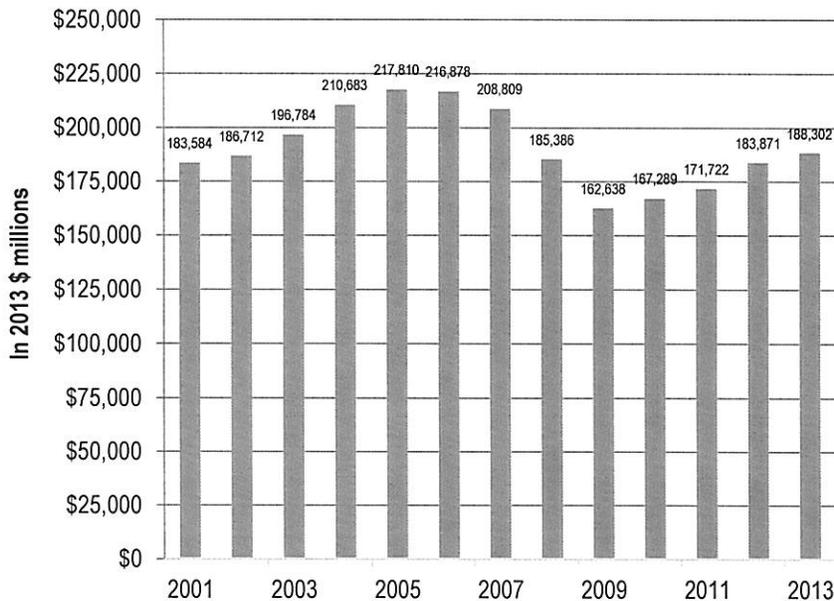
**Regional Median Sales Price for Existing Homes: 2000 - 2014**



Source: MDA Data Quick, 2014

- After reaching its peak in 2007, the median sales price for existing homes in the SCAG region dropped by almost half by 2011 from its 2007 level and rebounded from 2012 to 2014.
- Median home sales price was calculated based on total existing home sales in the SCAG region.

**Regional Real Retail Sales: 2001 - 2013**



Source: California Board of Equalization, 2001-2013

- Retail sales tend to follow closely with trends in personal income, employment rates, and consumer confidence.
- Between 2001 and 2005, real (inflation adjusted) retail sales increased steadily by 19 percent but then dropped between 2005 and 2009 by \$52 billion, or 25 percent.
- In 2013, total real retail sales were three percent higher than the 2001 level.

## **X. Data Sources**

California Department of Education

California Department of Finance, Demographic Research Unit

California Employment Development Department, Labor Market Information Division

California State Board of Equalization

Construction Industry Research Board

InfoGroup

MDA Data Quick

Nielsen Company

U.S. Census Bureau

## **XI. Methodology**

SCAG's Local Profiles utilizes the most up-to-date information from a number of publically available sources, including the U.S. Census Bureau, California Department of Finance, and the California Department of Education. In the event that public information is not available or is not the most recent, SCAG contracts with a number of private entities to obtain regional data. The following sections describe how each data source was compiled to produce the information displayed in this report.

### **Statistical Summary Table**

In the Statistical Summary Table (page 3), the values in the field "Jurisdiction Relative to County/Region" represent the difference between the jurisdiction's value and the county/region value, except for the following categories which represent the jurisdiction's value as a share of the county (or in the case of an entire county as a share of the region): Population, Number of Households, Number of Housing Units, Number of Jobs, Total Jobs Change, and K-12 Student Enrollment.

Median Age, Homeownership Rate, and Median Household Income are based on Nielsen Company data. Number of Housing Units is based on the 2010 Census and estimates from the California Department of Finance. Data for all other categories are referenced throughout the report.

### **Population Section**

Where referenced, data from 2000 to 2014 was taken from the California Department of Finance's (DOF) E-5 estimates, which were published in May 2014. This dataset was benchmarked to population figures from the 2000 and 2010 U.S. Decennial Censuses. Data relating to population by age group and by race/ethnicity was derived from the 2000 and 2010 U.S. Decennial Censuses, and Nielsen Co. The 2000 figure was based on U.S. Decennial Census figures for April 1, 2000 and the 2010 figure was based on U.S. Decennial Census figures for April 1, 2010.

Below are definitions for race and ethnicity, taken from the U.S. Census Bureau.

The Hispanic or Latino origin category is:

- A person of Cuban, Mexican, Puerto Rican, South or Central American, or other Spanish culture or origin, regardless of race.

The race categories are:

- American Indian or Alaska Native – A person having origins in any of the original peoples of North and South America (including Central America), and who maintains tribal affiliation or community attachment.
- Asian – A person having origins in any of the original peoples of the Far East, Southeast Asia, or the Indian subcontinent including, for example, Cambodia, China, India, Japan, Korea, Malaysia, Pakistan, the Philippine Islands, Thailand, and Vietnam.
- Black or African American – A person having origins in any of the black racial groups of Africa, including those who consider themselves to be "Haitian."

- White – A person having origins in any of the original peoples of Europe, North Africa, or the Middle East.
- Some other race – This category includes Native Hawaiian or Other Pacific Islander (a person having origins in any of the original peoples of Hawaii, Guam, Samoa, or other Pacific Islands) and all other responses not included in the "American Indian or Alaska Native," "Asian," "Black or African American," and "White" race categories described above.

Charts for population based on age were tabulated using 2000 and 2010 U.S. Decennial Census data and Nielsen Company data for 2014. Charts for race/ethnicity were tabulated using 2000 and 2010 U.S. Decennial Census data and Nielsen Company data for 2014.

### **Households Section**

The 2000 figure was based on U.S. Decennial Census figures for April 1, 2000 and the 2010 figure was based on U.S. Decennial Census figures for April 1, 2010. Information for 2014 was supplied by the Nielsen Company. Average household size was developed using information from the California Department of Finance (DOF). Households by Size was calculated based on Nielsen Company data. Households refer to the number of occupied housing units.

### **Housing Section**

Housing units are the total number of both vacant and occupied units. Housing units by housing type information was developed using data from the California Department of Finance (DOF). Age of housing stock information is from the Nielsen Company.

The number of residential units with permits issued was obtained using Construction Industry Research Board data, which are collected by counties and are self-reported by individual jurisdictions. It represents both new single family and new multifamily housing units that were permitted to be built, along with building permits that were issued for improvements to existing residential structures (e.g., re-roofs, remodels). Please note that SCAG opted to report the annual number of permits issued by each jurisdiction which may be different than the number of housing units completed or constructed annually. This was done using a single data source which provides consistent data for all jurisdictions. The Construction Industry Research Board defines "multi-family housing" to include duplexes, apartments, and condominiums in structures of more than one living unit.

The median home sales price, compiled from MDA Data Quick, was calculated based on total resales of existing homes in the jurisdiction, including single family units and condominiums. The median price does not reflect the entire universe of housing in the jurisdiction, only those that were sold within the calendar year.

### **Transportation Section**

The journey to work data for the year 2000 was obtained by using the 2000 U.S. Decennial Census Summary File 3. Data for 2010 is based on the 2010 U.S. Census American Community Survey. Information for 2014 was provided by the Nielsen Company.

## Employment Section

Data sources for estimating jurisdiction employment and wage information include the 2010 U.S. Census Bureau Local Employment Dynamics Survey, and information from the California Employment Development Department, InfoGroup, and SCAG for years 2007-2013. In many instances, employment totals from individual businesses were geocoded and aggregated to the jurisdictional level.

Employment information by industry type is defined by the North American Industry Classification System (NAICS). Although the NAICS provides a great level of detail on industry definitions for all types of businesses in North America, for the purposes of this report, this list of industries has been summarized into the following major areas: agriculture, construction, manufacturing, wholesale, retail, information, finance/insurance/real estate, professional/management, education/health, leisure/hospitality, public administration, other services, and non-classified industries.

A brief description of each major industry area is provided below:

- Agriculture – This industry includes crop production, animal production and aquaculture, forestry and logging, fishing hunting and trapping, and support activities for agriculture and forestry.
- Construction – Industries under this umbrella involve the construction of buildings, heavy and civil engineering construction, and specialty trade contractors.
- Manufacturing – This group includes the processing of raw material into products for trade, such as food manufacturing, apparel manufacturing, wood product manufacturing, petroleum and coal products manufacturing, chemical manufacturing, plastics and rubber products manufacturing, nonmetallic mineral product manufacturing and primary metal manufacturing.
- Wholesale – Wholesale industries do business in the trade of raw materials and durable goods.
- Retail – Retail industries engage in the sale of durable goods directly to consumers.
- Information – Businesses in this industry specialize in the distribution of content through a means of sources, including newspaper, periodicals, books, software, motion pictures, sound recording, radio and television broadcasting, cable or subscription programming, telecommunications, data processing/hosting, and other information mediums.
- Finance/Insurance/Real Estate – This sector includes businesses associated with banking, consumer lending, credit intermediation, securities brokerage, commodities exchanges, health/life/medical/title/property/casualty insurance agencies and brokerages, and real estate rental/leasing/sales.
- Professional Management – This industry involves businesses that specialize in professional/scientific/technical services, management of companies and enterprises, and administrative and support services. Types of establishments that would fall under this category range from law offices, accounting services, architectural/engineering firms, specialized design services, computer systems design and related services, management consulting firms, scientific research and

development services, advertising firms, office administrative services, facilities support services, among many others.

- Education/Health – Organizations include elementary and secondary schools, junior colleges, universities, professional schools, technical and trade schools, medical offices, dental offices, outpatient care centers, medical and diagnostic laboratories, hospitals, nursing and residential care facilities, social assistance services, emergency relief services, vocational rehabilitation services, and child day care services.
- Leisure/Hospitality – These industries include organizations in the performing arts, spectator sports, museums, amusement/recreation industries, traveler accommodations, and food and drink services.
- Public Administration – This classification includes public sector organizations, including legislative bodies, public finance institutions, executive and legislative offices, courts, police protection, parole offices, fire protection, correctional institutions, administration of governmental programs, space research and technology, and national security.
- Other Services – Groups in this group include, for example, automotive repair and maintenance, personal and household goods repair and maintenance, personal laundry services, dry-cleaning and laundry services, religious services, social advocacy organizations, professional organizations, and private households
- Non-Classified – Non-classified organizations involve work activities that are not included in the North American Industry Classification System.

### **Retail Sales Section**

Retail sales data is obtained from the California Board of Equalization, which does not publish individual point-of-sale data. All data is adjusted for inflation.

### **Education Section**

Student enrollment data is based on public school campuses that are located within each jurisdiction's respective boundary. Enrollment numbers by grade within a given jurisdiction are tabulated based upon data obtained from the California Department of Education. Enrollment year is based on the end date of the school year; for example, enrollment data for the year 2000 refers to the 1999-2000 school year. City boundaries used in the dataset for all years is based on data provided by the Local Agency Formation Commission for each county in the region.

### **Regional Highlights**

Information for this section was developed through data from MDA Data Quick and the California Board of Equalization.

## **Data Sources Section**

In choosing the data sources used for this report, the following factors were considered:

- Availability for all jurisdictions in the SCAG region,
- The most recognized source on the subject,
- Data sources within the public domain, and
- Data available on an annual basis.

The same data sources are used for all Local Profiles (except where noted) to maintain overall reporting consistency. The jurisdictions are not constrained from using other data sources for their planning activities.

The preparation of this report has been financed in part through grants from the Federal Highway Administration and Federal Transit Administration, U.S. Department of Transportation, under the Metropolitan Planning Program, Section 104(f) of Title 23, U.S. Code. The contents of this report do not necessarily reflect the official views or policy of the U.S. Department of Transportation. Additional assistance was provided by the California Department of Transportation.

## **XII. Acknowledgments**

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Rich Macias, Director, Transportation Planning  
Darin Chidsey, Director, Strategy, Policy & Public Affairs  
Basil Panas, Chief Financial Officer  
Catherine Kirschbaum, Chief Information Officer

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Alex Yu, Manager, Application Development

### **Reproduction**

Pat Camacho, Office Services Specialist

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Jeff Liu, Manager, Media and Public Affairs  
Jonathan Nadler, Manager (former), Compliance & Performance Monitoring  
Frank Wen, Manager, Research & Analysis  
Ying Zhou, Program Manager II

**Southern California Association of Governments  
Regional Council Roster  
May 2015**

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<b>Imm. Past President</b>	<b>4. Hon. Greg Pettis</b>	<b><i>Cathedral City</i></b>	<b>District 2</b>
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	31. Hon. Kris Murray	<i>Anaheim</i>	District 19
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	37. Hon. Sean Ashton	<i>Downey</i>	District 25
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**Southern California Association of Governments  
Regional Council Roster  
May 2015**

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58. Hon. Glen Becerra	<i>Simi Valley</i>	District 46
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65. Hon. Felipe Fuentes	<i>Los Angeles</i>	District 54
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68. Hon. Herb J. Wesson, Jr.	<i>Los Angeles</i>	District 57
69. Hon. Mike Bonin	<i>Los Angeles</i>	District 58
70. Hon. Mitchell Englander	<i>Los Angeles</i>	District 59
71. Hon. Mitch O'Farrell	<i>Los Angeles</i>	District 60
72. Hon. José Huizar	<i>Los Angeles</i>	District 61
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79. Hon. Rusty Bailey	<i>Riverside</i>	District 68
80. Hon. Julio Rodriguez	<i>Perris</i>	District 69

**Southern California Association of Governments  
Regional Council Roster  
May 2015**

81. Hon. Ross Chun	<i>Aliso Viejo</i>	TCA
82. Hon. Andrew Masiel, Sr.	<i>Pechanga Band of Luiseño Indians</i>	Tribal Government Representative
83. Mr. Randall Lewis	<i>Lewis Group of Companies</i>	(Ex-Officio)
84. Hon. Eric Garcetti	<i>Los Angeles</i>	(At-Large)

**Notes:**



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# Administrative Report

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**Planning Commission Hearing Date: December 17, 2015**

**AGENDA ITEM: NEW BUSINESS DISCUSSION ITEM # 9**

**SUBJECT: OVERVIEW AND DISCUSSION OF MIXED USE ZONING DISTRICTS AND DEVELOPMENT REGULATIONS/STANDARDS**

**DEPARTMENT'S RECOMMENDATION:**

1. Receive and file the report.
2. Provide direction as determined.

**EXECUTIVE SUMMARY**

The Mayor and City Council have adopted a Strategic Plan Objective directing the Community Development Department to investigate and report on the existing Mixed Use policies and development regulations/standards within the City's "General Plan" and "Zoning Ordinance".

The purpose of this investigation is to examine the City's existing regulations/standards and to determine whether the need exists to make changes/modify these requirements.

This report provides the Planning Commission with an overview of the City's existing Mixed Use policies and development regulations/standards in advance of the January 21, 2016 Planning Commission public hearing on this matter.

Specifically, this report aims to better frame and support this discussion by presenting answers to the following questions:

- What is Mixed Use?
- What are the various types of Mixed Use?
  - Some examples (renderings and a summary of development standards) of Mixed Use developments from other jurisdictions are attached to this report.
- Why do we have Mixed Use in Redondo Beach?
  - Where is it allowed?
  - What is the rationale for these locations?
- What are the existing policies and standards for Mixed Use development?
  - The existing Mixed Use Goals/Objectives/Policies contained within the General Plan Land Use Element are attached to this report.
  - Noncoastal and Coastal MU Mixed-Use regulations of the City's Zoning Ordinance are attached to this report.
- When were the Mixed Use development regulations/standards amended?
  - What were the nature of the changes made and why?

- Have any concerns with specific Mixed Use development regulations/standards been raised by the public, elected, and/or appointed officials since the last amendments to the MU development regulations/standards?

The goal of this meeting is to solicit input from the Planning Commission on any specific changes/modifications to existing Mixed Use General Plan Land Use Element policies and/or Zoning Ordinance development regulations/standards and bring back specific amendments, if any at the January 21, 2016.

## **BACKGROUND**

### **What is Mixed Use Development?**

Mixed Use is a development format where a variety of land uses are strategically located in an urban, suburban or village development or a single building. Most typically the combination of uses is residential with commercial, cultural or institutional use.

Mixed use is not a new type of development. The origins of Mixed Use predate zoning controls and suburbanization. Before the advent of public transportation and mass acceptance (and dependence) on the automobile, most cities were compact and walkable by necessity. Goods, services and employment all needed to be located within a reasonable distance from residences.

In larger cities the growth of industrial and manufacturing uses resulted in the need to separate residential use from the impacts of these activities. The National Zoning Enabling Act of 1923 set in place the framework to legislate a “separation of uses”. Many cities adopted Euclidean or Single-use zoning in the years that followed. Following the end of WWII there was massive growth in suburbanization and a resultant boom in tract home development. This era saw vast areas of land consumed for low density single family housing. At this same time commercial interests realized that customers and employees were fleeing traditional downtowns and the concept of a commercial strip mall was born. Downtowns suffered from the exodus of businesses and residents and the vitality of the urban core was eroded.

The land use history and zoning pattern of the City of Redondo Beach closely models the scenario described in the paragraph above. Redondo Beach’s early origins were as a port city with railroad, lumber and other core industrial uses. The City’s Downtown was a vibrant mixed use village with shops, services and residences all centered on the Harbor in a 2-3 story format. The city was compact, walkable and exhibited all of the characteristics of a classic Mixed Use environment. The city was also fortunate to have excellent transportation for residents and visitors to connect to virtually all locations throughout Los Angeles.



The growth of the City's suburbs through annexation and conversion of rural lands to housing tracts occurred relatively rapidly. By the late 1960's and early 1970's the City's downtown was in decline and new tracts of homes largely separated from the shops and services they required was the dominant urban form.

The decline of the City's Downtown and growth of the City's suburbs resulted in today's land use pattern where with few exceptions, residents are separated from the basic needs of daily living.

### What are the various types of Mixed Use Development?

Mixed Use development can take many forms. There is no one particular prototype. The following are some broad examples of Mixed Use:

- **Village Cluster**-horizontal or vertical Mixed Use prototype where shops and services are located central to surrounding higher density residential. In the horizontal example, residences are not located above commercial uses. In the vertical example, residences may be located above shops and services
- **Neighborhood commercial zoning** – convenience goods and service stores permitted in otherwise strictly residential areas
- **Main Street residential/commercial** – two to three-story buildings with residential units above and commercial units on the ground floor facing the street
- **Urban residential/commercial** – multi-story residential buildings with commercial and civic uses on ground floor
- **Office convenience** – office buildings with small retail and service uses oriented to the office workers
- **Office/residential** – multi-family residential units within office building(s)
- **Shopping mall conversion** – residential and/or office units added (adjacent) to an existing standalone shopping mall
- **Retail district retrofit** – retrofitting of a suburban retail area to a more village-like appearance and mix of uses
- **Live/work** – residents can operate small businesses on the ground floor of the building where they live
- **Studio/light industrial** – residents may operate studios or small workshops in the building where they live
- **Hotel/residence** – mix hotel space and high-end multi-family residential
- **Parking structure with ground-floor retail**
- **Single-family detached home district in proximity to a shopping center**

Attached to this report is a table that compares the City's current Mixed Use development regulations/standards to numerous other jurisdictions. Included in the attachment with the comparative table are some renderings that also provide examples of Mixed Use development common in other regions of California.

After carefully reviewing the mixed use standards from several other jurisdictions, it becomes evident that our standards are fairly consistent with those throughout California.

In addition to the development standards summary table, two (2) design guideline documents from the City of Santa Barbara are also attached to this report. They are included to illustrate how another jurisdiction goes beyond traditional development standards to improve the design solution for mixed use projects.

One option that the Planning Commission may consider is that the existing General Plan policies and Zoning development regulations/standards are adequate in their scope and details but lacking in specific quantifiable design standards and guidelines

### **Why do we have Mixed Use Development?**

Mixed Use zoning was reintroduced into the City's Zoning Ordinance and General Plan in the comprehensive update that was performed in 1992. There were four primary goals that were intended to be advanced through Mixed Use zoning:

1. Provide an incentive to revitalize under-performing commercial uses at key nodes or locations in close proximity to shops and services and well-served by transit.
2. Allow for new and continued exclusive commercial use of a property while providing the opportunity to include residential use.
3. Prevent up zoning impacts to established low density residential (R-1, R-2 and R-3) neighborhoods while still providing required housing development capacity.
4. Encourage transportation and resource efficient sustainable infill development that provides a transition from commercial districts to established residential neighborhoods.

Other considerations are as follows:

- Providing a greater housing variety and density, more affordable housing (smaller units), life-cycle housing (starter homes to larger homes to senior housing)
- Reducing distances between housing, workplaces, retail businesses, and other amenities and destinations
- Improving access to fresh, healthy foods (as food retail and farmers markets can be accessed on foot/bike or by transit)
- Allowing more compact development, land-use synergy (e.g. residents provide customers for retail which provide amenities for residents) and more utilization for longer hours (daytime and night time use) of commercial districts in service/proximity to the immediately adjacent residential uses
- Promoting stronger neighborhood and commercial district character, sense of place

- Developing walkable, bike-able districts and neighborhoods to improve Public Health and increase accessibility and linkage to transit resulting in decreased Vehicle Miles Traveled (VMT), lower emissions per capita and a smaller carbon footprint
- Providing an appropriate buffer and transition from higher intensity commercial uses to lower intensity residential neighborhoods.

What are some challenges associated with Mixed Use development?

- Mixed Use requires higher density. Single use developments are common at high, medium and low urban density. Low density Mixed Use developments are rare.
- Mixed Use development is much more costly to construct than single use development. The primary cost factor is the code-separation requirements. In addition, costs associated with meeting parking requirements and in turn constructing parking structures to meet the parking as required by all uses is also a significant barrier. Many argue that mixed use by location and design should require far fewer parking spaces than mandated by many codes as residents living in proximity to the immediately adjacent commercial uses anticipated within mixed use districts will not drive their automobiles to adjacent commercial establishments but rather walk or utilize other forms of transportation
- There is a perception that the transition between Mixed Use zones and lower density neighborhoods is not adequate

**Why do we have Mixed Use zoning in Redondo Beach and where is Mixed Use allowed?**

As previously discussed in this report, Mixed Use zoning was enacted for two (2) primary reasons:

1. To protect established low density neighborhoods from up zoning that may otherwise be required to provide adequate housing capacity pursuant to State housing laws; and
2. To encourage revitalization of blighted and underperforming sites.

The locations selected for Mixed Use development were carefully studied on a lot by lot basis for Mixed Use suitability. There are four primary areas, and two minor subareas of the community that are zoned for Mixed Use.

**Primary Areas**

1. **Zone MU-3A-** PCH Corridor south of Palos Verdes Boulevard
2. **Zone MU-3-** PCH and Torrance Boulevard from Garnet to Pearl Street
3. **Zone MU-1-** Artesia Boulevard between Aviation Way and Blossom Lane
4. **Zone CR-** The South Bay Galleria

**Subareas**

1. **Zone MU-3C-** 200 block of Avenue I in the Riviera Village
2. **Zone MU-2-** Salvation Army site

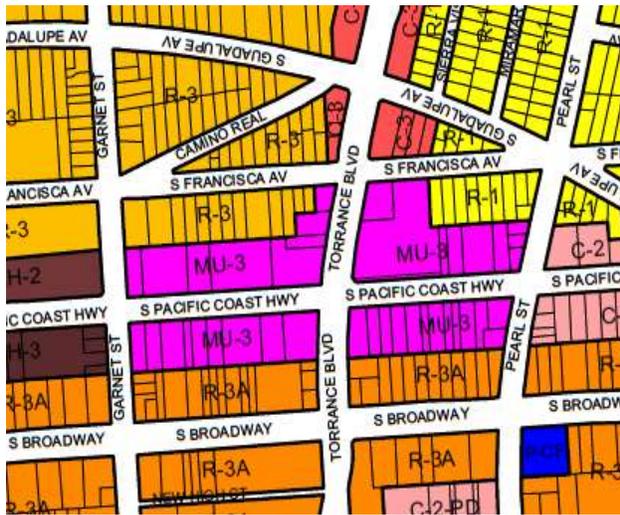


# Administrative Report

Overview and Discussion of Mixed-Use Zoning  
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December 17, 2015

## Zone MU-3- PCH and Torrance Boulevard from Garnet to Pearl Street

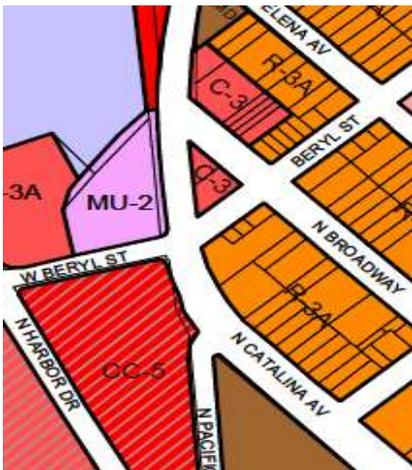


The illustration above shows the Torrance Boulevard Mixed Use node. The Mixed Use zoning is intended to encourage replacement of older low-performing residential uses and older mixed use development at a location that is well served by transit and is close to shops and services.

The photos below show examples of older mixed use development at the Torrance Boulevard and PCH node.



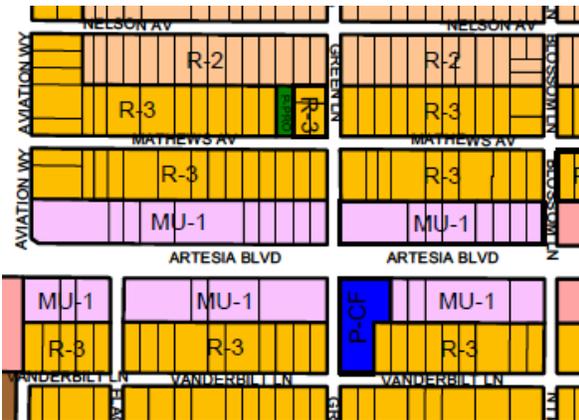
## Zone MU-2 Salvation Army site



This map shows the Salvation Army site. This is the only property that is zoned MU-2 and the property is developed with a senior housing and care facility.

**Zone MU-1- Artesia Boulevard between Aviation Way and Blossom Lane**

This illustration shows the 2-block sub-area on Artesia Boulevard that is zoned for mixed use. Unique to this Mixed Use district is the fact that an exclusively residential project without commercial use can be developed if the entire block is part of the project. The area is characterized by older shops and services, although there has been some reinvestment with new Mixed Use senior housing and a new library facility.



The mixed use senior project shown above is significantly higher than allowed by the base zoning and contains an additional 4<sup>th</sup> story. In this instance the height and story exceptions relate to senior housing code provisions, not the underlying mixed use standards. In staff's opinion the result is a project of questionable scale and compatibility.

**Zone CR- The South Bay Galleria**



The South Bay Galleria and surrounding properties are a unique Mixed Use district designated CR (Commercial Regional). This site provides most of the City's required RHNA allocation and extremely well served by 5 transit lines and a future light rail extension. The area has recently revitalized to include shops and services necessary to support potential vertical or horizontal Mixed Use development. Two full-service grocery stores are located within the immediate proximity and a new Regional Transit Center will begin construction in spring 2016.

**What are the existing policies and standards for Mixed Use development?**

Below are the guiding principles concerning Mixed Use development within the City's current General Plan Land Use Element that provide specific policies targeting the Cities three (3) primary Mixed Use areas. There are numerous specific "Goals, Objectives, and Policies" in addition to the policy "themes" provided below and those are included in an attached document entitled, "Existing General Plan Goals-Objectives-Policies for Mixed Use Development".

Following this discussion on the existing "General Plan Policies" governing Mixed Use Development is a summary of the existing development standards within the Noncoastal and Coastal Zoning Ordinance that prescribe and "implement" the specific development regulations currently applicable to Mixed Use development within the City of Redondo Beach.

**Existing General Plan Mixed Use Development Policies-Guiding Principles**

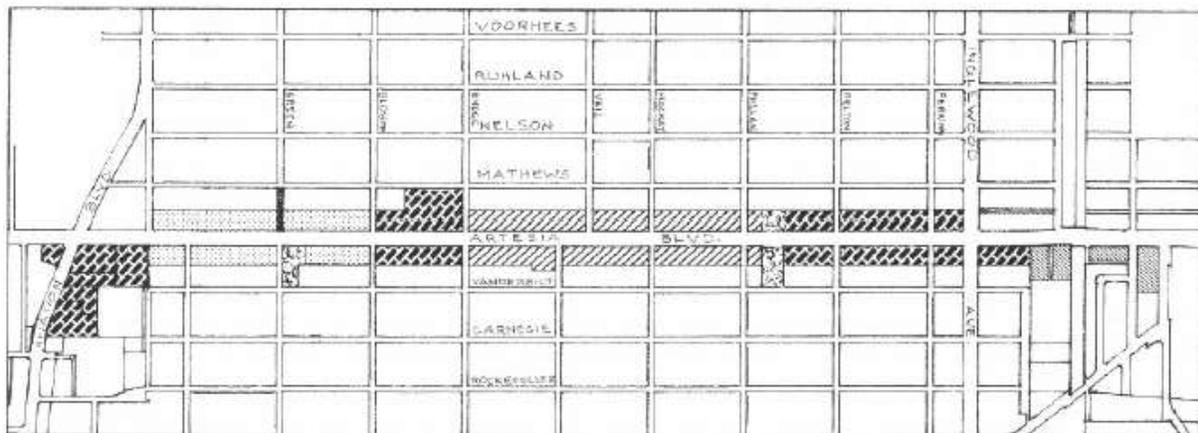
- ***Artesia Boulevard Corridor (Sub-Area 3): Mixed-Use Corridor-Blossom to West of Flagler Lane***
  - This is one of several areas within the City that has been designated for "mixed use." The mixed use designation permits commercial development by itself (and is therefore a commercial designation), but also permits the option of constructing residential units on the upper floors of a development with commercial uses on the ground floor. To complement the incorporated residential units, an emphasis is placed on a "pedestrian-oriented" character of the commercial component as described under the preceding sub-area.
  - The concept of mixing commercial and residential uses has been gaining in popularity in many cities. Traditional planning practice has dictated that residential uses should be physically separated and buffered from other types of "conflicting" uses. More recent experience, however, has shown that when properly planned and designed, mixed use developments can create a unique and positive environment for residents and businesses alike.
  - In mixed use developments, residential units are located and designed to provide sufficient privacy and security, while commercial uses are located and designed to provide easy accessibility and good visibility to the public.
  - While separated in this manner, the two types of uses also enjoy the benefits of their mutual proximity. For residents, they have the convenience and added dimension of having desirable retail businesses within a short walk. For businesses, they can draw vitality from having a "round-the-clock" source of patronage. This adds a type of "energy" to a development that would not exist if it were strictly commercial.
  - There are also several other potential benefits of mixed use development. These include (1) enhancing the opportunities for redevelopment of an area that may be currently lacking in vitality; (2) introducing a new and interesting form of development into the city; (3) increasing affordable housing

opportunities and providing an alternative type of housing; and (4) helping to curb traffic congestion by decreasing the need for automobile trips.

- This segment of Artesia Boulevard was designated for mixed use primarily because it is in substantial need of revitalization, and mixed use is viewed as a viable means of achieving this. Within this area only ("MU-1"), an option has also been provided for strictly residential development, provided that the entire side of a block is developed for this use. This is intended to provide yet another option for the revitalization of this area.
- **Objective 1.18:** Provide for the development of local-serving pedestrian-oriented commercial uses and integration of multi-family residential on the upper floors or in intervening clusters along the corridor, provided that they are compatible with adjacent commercial uses.

**Artesia Boulevard**

-  C-2 ( Sub-Area 4 )
-  C-2-PD ( Sub-Area 2 )
-  C-4 ( Sub-Area 1 )
-  MU-1 ( Sub-Area 3 )
-  P



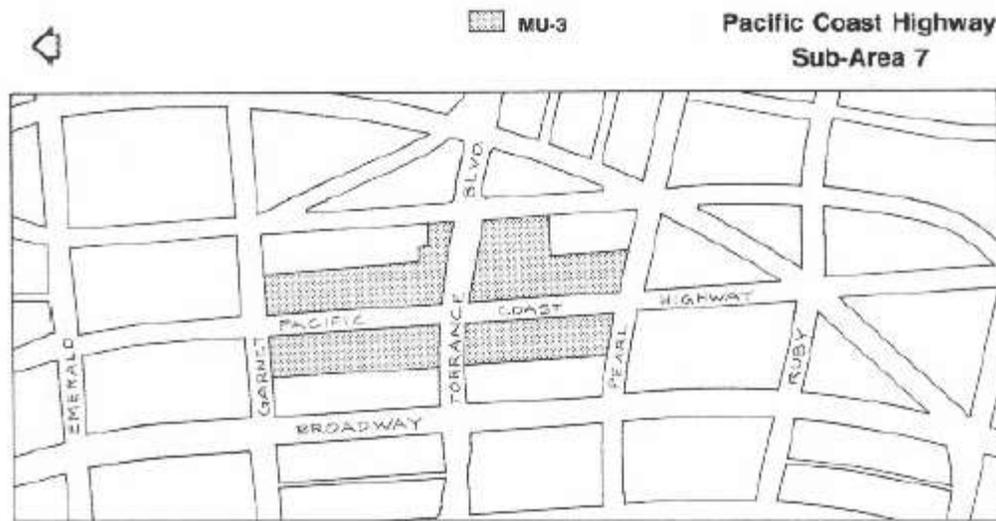
- **Pacific Coast Highway Corridor (Sub-Area 1): Mixed-Use Node-Palos Verdes Boulevard and South**
  - This area was designated for mixed use development ("MU-3") primarily because of its physical suitability for development of this scale. In particular, this area features lot depths in excess of 300 feet and is adjoined to the rear by high density apartment complexes situated at a higher elevation. Because of these factors, this area is more capable of supporting larger scale, higher intensity development without creating undue impacts. This fairly large area also provides a significant opportunity for the production of new affordable multiple-family housing.

- The standards for this area include a few differences from other mixed use areas. These are (1) residential units can be developed on the ground floor of buildings located behind buildings with ground floor commercial uses; and (2) the minimum commercial floor area ratio of 0.3 applies only the first 130 feet of property depth. These standards were instituted since it was not felt to be economically or physically practicable to extend commercial uses entirely to the rear of these relatively deep sites.
- **Objective 1.21:** Provide for the development of community-serving retail and office commercial and mixed-use projects integrating residential with commercial uses southeast of Palos Verdes Boulevard as a primary activity center of the City.



- **Pacific Coast Highway Corridor (Sub-Area 7): Mixed-Use Node-Torrance Boulevard Intersection**
  - Pacific Coast Highway and Torrance Boulevard is the most prominent and highly trafficked intersection in South Redondo. Taking advantage of this, this area was designated for mixed use ("MU-3") to make this location into a focal point of activity within South Redondo. Special attention will be given to encourage the type of design and uses that will make the area distinctive in terms of both appearance and activity.
  - Reference should also be made to the Harbor/Civic Center Specific Plan, Pacific Coast Highway Sub-Area, Zone 6, which establishes additional standards and policies for this area.

- **Objective 1.27:** Provide for the development of a higher intensity pedestrian-oriented activity node containing community-oriented commercial uses and/or mixed-use development projects, integrating residential with commercial uses, as a primary activity area of the City.



**Existing Mixed Use development standards within the Noncoastal and Coastal Zoning Ordinance**

Below is a table that summarizes the specific development standards prescribed for Mixed Use development. Attached to this report are the current Noncoastal and Coastal Zoning regulations governing Mixed Use development in their entirety.

Development standards: MU-1, MU-2, MU-3, MU-3A, MU-3B, and MU-3 Zones.
<p><b>Floor area ratio.</b></p> <ul style="list-style-type: none"> <li>• <b>Commercial uses*.</b> For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 0.5 (<b>MU-1 Zone</b>) and 0.7 (<b>MU-2 Zone</b>) and 1.0 (<b>MU-3, MU-3A, MU-3B, and MU-3C Zones</b>).</li> <li>• <b>Mixed-use.</b> For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply: <ul style="list-style-type: none"> <li>○ <b>Maximum commercial floor area.</b> All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.</li> <li>○ <b>Minimum commercial floor area*.</b> The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3 (<b>Additionally for MU-3A Zone Only, this standard is only applied to the lot area within 130 feet of the property line abutting Pacific Coast Highway.</b>)</li> </ul> </li> </ul>

<p><b>Residential density.</b></p> <ul style="list-style-type: none"> <li>• The maximum number of dwelling units on a lot shall be no more than one unit for each 1,245 square feet of lot area.</li> </ul>
<p><b>Minimum lot size, mixed-use projects.</b></p> <ul style="list-style-type: none"> <li>• 15,000 square feet of lot area.</li> </ul>
<p><b>Building height.</b></p> <ul style="list-style-type: none"> <li>• <b>Commercial uses*</b>. For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet. <b>(Not Applicable to MU-3C Zone Only)</b></li> <li>• <b>Mixed-use</b>. For projects including both commercial and residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.</li> <li>• <b>Mixed-use*</b>. For projects including both commercial and residential uses, no building or structure shall exceed a height of forty-five (45) feet. <b>(MU-2 Zone Only)</b></li> <li>• <b>Residential uses*</b>. For projects containing only residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review. <b>[MU-1 Zone Only (Artesia Boulevard). Residential Only Not Permitted in MU-3, MU-3A, MU-3B, and MU-3C Zones]</b></li> <li>• <b>Residential uses*</b>. For projects containing only residential uses, no building or structure shall exceed a height of forty-five (45) feet. <b>(MU-2 Zone Only)</b></li> </ul>
<p><b>Stories.</b></p> <ul style="list-style-type: none"> <li>• <b>Commercial uses*</b>. For projects containing only commercial uses, no building shall exceed two (2) stories. <b>(Not Applicable to MU-3C Zone Only)</b></li> <li>• <b>Mixed-use</b>. For projects including both commercial and residential uses, no building shall exceed three (3) stories.</li> <li>• <b>Residential uses*</b>. For projects containing only residential uses, no building shall exceed three (3) stories. <b>[MU-1 and MU-2 Zone Only. "Residential Only" Not Permitted in MU-3, MU-3A, MU-3B, and MU-3C Zones.]</b></li> </ul>
<p><b>Setbacks.</b> The minimum setback requirements shall be as follows:</p> <ul style="list-style-type: none"> <li>• <b>Front setback.</b> <ul style="list-style-type: none"> <li>○ <b>Minimum required*</b>. There shall be a minimum front setback average of five (5) feet <b>(MU-1 Zone)</b> and fifteen (15) feet <b>(MU-2 Zone)</b> ten (10) feet <b>(MU-3, MU-3A, and MU-3B Zones)</b> and three (3) feet <b>(MU-3C)</b></li> </ul> </li> </ul>

**Zone**), but at no point less than three (3) feet **(MU-1 Zone Only)** the full width of the lot, except as follows (see setback averaging in Section 10-2.1520):

- Display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade.
- Where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot. **(Not Applicable to MU-2 Zone)**
- Unenclosed pedestrian arcades, outdoor dining areas, and similar unenclosed features contributing to a pedestrian-oriented environment may project seven (7) feet into the required setback. **(MU-3A, MU-3B Zones Only)**
- **Maximum permitted\* (MU-2, MU-3, MU-3A, and MU-3B Zones. Not an MU-1 Zoning Development Standard).** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.
  - The front setback shall not exceed ten (10) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking. **(MU-3C Zone Only)**
- **Side setback.**
  - **Minimum required.** There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.
  - No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:
    - There shall be a minimum side setback of twenty (20) feet the full length of the lot;
    - The required side setback may be modified pursuant to Planning Commission Design Review.
  - **Minimum required.** There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot. No side setback shall be required along the interior lot lines. **(MU-2 Zone Only)**
- **Rear setback.**
  - No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

- There shall be a minimum rear setback of twenty (20) feet the full width of the lot; and
- The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).
- There shall be a rear setback of not less than ten (10) feet the full length of the lot. **(MU-2 Zone Only)**
- **Second story setback\*.**
  - The second story shall have a minimum setback of fifteen (15) feet **(MU-1 Zone)** and eighteen (18) feet **(MU-3, MU-3A, and MU-3B Zones)** and twenty-five (25) feet **(MU-2 Zone)** from any property line abutting a street.
- **Second story setback for residential uses\*.** All residential uses on the second floor shall be set back from the first floor building elevation facing the street, pursuant to Planning Commission Design Review (Section 10-2.2502), in order to provide appropriate separation from activity in the public right-of-way. **(MU-3C Zone Only)**
- **Third story setback.**
  - Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

**Outdoor living space.**

- Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

**Usable public open space.**

- Spaces such as public plazas, public walkways and other public spaces of at least ten (10%) percent of the F.A.R. shall be provided. **(MU-2 Zone. This Standard is Not Applicable)**
  - Public open space shall be accessible to the public and not be fenced or gated so as to prevent public access.
  - Public open space shall be contiguous to the maximum extent feasible.
  - Areas less than ten (10) feet in width shall not count as public open space.
  - The requirement of ten (10%) percent public open space may be modified by the Planning Commission for projects developed on lots less than 20,000 square feet in size.

**General regulations.** See Article 3 of this chapter.

**Parking regulations.** See Article 5 of this chapter.

**Sign regulations.** See Article 6 of this chapter.

**Landscaping regulations.** See Article 7 of this chapter.

<b>Procedures.</b> See Article 12 of this chapter.
Original Adoption and Amendments: (Ord. 2756 c.s., eff. January 18, 1996, as amended by § 7, Ord. 3076 c.s., eff. July 7, 2011)
<b>Additional land use regulations: MU-1, M-2, MU-3, MU-3A, MU-3B, and MU-3C Zones</b>
<b>Residential uses.</b> <ul style="list-style-type: none"><li>Residential dwelling units may only be located on the second floor and higher of structures developed with commercial uses on the lower levels, with the following exceptions:<ul style="list-style-type: none"><li><b>MU-1 zone.</b> In the MU-1 zone, lots may be developed exclusively for residential use where the entirety of the block frontage is developed exclusively for residential use.</li><li><b>MU-2 zone.</b> In the MU-2 zone lots may be developed exclusively for residential use.</li><li><b>MU-3A zone.</b> In the MU-3A zone, residential dwelling units may be located on any floor in structures located behind street-facing commercial or mixed-use structures, or above parking on the ground floor in structures located behind street-facing commercial or mixed-use structures.</li></ul></li></ul>
<b>Offices (Internal Inconsistency).</b> <ul style="list-style-type: none"><li>Offices may occupy up to a maximum of fifty (50%) percent of the linear frontage of the building in all mixed-use zones, except that such ground floor uses along the street frontage are permitted in the MU-3C zone within the Riviera Village overlay zone.</li><li>MU-2 and MU-3 Zones: Offices are permitted only on the second floor and/or above, or on the ground floor to the rear of other permitted retail or service uses provided that the pedestrian character of the corridor is not disrupted, except that such ground floor uses along the street frontage are permitted in the MU-3C zone within the Riviera Village overlay zone.</li></ul>
<b>Uses exceeding 30,000 square feet.</b> <ul style="list-style-type: none"><li>Uses exceeding 30,000 square feet shall be prohibited except where they are designed to be compatible with the intended pedestrian-oriented character of the zone, pursuant to the requirements for a Conditional Use Permit.</li></ul>
Original Adoption and Amendments: (Ord. 2756 c.s., eff. January 18, 1996, as amended by § 3, Ord. 2818 c.s., eff. May 21, 1998, and § 5, Ord. 3076 c.s., eff. July 7, 2011)

**When were the Mixed Use development regulations/standards amended and for what reason?**General Plan Amendments:

The General Plan originally adopted the majority of the existing “Goals, Objectives, and Policies” (attached to this report) as part of the City’s comprehensive General Plan update in 1992. Since their original adoption in 1992 there have been very few amendments to Mixed Use policies other than some strategic amendments in 2011 that were purposed to strengthen the desire of the City to make accommodations and ensure that any retail and professional office be developed in a “pedestrian-oriented” manner.

Zoning Ordinance Amendments:

Unlike the City’s Mixed Use General Plan “Goals, Objectives, and Policies”, which are largely unchanged since their original adoption, the City, conducted a thorough examination of the City’s existing standards in 2010. The following is a very brief ‘line item list’ of the changes that resulted from the City’s investigations in 2010/2011:

- **Specific Purposes:** Every zoning district in the City’s ordinance begins with a section titled “Specific Purposes”. This section articulates the specific purposes of each unique zoning district. Two (2) new statements of purpose were added to the Mixed Use zones.
  - “The primary character of mixed-use developments should be commercial in nature so as to integrate with and enhance the quality of the surrounding business district.”
  - “Provide high quality, public open spaces equivalent to a minimum 10% of a project’s Floor Area Ratio (FAR).”
- **Hotels:**
  - Hotels, which had been prohibited, are now a conditionally permitted use.
- **Car Wash:**
  - Due to historical development patterns an allowance for this use was added and is now conditionally permitted in the MU-3A and MU-3B Zones, but not within a mixed use project
- **Offices:**
  - The previous development regulations only permitted offices on the ground floor to the rear of other permitted retail or service uses, or on the second floor.
  - Due to high vacancy rates of commercial uses on the ground floor the previous restrictions were changed to allow up to 50% of the ground floor frontage commercial space to be developed with Office.

- **Residential Density:**

- The residential density for all mixed-use zones is currently a maximum of one unit for every 1,245 square feet of lot area or approximately 35 units per acre.
- The residential density for these zones has occasionally been considered too high by the public and the Planning Commission. Primary concerns related to density have been traffic congestion and a lack of parking.
- To address the past concerns (which remain today) it was proposed by the Community Development Department and recommended by the Planning Commission in 2010 to decrease the maximum residential density requirement to one unit for every 1,452 square feet of lot area or 30 units per acre.
  - While the Planning Commission and Staff recommended to the City Council a reduction in residential density to 30 units per acre, it was determined that such a change would trigger the requirement for a general election under Article 27 of the City Charter. The City Council accepted all recommendations of the Planning Commission except the density reduction due to the estimated \$180,000 cost of a public vote.
  - It is important to note that any future recommended change in density would likely trigger the need for an election on the matter.

- **Minimum Lot Size, Mixed-Use Projects:**

- The prior standard stated that no projects containing both commercial and residential uses shall be permitted on lots with less than 15,000 square feet of lot area. At the time that the mixed-use standards were first developed there was an opinion that such projects could only actualize in a large-scale scenario.
- The current standard still requires a 15,000 square foot minimum lot area for mixed-use projects. The Planning Commission may consider recommending that mixed use be allowed on smaller sites. While this would potentially result in smaller scale developments, staff should note that community members have challenged this proposed amendment as potentially triggering a public vote requirement by allowing an increase in the number of potential residential development units. A contrasting opinion would be that the same allowable land area zoned for mixed use would result in the same number of potential units regardless of lot size. Both arguments are worth consideration if the Commission desires to pursue a smaller lot size allowance for mixed use development.

- **Building Height:**

- Previously projects that included both commercial and residential uses were subject to a 45 foot height limit. Height compatibility with surrounding land uses and unobstructed views of scenic vistas are among the main concerns with mixed-use developments, especially those in coastal zones. Therefore, a decrease in the maximum height allowance from the 45 foot height limit to a 38 foot height limit was approved.

- Also approved is the current provision that an exception be granted where a height of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.
- **Usable Public Open Space:**
  - Prompted by concerns regarding the lack of public open space in mixed-use projects, a new standard was approved. The current standard requires mixed-use developments to provide at least 10% of their F.A.R. for Usable Public Open Space defined as public plazas, public walkways and other public spaces. Areas must be at least (10) feet in width to qualify.
- **Visitor Parking Requirements:**
  - The prior visitor parking space requirements for mixed-use dwelling units were the same as for non-mixed-use units. To address the concerns of the public regarding the lack of visitor parking spaces in mixed-use projects the regulations were changed to allow the Planning Commission to require additional visitor parking spaces if determined necessary due to unique characteristics of the project and/or the surrounding neighborhood.

### **The Mixed Use Discussion**

The Planning Department was directed to look at the issue of Mixed-Use development standards to see if they are still appropriate for our community. Do the current development standards act as an impediment for local businesses? Are the current standards allowing higher density development compatible with the neighborhood scale and needs?

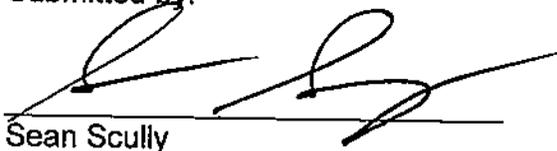
The Mixed-Use development standards are highly contested with regard to allowable scale, residential density, and potential issues with circulation and economic feasibility. Development standards impact how new developments fit in with surrounding uses, affecting residents and local businesses.

Past mixed use projects have yielded mixed results. Although all of the existing mixed use projects meet the required development standards, they have fallen short of the expectations in some areas. Specific issues such as those previously stated – residential density, allowable height – along with additional concerns regarding the allowable F.A.R. for commercial uses, parking, project design and automobile access have been repeatedly debated due to varying concerns regarding potential impacts on surrounding properties.

While no action by the Planning Commission is recommended at this time, staff does recommend that the Planning Commission provide staff with comments on areas of concern and potential areas for further study and examination. Staff will report back with Draft amendments at the January 21, 2016 Planning Commission meeting.

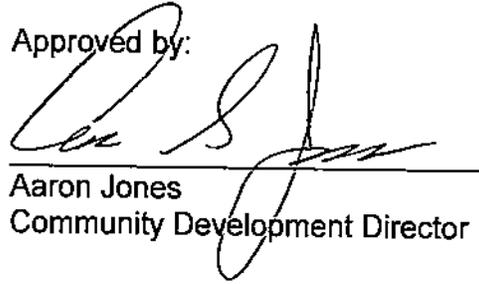
**December 17, 2015**

Submitted by:



Sean Scully  
Planning Manager

Approved by:



Aaron Jones  
Community Development Director

Attachments:

1. Photographic Survey of Existing Mixed Use Development in the City of Redondo Beach;
2. Comparative Table of Mixed Use Development Regulations with Other Jurisdictions;
3. Santa Barbara Design Guidelines;
4. Santa Barbara Measuring Size-Mass-Bulk-Scale
5. Existing General Plan Goals-Objectives-Policies for Mixed Use Development;
6. Current Noncoastal and Coastal Mixed Use Zoning Regulations.

# City of Redondo Beach

## Photographic Survey of Existing Mixed Use Development

Planning Commission

December 17, 2015



Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
321 Diamond Street Historic Property Landmark No. 1	R-3A	12 Residential Units Approximately 5,256 square feet of Residential FAR .92 Approximately 5,256 square feet of Commercial FAR .92 Approximately 5,720 square feet of lot Legal non-conforming





Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
307-315 S. PCH 3 Separate Parcels	MU-3	6 Residential Units Approximately 2,923 square feet of Residential FAR .16 Approximately 3,344 square feet of Commercial FAR .18 Approximately 18,750 square feet of lot (3 lots) Legal non-conforming



Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
705 S. PCH	C-2	4 Residential Units Approximately 3,649 square feet of Residential FAR .49 Approximately 900 square feet of Commercial FAR .12 Approximately 7,470 square feet of lot Legal non-conforming



Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
717-719 S. PCH	C-2	9 Residential Units Approximately 7,100 square feet of Residential FAR .47 Approximately 800 square feet of Commercial FAR .05 Approximately 15,000 square feet of lot Legal non-conforming



Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
1010-1012 S. PCH	C2-PD	<p>According to building card no residential units on site</p> <p>Approximately 3,550 square feet of Commercial FAR .65</p> <p>Approximately 5,470 square feet of lot</p> <p>Legal non-conforming</p>



Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
215 Avenue I	MU-3C Rezoned 1998 from C4-PD	12 Residential Units 883 to 927 square feet per unit 8,853 Commercial Area FAR 1.01 Project Approved 1998



Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
1800 S. Pacific Coast Highway	MU-3A	98 Detached Condominiums 19,980 Commercial Area Project Approved 1998





Location/Address	Zoning	Residential Density: Commercial SF: FAR Ratio: Conforming?
2001 Artesia Blvd.	MU-1	48 Residential Units Senior Housing 6,787 Commercial Area Project Approved 2004



**Comparative Table of Mixed Use Development Regulations with Other Jurisdictions**

<b>DEVELOPMENT STANDARD</b>	<b>REDONDO BEACH</b>	<b>HERMOSA BEACH</b>	<b>SAN CLEMENTE</b>	<b>JEFFERSON SUB-AREA, DWNTOWN SAN DIEGO</b>	<b>MANHATTAN BEACH</b>	<b>IRVINE</b>	<b>ISLA VISTA, SANTA BARBARA</b>
<b>Lot Area:</b>	15,000sf	No standard set	6,000sf	6,000sf	No Standard set	10,000sf	7,000sf min lot width 65 feet
<b>F.A.R : Commercial Only Project</b>	MU-1 0.5 MU-2 1.0 MU-3 1.0	No standard set	2.0	No standard set	1.5	0.65 max site coverage for Non-Residential Uses	No standard set
<b>F.A.R: Mixed-Use Project</b>	1.5	No standard set	0.75, 1.0 with public benefit	No standard set	1.5	No standard set	No standard set
<b>Density: Residential within Mixed Use Project</b>	1 dwelling unit per 1,245sf	No standard set	1 dwelling unit per 1,200sf	1 dwelling unit per 1,750sf	1 dwelling unit per 1,125sf	No standard set	1 dwelling unit per 968sf
<b>Density: Commercial within Mixed Use Project</b>	Min FAR 0.3 Max FAR 0.7	No standard set	No standard set	No standard set	Max FAR 0.7	No standard set	No standard set
<b>Building Height</b>	38ft to 45ft	30ft	No standard set	36ft	30ft	Cannot exceed 200ft above ground level or 200ft above airport elevation, or exceed 500ft or more above ground level at the object site.	40ft
<b>Stories Allowed</b>	3 stories	No standard set	3 stories for lots greater than 12,000sf, otherwise 2 stories	3 stories	3 stories	No standard set	3 stories

**Comparative Table of Mixed Use Development Regulations with Other Jurisdictions**

<b>DEVELOPMENT STANDARD</b>	<b>REDONDO BEACH</b>	<b>HERMOSA BEACH</b>	<b>SAN CLEMENTE</b>	<b>JEFFERSON SUB-AREA, DWNTOWN SAN DIEGO</b>	<b>MANHATTAN BEACH</b>	<b>IRVINE</b>	<b>ISLA VISTA, SANTA BARBARA</b>
<p><b>Permitted Use Restrictions</b></p> <p><b>AND</b></p> <p><b>Rules of Location</b></p>	<p>Office: Ground floor office limited to 50% of linear frontage</p> <p>MU-1: project can be residential only</p> <p>MU-3: residential can be on ground floor but not street facing</p>	<p>Non-Residential uses can only occupy the first floor</p>	<p>Non-Residential uses can only occupy the first floor</p>	<p>Offices, creative arts schools, and lodging cannot occupy ground floor frontage.</p>	<p>50% maximum of commercial space may be occupied by office use</p>	<p>Non-Residential uses can only occupy the first floor</p>	<p>Office allowed above ground floor or setback a minimum of 60ft from the front property line.</p>

**Comparative Table of Mixed Use Development Regulations with Other Jurisdictions**

DEVELOPMENT STANDARD	REDONDO BEACH	HERMOSA BEACH	SAN CLEMENTE	JEFFERSON SUB-AREA, DWNTOWN SAN DIEGO	MANHATTAN BEACH	IRVINE	ISLA VISTA, SANTA BARBARA
<b>Setbacks</b>	<p><b>MU-1, MU-2</b>  <b>Front:</b> 5ft  <b>Side:</b> 10ft, 20ft if contiguous to residential  <b>Rear:</b> No setback except if near residential – 20ft  <b>2<sup>nd</sup> story:</b> 15ft  <b>3<sup>rd</sup> story:</b> 5ft  <b>MU-3, 3A, 3B:</b>  <b>Front:</b> 10ft, 15ft max  <b>Side:</b> 10ft, 20ft when near residential  <b>Rear:</b> No setback except if near residential – 20ft</p>	<p><b>Front:</b> 5ft  <b>Side:</b> 5ft (if near residential zone)  <b>Rear:</b> 5ft (if near residential zone)  <b>2<sup>nd</sup> story:</b> no standard set  <b>3<sup>rd</sup> story:</b> no standard set</p>	<p><b>MU-3</b>  <b>Front:</b> 0ft  <b>Side:</b> 0ft (street side)  <b>Rear:</b> 0ft  <b>2<sup>nd</sup> story:</b> no standard set  <b>3<sup>rd</sup> story:</b> no standard set    <b>MU-5.1</b>  <b>Front:</b> 10ft  <b>Side:</b> 8ft (street side)  <b>Rear:</b> 5ft  <b>2<sup>nd</sup> story:</b> no standard set  <b>3<sup>rd</sup> story:</b> no standard set</p>	<p><b>Front:</b> 10ft  <b>Side:</b> Properties 50ft or less in width requires 3ft for 1<sup>st</sup> story, 6ft for 2<sup>nd</sup>, 9ft for 3<sup>rd</sup>  <b>Rear:</b> 0ft  <b>2<sup>nd</sup> story:</b> no standard set  <b>3<sup>rd</sup> story:</b> no standard set</p>	<p><b>Front:</b> 11ft  <b>Corner Side:</b> 1ft  <b>Rear:</b> 4ft  <b>Interior Side:</b> 5ft  <b>2<sup>nd</sup> story:</b> no standard set  <b>3<sup>rd</sup> story:</b> no standard set    <b>Condo Building</b>  <b>Front:</b> 5ft  <b>Side:</b> 5 ½ft  <b>Rear:</b> 5ft</p>	<p><b>Front:</b> no standard set  <b>Side:</b> 10'- 6"  <b>Rear:</b> Residential uses: 10'- 6" Nonresidential uses: 5ft  <b>2<sup>nd</sup> story:</b> no standard set  <b>3<sup>rd</sup> story:</b> no standard set</p>	<p><b>Front:</b> None, except that lots shall have a BTL.  <b>Side:</b> None, except when adjacent to residential zoning districts minimum is 5ft.  <b>Rear:</b> 10ft, if it abuts a residential zone 20ft  <b>2<sup>nd</sup> story:</b> no standard set  <b>3<sup>rd</sup> story:</b> no standard set</p>

**Comparative Table of Mixed Use Development Regulations with Other Jurisdictions**

<u>DEVELOPMENT STANDARD</u>	REDONDO BEACH	HERMOSA BEACH	SAN CLEMENTE	JEFFERSON SUB-AREA, DWNTOWN SAN DIEGO	MANHATTAN BEACH	IRVINE	ISLA VISTA, SANTA BARBARA
<b>Setbacks (continued)</b>	<p><b>2<sup>nd</sup> story:</b> 18ft</p> <p><b>3<sup>rd</sup> story:</b> 5ft</p> <p><b>MU-3C:</b> <b>Front:</b> 3ft, 10ft max <b>Side:</b> 10ft, near residential 20ft <b>Rear:</b> No setback except if near residential – 20ft</p> <p><b>2<sup>nd</sup> story:</b> PCDR to separate from PROW</p> <p><b>3<sup>rd</sup> story:</b> 5ft</p>						
<b>Outdoor Living Space</b>	200sf per unit	No standard set	No standard set	No standard set	No standard set	15 % landscaping is required for each improved nonresidential building site.	No standard set

**Comparative Table of Mixed Use Development Regulations with Other Jurisdictions**

**SAN CLEMENTE MIXED USE DISTRICT DESIGN SAMPLE:**



**Comparative Table of Mixed Use Development Regulations with Other Jurisdictions**

**RENDERING FROM ISLA VISTA MASTER PLAN IN SANTA BARBARA COUNTY:**



**Comparative Table of Mixed Use Development Regulations with Other Jurisdictions**

**HISTORIC MISSION HILLS NEIGHBORHOOD IN SAN DIEGO COUNTY: MIX OF RESIDENTIAL AND RETAIL:**



**CITY OF SANTA BARBARA URBAN DESIGN GUIDELINES:  
CITY GRID**

**PREPARED BY THE CITY OF SANTA BARBARA PLANNING DIVISION  
DECEMBER, 1999**

**CITY OF SANTA BARBARA URBAN DESIGN GUIDELINES:  
CITY GRID**

**PREPARED BY THE CITY OF SANTA BARBARA PLANNING DIVISION  
ILLUSTRATIONS BY JEFF SHELTON, ARCHITECT**

**ADOPTED BY CITY COUNCIL RESOLUTION 99-138 ON DECEMBER 14, 1999**

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## ACKNOWLEDGMENTS

The following people participated in the development of the Urban Design Guidelines for the City Grid. The City thanks them and values their support.

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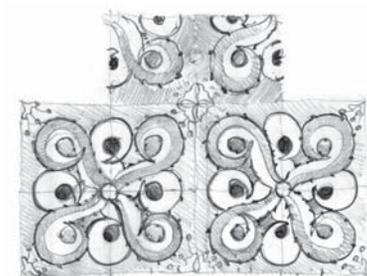
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## GOALS OF THE URBAN DESIGN GUIDELINES

### COMPATIBILITY OF NEW DEVELOPMENT WITH THE EXISTING ENVIRONMENT

- ❖ *Design developments to complement and enhance the character of Santa Barbara, the surrounding neighborhood, and existing adjacent developments, while allowing each development to retain a distinct visual identity.*
- ❖ *Incorporate natural features and landscaped open spaces into developments to provide a sense of openness and continuity, and enhance the environment of the City grid.*
- ❖ *Design developments to respect the arrangement of buildings and open spaces on adjacent sites and provide opportunities for enhanced circulation, solar access, and views.*

### HUMAN SCALE CHARACTER – VISUAL RELATIONSHIP BETWEEN DEVELOPMENT AND PEDESTRIANS

- ❖ *Preserve the human scale character of the grid by using design techniques that reduce the apparent size, bulk, scale, and height of buildings.*
- ❖ *Provide visual interest for pedestrians by incorporating building details that relate to the surrounding built environment at a human scale.*

### THE BUILDING / STREET EDGE – FUNCTIONAL RELATIONSHIP BETWEEN DEVELOPMENT AND PEDESTRIANS

- ❖ *Encourage pedestrian activity on the street through building design. Frequent building entrances, windows at pedestrian height, and outdoor activity spaces create a lively, pedestrian-friendly environment along public streets.*
- ❖ *Create visually unified street spaces by planning the orientation of buildings and building setbacks to enhance the character of the street.*

### PEDESTRIAN FACILITIES AND AMENITIES

- ❖ *Create and maintain a continuous, convenient network of pedestrian facilities throughout the City grid to reduce dependence on the automobile.*
- ❖ *Provide pedestrian amenities, including street furniture, landscaping, lighting, and trash receptacles, to make walking more attractive and convenient.*
- ❖ *Design and locate pedestrian facilities and amenities to promote the uninterrupted flow of pedestrian traffic.*
- ❖ *Create pedestrian links to transit and bicycle facilities to increase the convenience of transit and bicycle travel.*

## COURTYARDS, PLAZAS, AND PLACITAS

- ❖ *Encourage the provision of courtyards, plazas, and placitas throughout the City grid to create activity nodes, provide pedestrians with a more intimate gathering space away from the street, and maintain an inviting environment for pedestrians.*

## TRANSIT STOPS

- ❖ *Design and locate transit stops to promote the increased use of transit, facilitate multi-modal travel, and reduce dependence on the automobile.*
- ❖ *Provide transit stops that are attractive, safe and convenient places in which to wait for a transit vehicle.*

## BICYCLE FACILITIES

- ❖ *Provide bicycle facilities throughout the City grid to make bicycling a more viable and convenient mode of transportation.*
- ❖ *Design and locate bicycle facilities, both on private property and in the public right-of-way, to enhance the City's transportation network and facilitate multi-modal travel.*

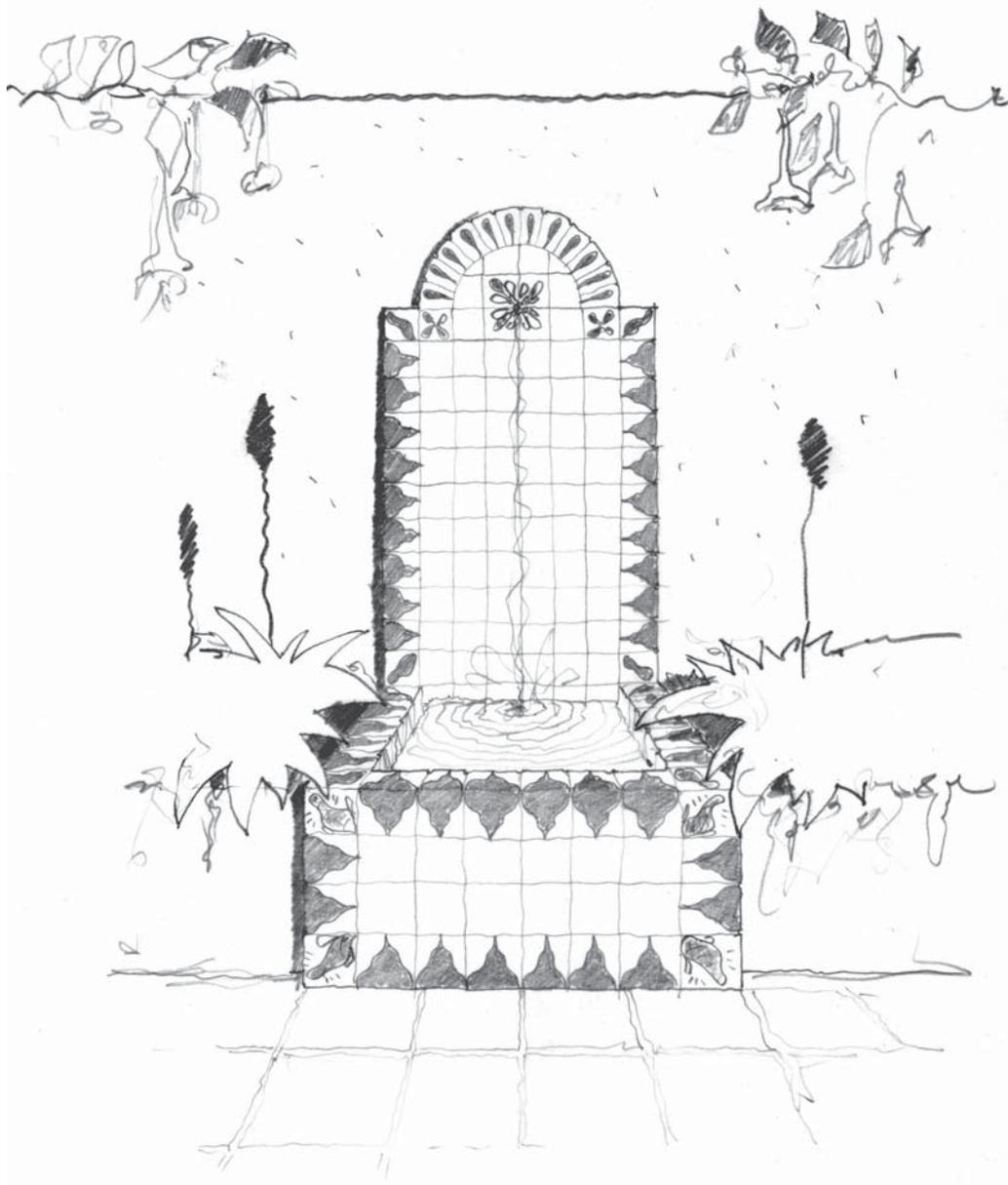
## AUTOMOBILE PARKING FACILITIES

- ❖ *Design automobile parking facilities to reduce the visibility of automobiles and allow features of greater pedestrian interest to dominate the streetscape.*
- ❖ *Design automobile parking facilities to confer priority on the safety and convenience of pedestrians and encourage pedestrian activity on the street.*
- ❖ *Create links between automobile parking facilities and other transportation facilities to increase the convenience of walking, bicycling, and transit.*

## BUILDING EQUIPMENT AND SERVICE AREAS

- ❖ *Design and locate building equipment and service areas so that they do not dominate the appearance of the site or interfere with pedestrian or vehicular circulation.*





**INTRODUCTION**

# INTRODUCTION

## BACKGROUND

Santa Barbara has long been recognized as a leader in community planning and design, and its programs and policies are emulated throughout the United States. Generations of community members have given their energies to preserving and enhancing the natural and scenic beauty of the City, and have subsequently adopted and upheld exceptionally high standards for the City's built environment and landscaped open spaces.

Santa Barbarans recognize that the form of the City's built environment and open spaces profoundly affects the quality of life. In order to maintain and enhance Santa Barbara's desirability as a place to live, work and visit, community planning must focus on the physical and spiritual needs of people, increasing access and creating public spaces that foster community interaction. The purpose of the Urban Design Guidelines is to ensure that new development projects in the City reflect these core community values.

## TRADITIONAL TOWN PLANNING AND URBAN DESIGN GUIDELINES

Traditional planning generally promotes human scale development by encouraging a mix of residential and nonresidential land uses (instead of the separation of land uses) and encouraging projects that are pedestrian, bicycle, and transit friendly (rather than automobile focused). Traditional planning also seeks to balance public and private interests, considering first and foremost the

people that are served by the public realm and how the quality of their lives are affected by a city's public spaces.

One of the tools that cities are beginning to use to implement traditional planning principles are Urban Design Guidelines. Urban Design Guidelines provide direction to decision makers, design professionals, and the public regarding site planning, building, landscaping, and infrastructure design.

## DEVELOPMENT OF THE URBAN DESIGN GUIDELINES FOR SANTA BARBARA

Late in 1996, an ad-hoc subcommittee of the Architectural Board of Review (ABR) drafted recommendations for Urban Design Guidelines centered around the goal that "buildings should be pedestrian friendly in terms of location on site, fenestration, scale, and detail." The ABR subcommittee then met with selected members of the City Council, Planning Commission, Historic Landmarks Commission, and the Planning Division Staff to discuss the possibility of creating Urban Design Guidelines for the City. Staff prepared a compendium of existing City policies and guidelines that supported the design principles envisioned for the new guidelines. It was recognized that, while the compilation of existing policy and text was impressive, there was still a need for a comprehensive, easy to use guide for the design of buildings and infrastructure in the City.

In 1997 and 1998, the update of the Circulation Element of the General Plan and the Downtown/Waterfront Visioning process were completed. Both of these major projects resulted in recommendations relating to the development or revision of design standards for residential, nonresidential, and public infrastructure

projects in the City. These recommendations became the policy foundation on which the Urban Design Guidelines were developed.

The Urban Design Guidelines Subcommittee was first convened in June, 1998. The subcommittee consisted of two members each from the City Council, Planning Commission, Historic Landmarks Commission, and the Architectural Board of Review.

A series of three panel discussions was held in July, 1998. The purpose of the meetings was to gain input from community representatives as to the scope and content of the guidelines. The public panelists were chosen either because of their participation in previous planning efforts, their utilization of design guidelines in a professional capacity, or their varying perspectives on the design and development review processes.

Using the input from these meetings, staff developed draft Urban Design Guidelines for the subcommittee to review. This involved extensive research of the City's existing plans and policies, reviewing comparable guidelines from other jurisdictions, and extensive collaboration with the Public Works Staff.

The Urban Design Guidelines Subcommittee reviewed the staff draft guidelines in a series of 16 meetings held from April through July, 1999. The meetings included the development of illustrations for the guidelines. The fully illustrated Subcommittee Draft Urban Design Guidelines were released for public review in September, 1999.

The Subcommittee Draft Guidelines were then reviewed at an October 13, 1999 joint hearing of the Planning Commission, Architectural Board of Review, and

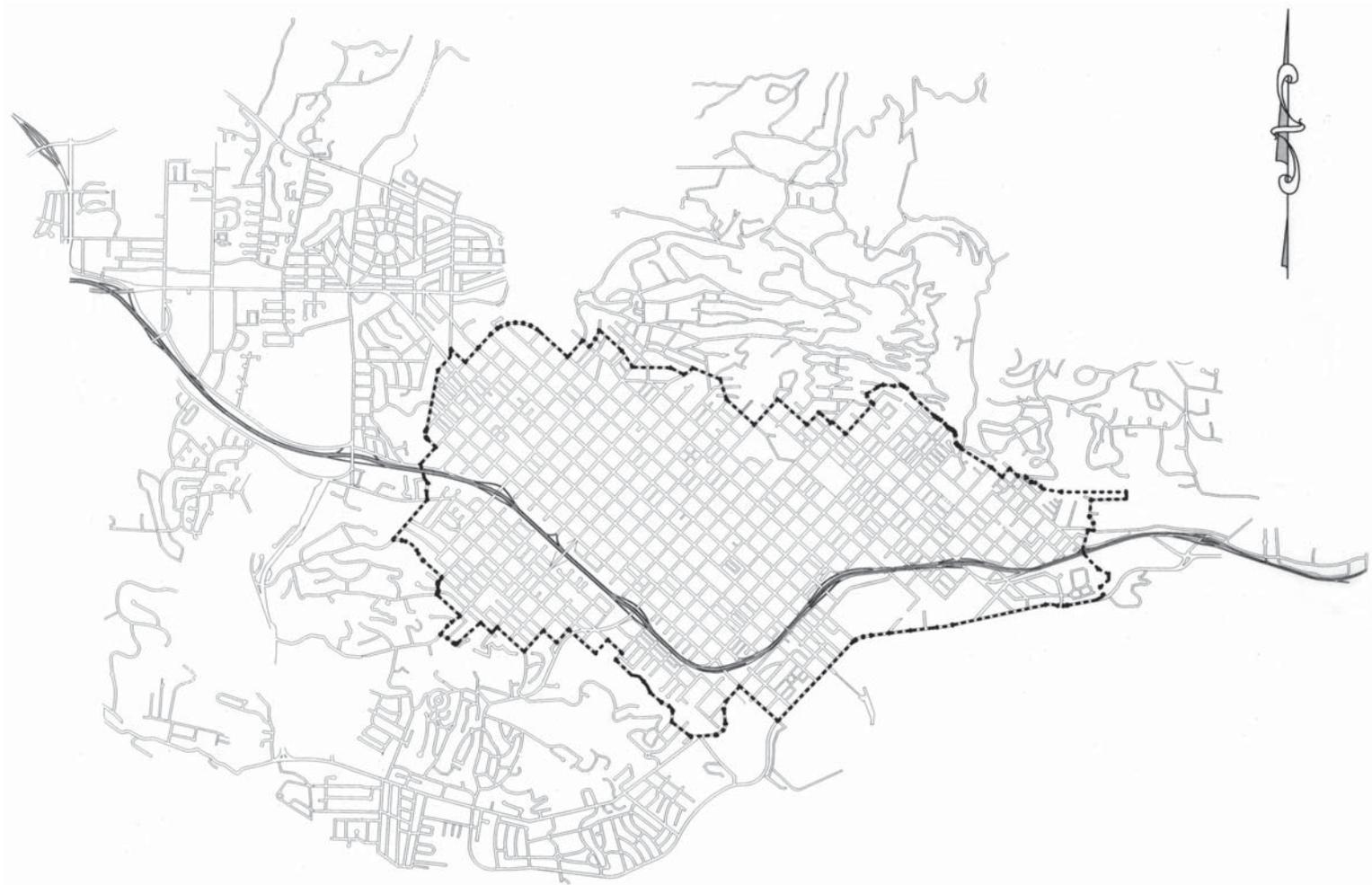
Historic Landmarks Commission. The Planning Commission then held a public hearing on the guidelines on November 4, 1999. At the conclusion of the hearing, the Planning Commission recommended that the City Council adopt the guidelines, incorporating the changes suggested at the October 13 and November 4 hearings.

In November, 1999, the Final Draft Urban Design Guidelines were released for public review. On December 14, 1999, the City Council reviewed the final draft document and adopted the Urban Design Guidelines.

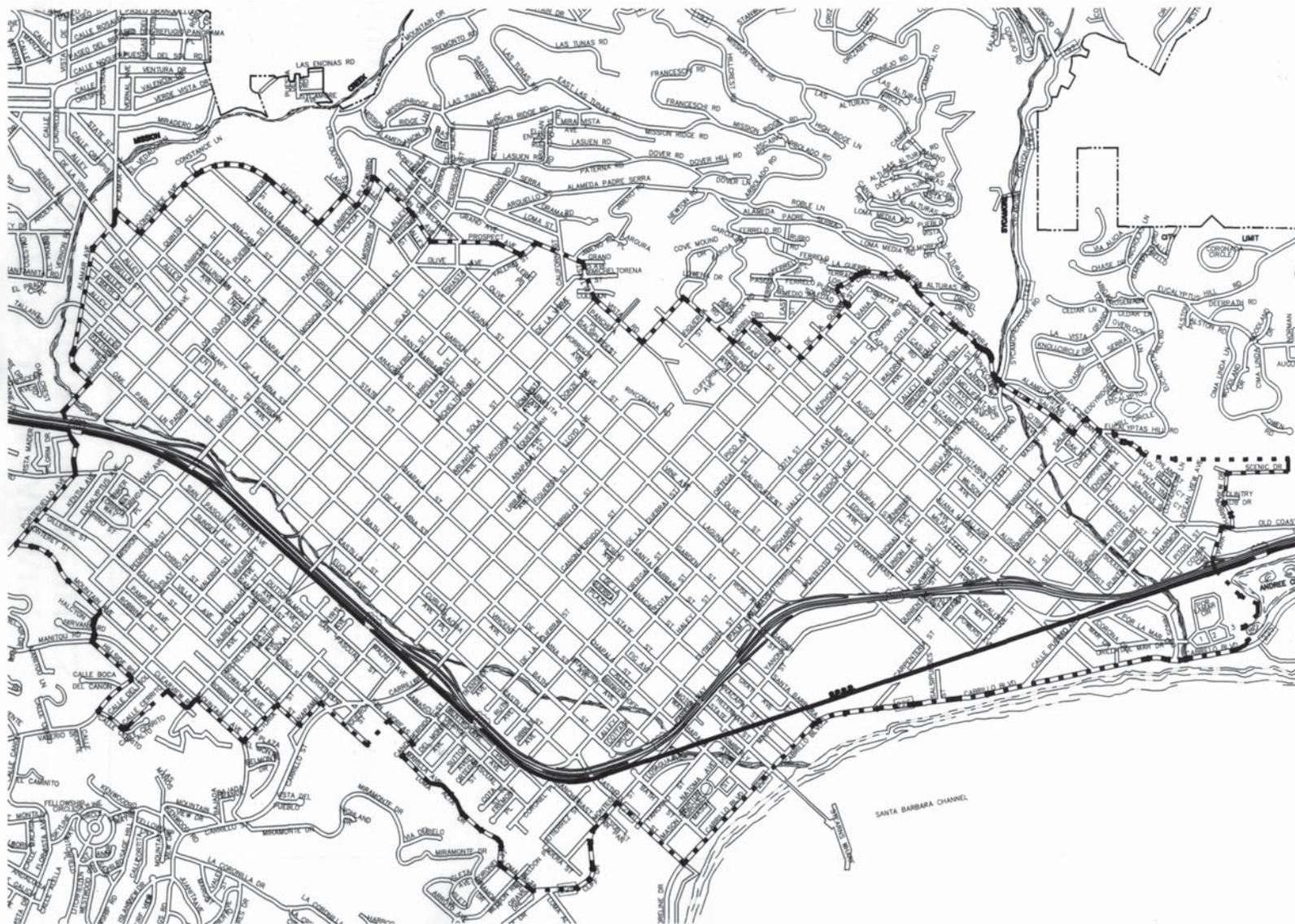
### **PURPOSE OF THE GUIDELINES**

This document contains Urban Design Guidelines for the City grid (see maps on the following pages). The grid area is roughly bounded by Portesuello Avenue and Alamar and Constance Streets on the north, a line following Constance Street, Prospect Avenue, Milpas Street, Alameda Padre Serra, and Salinas Street on the east, Cabrillo Boulevard on the south, and a line following the eastern slope of the Mesa Hills to Clearview Road on the west. The grid does not include properties that are within the Hillside Design District.

The traditional center of Santa Barbara and the South Coast is the grid. The grid is the City's strength. The grid contains a diverse mix of residential and nonresidential land uses. Its residential areas have easily accessible neighborhood services. The topography allows for walking, biking or using transit, and there are numerous travel routes to every destination. In addition, the grid is largely developed in traditional patterns and contains a pleasing mix of buildings in varied architectural styles. Santa Barbarans value the grid for its human scale character and pedestrian orientation.



**CITY OF SANTA BARBARA**  
**CITY GRID**  
**VICINITY MAP**



**CITY OF SANTA BARBARA**  
**CITY GRID**  
**STREET MAP**

The Urban Design Guidelines for the grid will serve several important purposes, as follows:

- ❖ The guidelines will apply the traditional design principles contained in existing City policy documents to development projects within the grid;
- ❖ The guidelines will help to protect and enhance the existing human scale character of the grid, building on its traditional development pattern;
- ❖ The guidelines will enhance the City’s distinctive architectural character by encouraging a sensitive relationship between historic older structures and new buildings; and
- ❖ The guidelines will support the development of a balanced transportation network that provides a variety of travel options within the grid.

## USE OF THE GUIDELINES

The Urban Design Guidelines provide a framework for the City’s design review process, telling staff and decision makers (e.g. the Architectural Board of Review, Historic Landmarks Commission, Planning Commission, and City Council) how to implement the City’s policies. In addition, the guidelines give direction to design professionals (e.g. developers and architects) and the general public regarding how to enhance the aesthetic character of the City and support the City’s transportation network through the design of buildings and infrastructure. The guidelines also provide design professionals and the public with the criteria by which their development proposals will be evaluated.

The guidelines in this document are not intended to be rigid standards that discourage creativity in design. Guidelines are tools that are inherently flexible and are crafted to allow for innovative design solutions that are consistent with the City’s goals.

## APPLICABILITY OF THE GUIDELINES

### TYPES OF PROJECTS SUBJECT TO THE URBAN DESIGN GUIDELINES

The Urban Design Guidelines shall be applied to projects that involve the erection or exterior alteration of nonresidential, multiple-family, two or more story two-family, and mixed use (residential and nonresidential) buildings or structures within the City grid. These guidelines shall also apply to the erection or exterior alteration of buildings or structures where two or more detached dwellings exist on a single lot, or where residential and nonresidential buildings exist together on a single lot. Infrastructure improvements that require review and approval by the City (including, but not limited to, street furniture, lighting, transit facilities and bicycle facilities) must also comply with these guidelines.

One-family and one story two-family dwellings are specifically exempted from these guidelines (this exemption does **not** apply to single lots containing more than one residence or duplex – see above). Please see the *Single Family Residence Design Guidelines* for the City of Santa Barbara for information regarding the design of single-family and one story two-family dwellings.

**ADDITIONAL DESIGN GUIDELINES THAT APPLY TO PROJECTS WITHIN THE CITY GRID**

There are three different design review boards that guide development in the City grid: the Architectural Board of Review; the Historic Landmarks Commission; and the Sign Committee. Each of the review boards has a unique purview, and each uses different sets of design guidelines to evaluate proposed developments. The Urban Design Guidelines are intended to be used in conjunction with these existing guidelines, which are predominantly architectural. The Urban Design Guidelines focus on site planning, planning to support a balanced transportation network, and neighborhood compatibility. Used together, the two types of guidelines will facilitate the comprehensive review of development proposals. The Urban Design Guidelines shall not override any other applicable set of design guidelines.

The **Architectural Board of Review** (ABR) is required to review applications for building permits as described in Section 22.68.040 of the City of Santa Barbara Municipal Code. The ABR does not review projects that are subject to the review of the Historic Landmarks Commission (see description below). In addition to the Urban Design Guidelines, the ABR will use the following guidelines to evaluate proposed projects within the City grid:

- ❖ Architectural Board of Review Guidelines;
- ❖ Haley-Milpas Design Manual;
- ❖ Highway 101 Santa Barbara Coastal Parkway Design Guidelines;
- ❖ Harbor Master Plan Design Guidelines;
- ❖ Landscape Design Standards for Water Conservation;

- ❖ Outdoor Lighting Design Guidelines;
- ❖ Sign Review Guidelines;
- ❖ Single Family Residence Design Guidelines; and
- ❖ Waterfront Area Design Guidelines.

The **Historic Landmarks Commission** (HLC) is required to review all applications for building permits involving properties within El Pueblo Viejo Landmark District or other designated Landmark Districts, or those involving a designated Landmark (see Chapter 22.22 of the City of Santa Barbara Municipal Code). The HLC also has the power to recommend to the City Council that certain structures, natural features, sites, or areas having historic, architectural, archaeological, or cultural significance be designated as a Landmark or Structure of Merit. In addition to the Urban Design Guidelines, the HLC will use the following guidelines to evaluate proposed projects within the City grid:

- ❖ Brinkerhoff Avenue Landmark District Guidelines;
- ❖ El Pueblo Viejo District Guidelines;
- ❖ Highway 101 Santa Barbara Coastal Parkway Design Guidelines;
- ❖ Landscape Design Standards for Water Conservation;
- ❖ Outdoor Lighting Design Guidelines;
- ❖ Single Family Residence Design Guidelines;
- ❖ State Street Landscaping Guidelines; and
- ❖ Waterfront Area Design Guidelines.

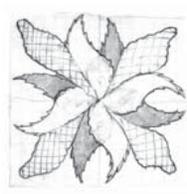
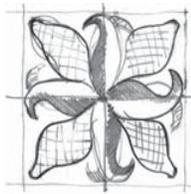
The **Sign Committee** reviews all applications for signs that require permits, as outlined in Chapter 22.70 of the City of Santa Barbara Municipal Code. In addition to the Urban Design Guidelines, the Sign Committee will use the Sign Review Guidelines to evaluate proposed signs within the City grid.

All of the design guidelines listed previously are available for purchase at the Planning Division Counter at 630 Garden Street.

In addition to the design review boards, the **Planning Commission** reviews development applications as outlined in Section 806 of the Charter of the City of Santa Barbara and the Santa Barbara Municipal Code. The purview of the Planning Commission includes zoning, building, land use, redevelopment, conservation, proposed public works and other related matters. The Planning Commission will use the Urban Design Guidelines in conjunction with other applicable planning and policy documents.

### **ADDITIONAL DEVELOPMENT REQUIREMENTS FOR PROJECTS WITHIN THE CITY GRID**

The guidelines in this document were created to apply to the development of both public and private property. Implementation of the guidelines in some instances will require special City review or permits, or development or easement agreements with the City or other public entities. In some circumstances, licenses may be required. The purpose of this section is to outline common situations that require such arrangements. If you have any questions regarding whether your project requires special review, please consult the applicable City department.



### **PUBLIC WORKS STANDARDS AND DESIGN GUIDELINES**

Certain types of projects may be subject to the following standards and design guidelines, in addition to the guidelines that were previously identified;

- ❖ Public Works Design Standards (includes streets, transportation, water, sewer, drainage, and engineering standards); and
- ❖ City of Santa Barbara Access and Parking Design Guidelines (contains criteria for the design of bicycle and automobile parking facilities in the City).

These documents are available at the Public Works Counter at 630 Garden Street.

### **DEVELOPMENT IN THE PUBLIC RIGHT-OF-WAY**

Per Municipal Code Chapter 22.60, any improvement in a public street, alley, court, right-of-way, public easement, or public place within the City, requires a written permit from the Public Works Director. Applicable improvements include, but are not limited to, any construction, reconstruction, repair, removal, or replacement of pavement, sidewalks, driveways, curbs, gutters, bicycle facilities, transit facilities, or pedestrian facilities (including benches and other street furniture). In some cases, maintenance agreements or licenses for such improvements may be required.

## **PARKWAY LANDSCAPING**

A parkway is defined as either the area between the curb and sidewalk within a fully improved right-of-way, or the area extending six feet (6') from the curb towards the nearest right-of-way line in areas without sidewalks, or any area within a street right-of-way in which a designated parkway tree species is located.

Per Municipal Code Chapter 15.20, all trees, plants whose ultimate growing height is over eight inches (8"), or non-living ground cover materials within a parkway require a written permit from the Parks and Recreation Department Director. All trees within a parkway must be planted and maintained according the standards of the Parks and Recreation Department.

In addition, whenever a property owner or occupant desires to plant, prune, trim, perform maintenance on, or remove any tree within a parkway, tree well, public area, or street right-of-way, an application must be filed with the Public Works Department and coordinated with the Parks and Recreation Department. The applicant shall be responsible for this coordination.

## **FACILITIES FOR GENERAL PUBLIC USE ON PRIVATE PROPERTY**

Facilities that are designed for general public use, but are located on privately owned land (i.e. a new paseo), may require public access easements and/or maintenance agreements with the City.

## **STATE REQUIREMENTS**

In addition to the above listed City requirements, there are State requirements that all developments must adhere to, including the following:

- ❖ All new construction pursuant to these guidelines must comply with the disabled access regulations contained in the California Building Code. Access agreements or dedications may be necessary in some instances to provide facilities for disabled access.
- ❖ All projects that require discretionary review (including review by the Architectural Board of Review, Historic Landmarks Commission, Planning Commission, and the City Council) are subject to environmental review pursuant to the California Environmental Quality Act (CEQA).

## **FORMAT OF THE DOCUMENT**

The Urban Design Guidelines document is divided into nine numbered chapters. Each chapter is formatted and arranged as follows:

### **GOALS**

The goals of each chapter describe the ideal future end toward which planning measures are directed. The goals are general expressions of community values and, therefore, are abstract in nature.

The goals are shown under the title of each chapter as bulleted, italicized text. For example:

- ❖ *Preserve the human scale character of the grid by using design techniques that reduce the apparent size, bulk, scale, and height of buildings.*

## CHAPTER DIVISIONS

Each chapter is divided into sections. The first section is an introduction, the purpose of which is to relate the Urban Design Guidelines to existing conditions present in the City grid.

Subsequent sections are numbered using two digits (e.g. 2.1) and labeled as to their content. The first digit of the section number corresponds to its chapter number.

## GUIDELINES

Guidelines are principles that are used to help determine a course of action. Guidelines are inherently flexible and allow for innovative design solutions that are consistent with the chapter goals.

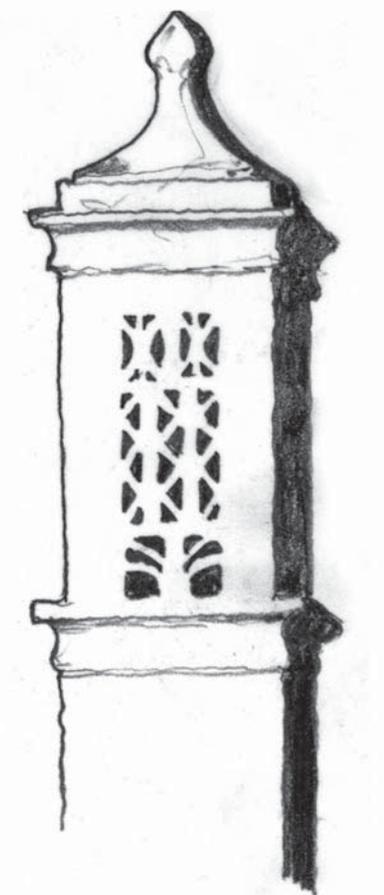
Guidelines are shown in normal text and numbered using three digits, as follows:

- 2.1.1 The building base should visually anchor the building, establishing a strong connection to the ground and the site.

The first two digits of of the guideline number correspond to its section number.

## ILLUSTRATIONS

The illustrations in this document provide examples of development that incorporates the concepts contained in the Urban Design Guidelines. The caption for each illustration refers the user to any specific design guidelines that are depicted.





**CHAPTER 1: COMPATIBILITY OF NEW DEVELOPMENT  
WITH THE EXISTING ENVIRONMENT**

# 1. COMPATIBILITY OF NEW DEVELOPMENT WITH THE EXISTING ENVIRONMENT

- ❖ *Design developments to complement and enhance the character of Santa Barbara, the surrounding neighborhood, and existing adjacent developments, while allowing each development to retain a distinct visual identity.*
- ❖ *Incorporate natural features and landscaped open spaces into developments to provide a sense of openness and continuity and enhance the environment of the City grid.*
- ❖ *Design developments to respect the arrangement of buildings and open spaces on adjacent sites and provide opportunities for enhanced circulation, solar access, and views.*

## INTRODUCTION

The urban grid area of Santa Barbara is known for its historic character, pedestrian-friendly qualities, and exemplary architecture. It is a distinctly urban environment, softened by vistas of the mountains, ocean, and the attention to detail that is evident in both the built environment and landscaped open spaces.

Most new development in the grid will be either infill development of vacant properties or the redevelopment of existing buildings. Due to the close proximity of buildings to one another and the established urban fabric of the grid, it is important that new development be compatible with and complement the character of the grid, enhance existing natural features, and incorporate appropriate landscaped open spaces.

### 1.1 COMPATIBILITY WITH THE CHARACTER OF THE CITY, THE SURROUNDING NEIGHBORHOOD, AND ADJACENT PROPERTIES

Because every project will be unique in its setting and form, the design review boards will need to exercise discretion when evaluating whether a proposed development will be compatible with the existing environment.

Generally, proposed developments should demonstrate compatibility on three different levels:

- ❖ The development should be compatible with the distinctive architectural character of Santa Barbara;
- ❖ The development should be compatible with the character of the surrounding neighborhood; and
- ❖ The development should be compatible with immediately adjacent developments.

**While it is generally desirable for new developments to adapt and use design palettes similar to those of surrounding developments, they must also be made to possess unique characteristics and qualities.** The elements listed in the following sections may make a development compatible with its surroundings while allowing it to retain a distinct identity.

- 1.1.1 The design review process should involve an evaluation of the compatibility of proposed developments with Santa Barbara's distinctive architectural character, the overall neighborhood, and adjacent developments. Architects and designers must demonstrate thoughtful planning and consideration as to the degree of compatibility that their proposed projects exhibit.

Drawings, models, or other graphic communications presented to the design or development review boards should show neighboring buildings and important features of adjacent sites in sufficient detail to demonstrate the relationship between the proposed development and its surroundings. As a general rule, views of the proposed project and its neighbors should be provided as seen from public areas (e.g. the street and sidewalk). Story poles may be required in order to evaluate a proposed development.



*This elevation shows how the design elements listed in section 1.2 can be applied to make new developments compatible with existing adjacent developments and the surrounding neighborhood.*

*While having similar architectural features, each building has a unique form and distinctive detailing that enhances the streetscape. Reference Guideline: 1.2.1.*

## 1.2 STRUCTURES

The following structure design techniques, when applied in combination with the design techniques in sections 1.4 and 1.5, may make a proposed development compatible with the existing environment. These techniques include, but are not limited to, the following;

- ❖ Use an architectural style similar to surrounding structures;
- ❖ Adapt and incorporate prominent or distinctive design elements from neighboring structures (e.g. rooflines, recesses, projections, towers, and balconies);
- ❖ Design the structure in a size, bulk, and scale that is comparable to existing surrounding developments;
- ❖ Coordinate the form and height of the new structure with existing structures in a block; and
- ❖ Use colors or materials similar to those of adjacent developments.

- 1.2.1 Consider the transition from one structure to the next. Each structure must exhibit its own unique character, while displaying careful consideration of the character of surrounding structures.

## 1.3 STRUCTURE EXCEPTIONS

In some cases, the design review boards may determine that a structure that looks substantially different than its immediate surroundings (in terms of its size, bulk, scale, height, or architectural style) would be appropriate.

Where a structure is proposed in an area without established design goals, it must be compatible with Santa Barbara's distinctive architectural character. It must also be held to an exceptionally high standard of design, since it will be a highly visible and precedent setting example for the design of surrounding developments.

- ❖ *A structure might be proposed with a size, bulk, scale, or height that is substantially greater than that of the surrounding developments.*

There are circumstances where a larger scale structure may be desirable, even when the surrounding developments are built to a smaller scale. For example, the size, bulk, scale, and height of the Arlington Theater are considerably greater than the surrounding buildings on the block. The building was designed as a public building and an active center of a block of smaller shops, as well as a visual centerpiece for the City. Rather than appearing out of scale with the smaller structures in the surrounding area, the form and height of the adjacent structures set off the theater and accent its grandeur.

- ❖ *A structure might be proposed in an architectural style that differs from surrounding developments.*

A structure in an architectural style that differs from surrounding developments may be allowed if it is consistent with design goals for the larger neighborhood. For example, the guidelines for the El Pueblo Viejo District state that any new development must be in a Hispanic architectural style.

However, there are many pockets of development within the district that have not yet transitioned to Hispanic styles and contain predominantly structures of other styles (e.g. Craftsman and Victorian). New developments in El Pueblo Viejo, because they must exhibit a Hispanic architectural style, may look different than the existing surrounding development.

- 1.3.1 Structures that differ in size, bulk, scale, height, or architectural style from adjacent developments may be allowed if they are consistent with design goals for the larger neighborhood or the distinctive architectural character of Santa Barbara. Such structures shall be held to an exceptionally high standard of design, since they will be highly visible and precedent setting examples for the design of surrounding developments.

## 1.4 LANDSCAPING

Landscaped areas in the City grid provide a sense of natural beauty and openness, encourage continuity between developments, and enhance the overall cityscape. A variety of landscaping elements (including distinctive and native tree species) can break up the monotony of paved and built surfaces, screen undesirable views, provide essential shade and oxygen, provide habitats for a variety of species, lessen reflected heat, and capture airborne particulate pollutants and exhaust. Landscaping elements contribute greatly to a cleaner environment and more healthy, livable neighborhoods.

The following landscape design techniques, when applied in combination with the design techniques in sections 1.2 and 1.5, may make a proposed development compatible with the existing environment. These techniques include, but are not limited to, the following:

- ❖ Preserve and incorporate existing natural and landscaping features and mature trees into new development;
- ❖ Select landscaping elements that are appropriate to the site and complement the overall character of the grid; and
- ❖ Use landscaping elements that complement the characteristics of nearby developments.

1.4.1 The preservation and protection of natural features and mature trees is highly desirable. These elements shall be incorporated into development projects to the greatest extent possible.

1.4.2 Appropriate landscaping elements shall be selected based on their suitability for the climate, geology, and topography of the site.

1.4.3 The use of canopy trees is encouraged. Tree selection shall take into account the density, shape, size, solar orientation, maintenance requirements, and neighborhood impacts of the mature tree.

1.4.4 Landscaping should complement the color, materials, architectural style, scale, and landscaping of nearby developments. Use a variety of sense-stimulating plantings that add color and texture to the built environment.

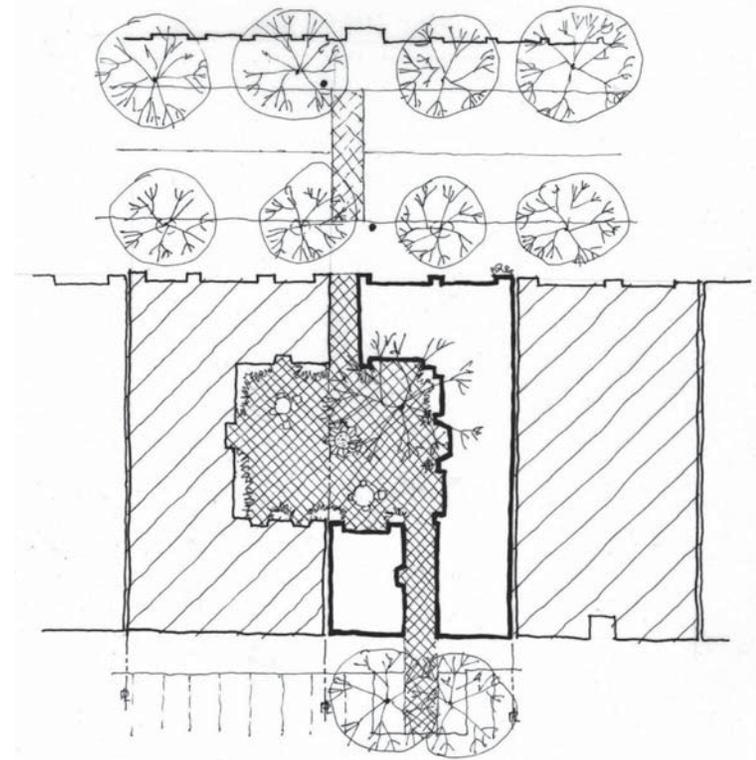
Guidelines for incorporating landscaping into development projects also appear in sections 3.1, 3.2, 4.1, 4.2, 4.3, 5.2, 6.2, 6.3, 8.1, 8.2, and 9.2 of this document. They are grouped with the specific types of developments and infrastructure improvements that they address.

## 1.5 SITE ORGANIZATION

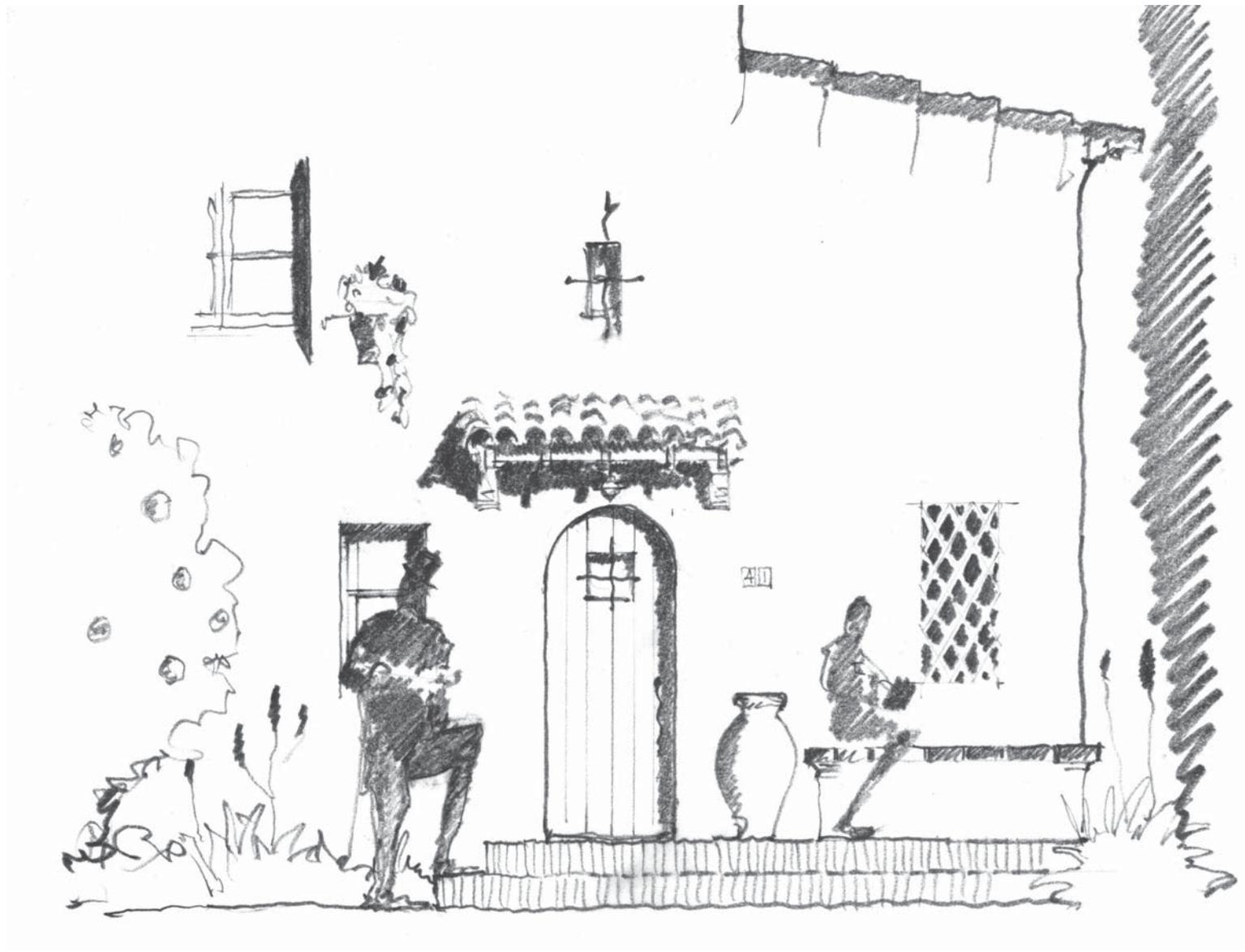
The following site organization techniques, when applied in combination with the design techniques from sections 1.2 and 1.4, may make a proposed development compatible with the existing environment. These techniques include, but are not limited to, the following:

- ❖ Use setbacks and building orientations that are compatible with surrounding developments;
- ❖ Use a site plan arrangement that respects the layout of adjacent developments;
- ❖ Create or enhance public scenic view corridors; and
- ❖ Enhance circulation within a block or neighborhood.

- 1.5.1 The site organization of a proposed development should respect the arrangement of buildings and open spaces on adjacent sites to maximize the shared benefits of sunlight, circulation, and views.



*The new development in this illustration (center, bold) has been planned to enhance and expand an existing open space. The site arrangement has also provided an opportunity for a paseo between the interior parking lot and the street frontage. Reference Guidelines: 1.5.1 and 4.3.1.*



## CHAPTER 2: HUMAN SCALE CHARACTER

## 2. HUMAN SCALE CHARACTER

### VISUAL RELATIONSHIP BETWEEN DEVELOPMENT AND PEDESTRIANS

- ❖ *Preserve the human scale character of the grid by using design techniques that reduce the apparent size, bulk, scale, and height of buildings.*
- ❖ *Provide visual interest for pedestrians by incorporating building details that relate to the surrounding built environment at a human scale.*

#### INTRODUCTION

In order to support a vital pedestrian network, buildings must visually relate to the street at a pedestrian scale. Buildings in the grid must be designed with sufficient attention to scale and detail that the pedestrian's sense of discovery is renewed with each viewing. Creating human scale usually requires reducing the apparent size, bulk, scale and height of buildings, so that they do not overwhelm pedestrians. There are many architectural and design techniques that can achieve or convey a sense of human scale. Although the methods outlined in this chapter are encouraged, other approaches will be considered acceptable if they achieve the same objectives.



*While the base of the above building appears massive, it contains details that are comprehensible to the pedestrian and proportionate to the scale of the building. Reference Guideline: 2.1.1.*

If the application of the suggested techniques is not successful, the design review boards may request that the size of buildings be reduced.

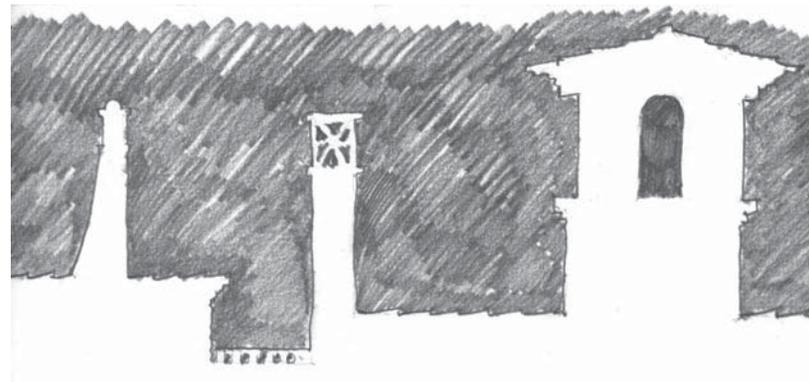
New developments should demonstrate consideration of building composition and detailing with the goal of achieving a human scale environment. This may be shown through elevation drawings, models, or other graphic communications presented to the design and/or development review boards. As a general rule, views of the proposed project should be shown from public areas (e.g. streets and sidewalks).

## 2.1 BUILDING COMPOSITION

- 2.1.1 The building base should visually anchor the building, establishing a strong connection to the ground and the site. The base of the building should appear more massive than the upper stories. Building details and public art elements are encouraged to provide visual interest and a sense of discovery. Details should be comprehensible to passing pedestrians and proportionate to the scale of the building.
- 2.1.2 The upper stories of the building should exhibit a lighter character than the base, possibly by reducing floor area and building mass. Architectural details on the upper stories should be at a scale that relates to the overall building composition. As a general rule, massing and details should be simple and proportionate to the scale of the building. The length and depth of cantilevers should be minimized.
- 2.1.3 Where appropriate, building tops should be articulated using elements such as: tapered or sculpted roof forms to create silhouettes against the sky (including false chimneys, towers, and decorative vents and caps); roof materials and overhangs to create strong shadow patterns; and decorative cornices to provide visual interest. Break up the horizontal lines of long parapets using variations in height or other appropriate design techniques.

## 2.2 REDUCTION OF APPARENT SIZE, BULK, SCALE, AND HEIGHT

- 2.2.1 Buildings should be designed as carefully orchestrated compositions of smaller parts. The perceived size, bulk, scale, and height of a building should be reduced by either visually or physically dividing its mass into smaller scale components. The following are techniques that are encouraged to create human scale in new buildings:
- ❖ Reduce the actual bulk of a large building by dividing it into several smaller buildings to create a “campus” or “village”. Groups of smaller buildings are generally visually preferable to one large, bulky building, and are also more easily adaptable to a variety of uses;



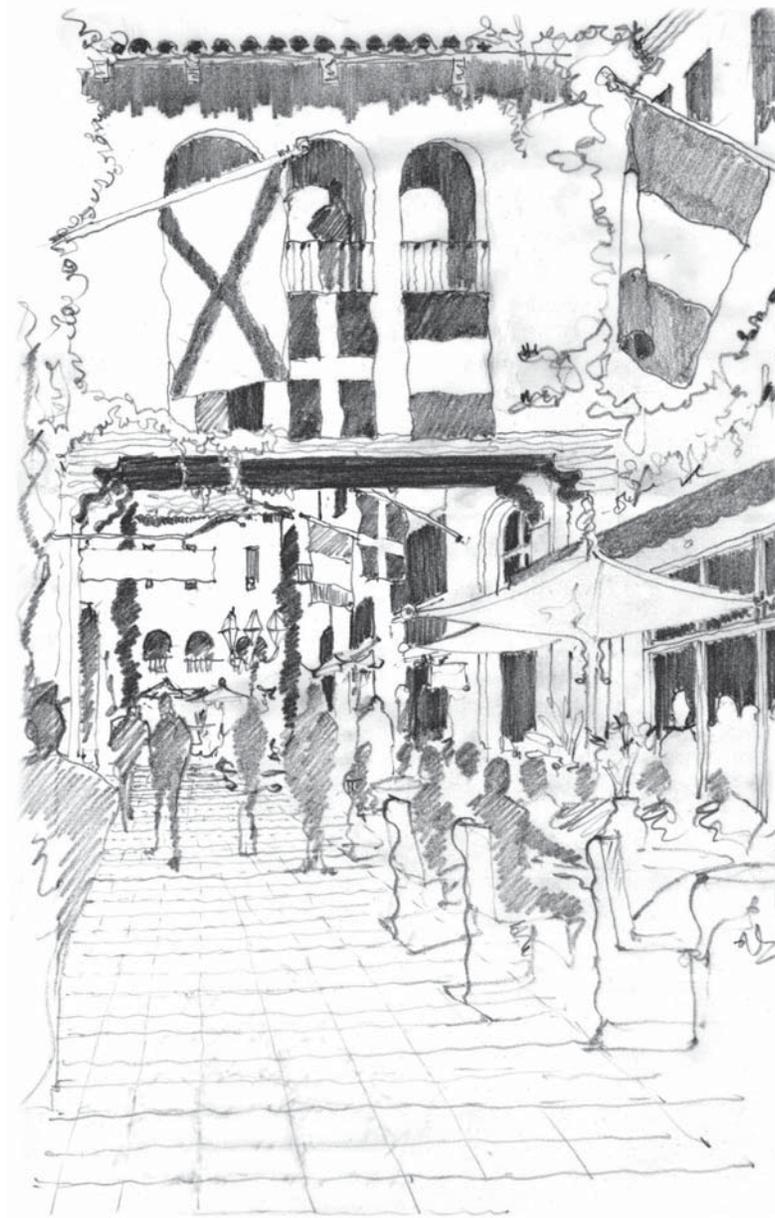


*This large building appears smaller due to the variations in height and roofline and the use of recesses and projections. Reference Guidelines: 2.2.1 and 2.2.2.*

- ❖ Use variations in height and roofline to reduce the perceived height of the building;
- ❖ Use planter walls to reduce the apparent height of the building;
- ❖ Organize the façades of a large project or building into several visually distinct parts to create the appearance of several smaller buildings;
- ❖ Use roof overhangs to decrease the vertical appearance of the walls;
- ❖ Use color to visually reduce the size, bulk, and scale of the building; and
- ❖ Use recesses and projections to visually divide building surfaces into smaller scale elements (see guideline 2.2.2).

2.2.2 The use of recesses and projections is encouraged to divide the surfaces of buildings into smaller scale elements, as follows:

- ❖ Large or long, continuous wall surfaces should be avoided. As a general principle, building surfaces should be relieved with a change of wall plane that provides strong shadow and visual interest;
- ❖ Use recesses to define courtyards, entryways, circulation routes, or other outdoor spaces that are accessible from the exterior of the building;
- ❖ Expression of wall thickness is desirable. Reveals, returns, and deep recesses at door and window openings are encouraged;
- ❖ Recessed balconies, arcades, and loggias create a sense of depth in the building walls, contrasting surfaces exposed to the sun with those in shadow;
- ❖ Use projections to emphasize important architectural elements, such as stairs, towers, balconies, and verandas; and
- ❖ Use materials with textural interest to break up large wall surfaces.



### CHAPTER 3: THE BUILDING / STREET EDGE

## 3. THE BUILDING/STREET EDGE



### FUNCTIONAL RELATIONSHIP BETWEEN DEVELOPMENT AND PEDESTRIANS

- ❖ *Encourage pedestrian activity on the street through building design. Frequent building entrances, windows at pedestrian height, and outdoor activity spaces create a lively, pedestrian-friendly environment along public streets.*
- ❖ *Create visually unified street spaces by planning the orientation of buildings and building setbacks to enhance the character of the street.*

### INTRODUCTION

Streets are the center of city life; a high level of social interaction on neighborhood and commercial streets is one of the foundations of a healthy, vibrant community. To contribute to the pedestrian-friendly character of the City grid, buildings must be designed to actively contribute to the life of the street.

Buildings that are oriented to the street (with doors, windows, and public spaces facing the street) encourage street activity and create a lively atmosphere. Buildings

that are oriented away from the street should be avoided since they send the message that activity on the street is undesirable. Areas where the majority of buildings are oriented away from the street are very discouraging to pedestrians, who perceive them to be unwelcoming or even unsafe.

For the purposes of these guidelines, a *street* will be defined as any existing or proposed street, road, avenue, boulevard, land, parkway, place, public alley, bridge, viaduct, or easement for public access. A street includes all land within the street right-of-way, whether improved or unimproved (see Glossary for expanded definition).

### 3.1 ACTIVITY NODES

#### BUILDING ENTRANCES AND WINDOWS

Building entrances and windows are essential elements that physically connect outdoor and indoor activity for pedestrians, making walking a more enjoyable and interesting experience.

Decisions regarding the placement of building entrances and windows will be considered in the following context:

- ❖ The potential for pedestrian activity around the building and existing pedestrian circulation routes will be assessed to determine appropriate pedestrian access points;



*This streetscape illustrates several principles of the Urban Design Guidelines, as follows:*

- ❖ *The buildings are visually compatible, yet retain a distinct identity;*
- ❖ *The buildings are of a human scale, with details that are attractive to pedestrians;*
- ❖ *The building entrances, windows, and active spaces provide opportunities for interaction on the street; and*
- ❖ *The pedestrian amenities make walking more attractive and convenient.*

- 
- ❖ For nonresidential or mixed use structures, the intended function or program of the building will be considered with specific attention to the placement of doors and windows. Common concerns include security (i.e. the prevention of theft and employee safety), the internal organization of building activities, and the distances that customers will have to travel to their cars with purchased goods;
  - ❖ The potential future uses of buildings will be considered when deciding upon the placement of building entrances and windows, especially with larger buildings. Building entrances and windows should be designed to allow the building to be adapted for a variety of uses; and
  - ❖ Visual and access compatibility between proposed buildings and adjacent developments will be considered when discussing the optimal placement of building entrances and windows.
- 3.1.1 Where a building with street frontage has only one entrance, that entrance shall be oriented to the street.
- 3.1.2 Where a building with street frontage has multiple entrances, the primary entrance shall be oriented to the street. Street entrances shall be as prominent or more prominent than other entrances, and are encouraged to remain open for pedestrian use.
- 3.1.3 Provide building entrances where appropriate, taking into consideration the location of the building, present and potential future uses of the building, pedestrian circulation routes, and the character of surrounding developments.
- 3.1.4 Provide windows at pedestrian height to provide interest for pedestrians on the street.
- 3.1.5 Corner buildings shall exhibit a strong visual and functional connection with the sidewalks of adjacent streets. This can be accomplished by placing entrances on each abutting street frontage or placing an entrance on the corner itself. Other features (including windows at pedestrian height, wall detailing, and public art) shall also be used to provide visual interest for pedestrians.
- 3.1.6 For mixed use and multiple-family residential buildings, the following guidelines regarding the placement and design of building entrances should be adhered to:
- ❖ Provide direct pedestrian access to the sidewalk from the front residential unit;
  - ❖ Provide a strong visual connection from the sidewalk to the entrances of interior residential units; and
  - ❖ Provide entry porches facing the street and/or the main internal pedestrian circulation route.

## ACTIVE SPACES AND LANDSCAPING

- 3.1.7 Where buildings are set back from the public right-of-way, incorporate courtyards or patio spaces that encourage outdoor activities along the building frontage. Such areas should include appropriate landscaping elements to soften the paved areas and provide shade for pedestrians.
- 3.1.8 Corner buildings shall be designed to enhance the character and pedestrian activities of the entire intersection, taking into consideration the contributions of all of the other existing corner buildings.

## 3.2 CONTINUITY OF STREET SPACES

The intent of the following guidelines is to create unified street spaces. Street spaces include both the public right-of-way and the adjacent building setback zone (where applicable). The network of street spaces establishes the scale and character of the environment. The setbacks and placement of buildings can create a feeling of consistency that visually unifies separate buildings and developments.

### BUILDING PLACEMENT

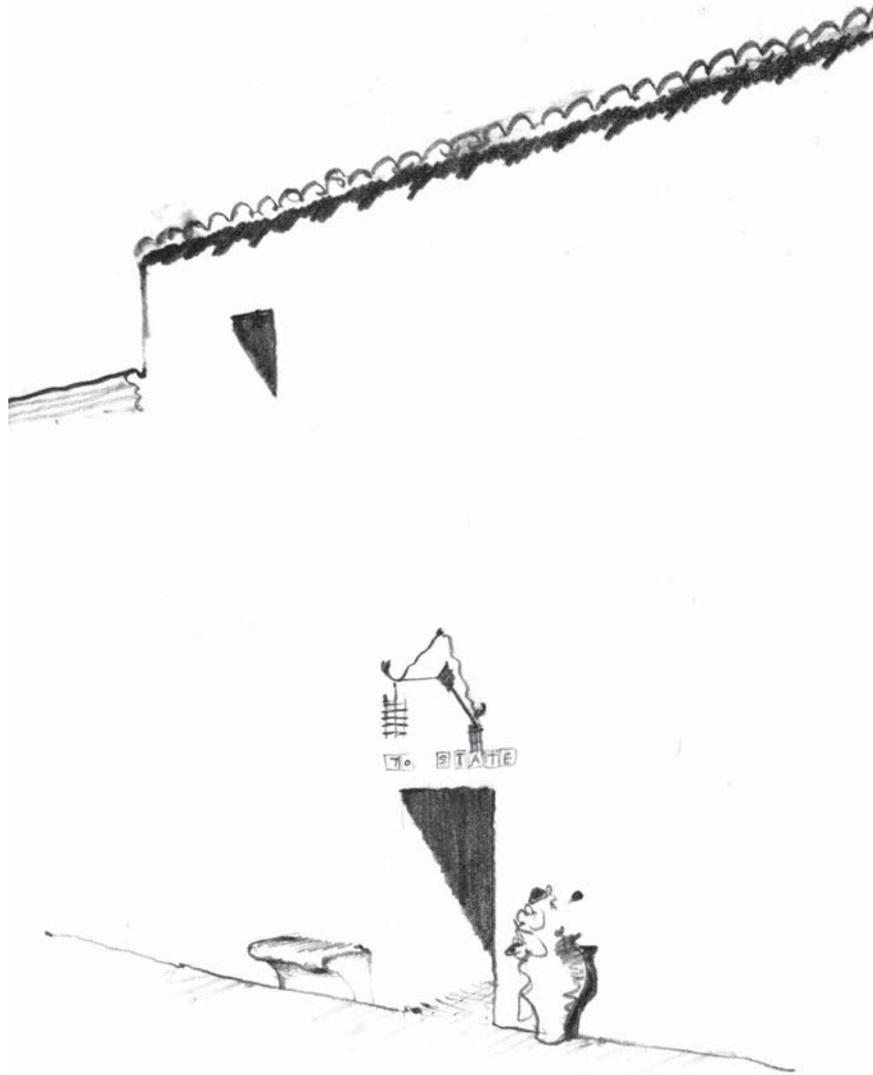
- 3.2.1 On lots with one street frontage, place the primary mass of buildings parallel to the street.
- 3.2.2 Avoid siting corner buildings with their primary mass at an angle to the corner. This shall not preclude angled or sculpted building corners or open plazas at corners.



*The form of the new building (left) is sensitive to the setback and height of the existing residence. Reference Guideline: 3.2.3.*

### SETBACKS AND LANDSCAPING

- 3.2.3 When siting a new building, consider the setbacks and scale of the existing neighborhood and adjacent buildings.
- 3.2.4 Where appropriate and consistent with neighboring development, locate new buildings on the edge of the public right-of-way to define the sidewalk line.
- 3.2.5 Where buildings are set back from the public right-of-way, place City reviewed and approved landscaping or architectural elements (e.g. arcades or low decorative walls) along the edge of the right-of-way to define the sidewalk line.



## CHAPTER 4: PEDESTRIAN FACILITIES AND AMENITIES

## 4. PEDESTRIAN FACILITIES AND AMENITIES

- ❖ *Create and maintain a continuous, convenient network of pedestrian facilities throughout the City grid to reduce dependence on the automobile.*
- ❖ *Provide pedestrian amenities, including street furniture, landscaping, lighting, and trash receptacles, to make walking more attractive and convenient.*
- ❖ *Design and locate pedestrian facilities and amenities to promote the uninterrupted flow of pedestrian traffic.*
- ❖ *Create pedestrian links to transit and bicycle facilities to increase the convenience of transit and bicycle travel.*

### INTRODUCTION

#### PEDESTRIAN FACILITIES

For the purposes of these guidelines, *pedestrian facilities* will be defined as improved walkways that are designed to carry pedestrian traffic between destinations. A complete pedestrian network must consist of several types of pedestrian facilities, each designed to serve different types of users and differing levels of use.

Pedestrian facilities can be categorized as follows:

**Sidewalks:** Generally, sidewalks are located in the public right-of-way and owned by the City and designed for public use at all times of the day. In the grid, sidewalks are generally provided around the perimeter of blocks and connect the street frontage of private lots.

**Paseos:** Paseos are a series of connecting walkways that join streets, open plazas, courtyards, cafes, and shops through the central portions of City blocks. They sometimes serve as connectors between parking facilities, commercial street frontage, and other popular destinations. Paseos promote a human scale environment by linking businesses and activities throughout a block and providing more travel routes for pedestrians. Paseos may be either publicly or privately owned and maintained, and are intended for general public use. Examples of privately owned paseos include El Paseo and La Arcada.

**Pathways:** Pathways are pedestrian facilities on private property. Pathways can serve a variety of functions, including linking separate buildings on a single site, linking buildings on adjacent sites, and connecting private buildings to sidewalks or paseos. Appropriate pathway designs will vary widely depending on the type and level of use the pathway is expected to support.

#### PEDESTRIAN AMENITIES

Pedestrian amenities are items that enhance the walking experience for the pedestrian. Examples include seating areas, canopy trees or other landscaping elements, lighting, drinking fountains, newsracks, trash containers, and telephones.

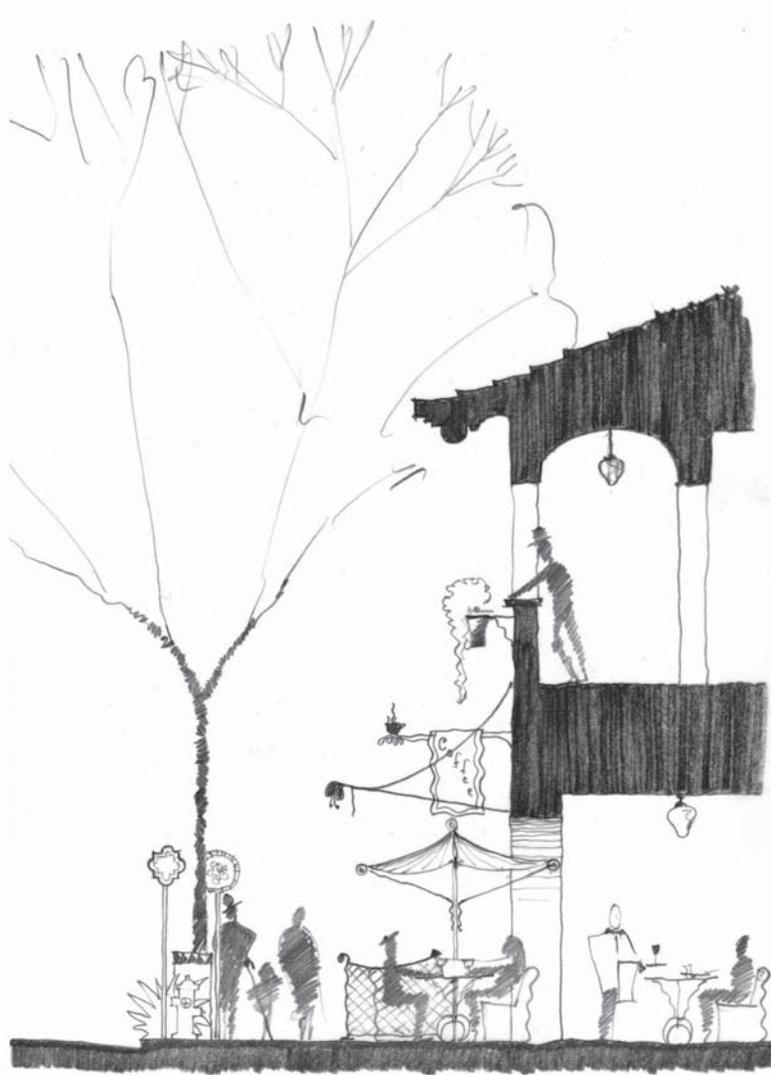
## 4.1 GENERAL GUIDELINES

These general guidelines apply to sidewalks, paseos, and pathways.

### PEDESTRIAN FACILITIES

- 4.1.1 Look for opportunities to create a continuous network of pedestrian facilities throughout the City grid. Consider future connections between residential and commercial areas, educational facilities, and recreational facilities, so that such connections are not prevented by buildings, fences, or other permanent improvements. Design pedestrian facilities to follow the most direct route between destinations.
- 4.1.2 Design pedestrian facilities to minimize interruption by automobiles (i.e. driveways, parking lots, and service areas).
- 4.1.3 Where automobiles intersect pedestrian facilities, ensure that pedestrians and automobiles are visible to each other and are not blocked by building projections, signs, or landscaping. Consider design features (including changing the surface composition) to clearly confer the right-of-way to pedestrians.
- 4.1.4 Create a buffer between pedestrian facilities and automobiles. Consider using a low wall, permanent landscaping, street furniture, curbside parking, or other appropriate method. Each of these methods must be reviewed and approved by the City.

- 4.1.5 Identify existing obstructions to pedestrian travel (e.g. utilities, signs, and overgrown landscaping) and remove or relocate where feasible. The undergrounding of overhead utilities is encouraged.
- 4.1.6 Consider using pedestrian bridges in public and private development projects to enhance access. Where pedestrians and automobiles must share a bridge, the clear width of the pedestrian facility shall be a minimum of five feet (5'). Buffers to separate automobiles and pedestrians shall be incorporated wherever possible. The design of bridges shall complement the scale and architectural style of surrounding buildings and infrastructure.
- 4.1.7 Consider historic pedestrian connections between properties and buildings and incorporate them in new developments whenever possible.
- 4.1.8 During the review process for City street abandonment, consider retaining portions of the street right-of-way to enhance the City's network of pedestrian facilities.
- 4.1.9 Drainage facilities shall be designed and located to minimize visibility and interference with pedestrian circulation. For the purposes of this guideline, drainage facilities shall not include creeks or other natural watercourses.
- 4.1.10 Ensure that pedestrian facilities are designed for disabled access in compliance with the California Building Code.



*The pedestrian amenities in this sidewalk scene are placed where they do not interrupt the flow of pedestrian traffic. The pedestrian amenities, pedestrian oriented signage, and parkway landscaping create a welcoming image for the street. Please refer to page 17 for information on the permits required for outdoor dining. Reference Guidelines: 4.1.4, 4.1.11-13, 4.1.15-19, and 4.2.3-6.*

## PEDESTRIAN AMENITIES

Several factors need to be considered when determining the types of pedestrian amenities that will be provided in a given area. These factors include the nature of the surrounding land uses, the level and hours of pedestrian use, the existing amount and type of pedestrian amenities, and the proximity of adjacent buildings.

- 4.1.11 Create seating opportunities (e.g. benches, raised planters, low walls, or sculptured stairs) in areas where pedestrians congregate. Seating should be located where it will not interrupt the flow of pedestrians and placed in sheltered or protected areas, wherever possible. Outdoor dining facilities shall also be located where they will not interrupt the flow of pedestrian traffic.
- 4.1.12 Place trash cans, drinking fountains, newspaper vending machines, telephones, or other pedestrian amenities in areas with high levels of pedestrian traffic. Pedestrian amenities should be placed in groups for maximum use, and located where they will not interrupt the flow of pedestrian traffic.
- 4.1.13 Design new pedestrian amenities to complement the architectural styles of existing amenities and surrounding buildings, while not overpowering the streetscape. Functional elements should be made decorative wherever possible to support the design theme of the street.

- 4.1.14 Use City reviewed and approved paving materials that complement the architectural style of surrounding buildings and minimize hazards such as slipping or tripping. Use permeable materials wherever possible to increase percolation and decrease run-off.
- 4.1.15 Provide pedestrian-scale lighting that complements the surrounding built and natural environment. When determining the proper level of illumination for a pedestrian area, it is important to consider the quality of light versus the quantity of light. The lighting should be subtle and avoid overlighting while being bright enough to provide a sense of security. Consider a variety of lighting types, including footlighting, indirect lighting (wall washing), and overhead lamps. All lighting fixtures shall conform to the City's Outdoor Lighting Design Guidelines.
- 4.1.16 Building identification signage adjacent to pedestrian facilities shall be small scale and oriented to pedestrians, rather than passing cars. Incorporate historical interpretive signage, where appropriate, to enhance the pedestrian experience. All signs shall conform to the City's Sign Ordinance.

## LANDSCAPING

The landscaping guidelines in this chapter should be applied in addition to the guidelines contained in section 1.4 of this document.

- 4.1.17 Ensure that landscaping is appropriate for the site, is well-maintained, and does not create a safety hazard by concealing or overgrowing the pedestrian facility.
- 4.1.18 Use canopy trees wherever possible to provide shade and weather protection for pedestrians. Adequate room for tree growth should be provided so that the pedestrian facility will not be damaged by tree roots. Minimum spacing requirements for planting specific tree species can be obtained from the City Arborist.
- 4.1.19 Use landscaping to provide a buffer between vehicles and pedestrians and to screen parking and utility areas.

## 4.2 SIDEWALKS AND PARKWAYS

The following guidelines apply specifically to sidewalks and parkways, in addition to the general guidelines outlined in section 4.1 of this document.

*Parkways* are defined by the City of Santa Barbara Municipal Code as either of the following:

- ❖ The area between the curb and the sidewalk within a fully improved street right-of-way;
- ❖ That area extending six feet from the curb towards the nearest right-of-way line in an area with no sidewalk; or
- ❖ Any area within a street right-of-way in which an official or parkway tree is located.



*This cross-section illustrates a possible sidewalk/parkway configuration for the Downtown core or other areas where multi-story buildings abut the right-of-way line. The parkway provides space for pedestrian amenities and landscaping away from the flow of pedestrian traffic. The width of the sidewalk is appropriate for the level of use. Reference Guidelines: 3.2.4, 4.1.4, 4.1.12, 4.1.17-19, 4.2.1, and 4.2.3-6.*

Per Chapter 15.20 of the City of Santa Barbara Municipal Code, all trees, plants whose ultimate growing height is over eight inches (8"), or non-living groundcover materials within a parkway require a written permit from the Parks and Recreation Department Director. All trees within a parkway must be planted and maintained according to the standards of the Parks and Recreation Department.

Sidewalk and/or parkway improvements may be required in conjunction with proposed development projects.

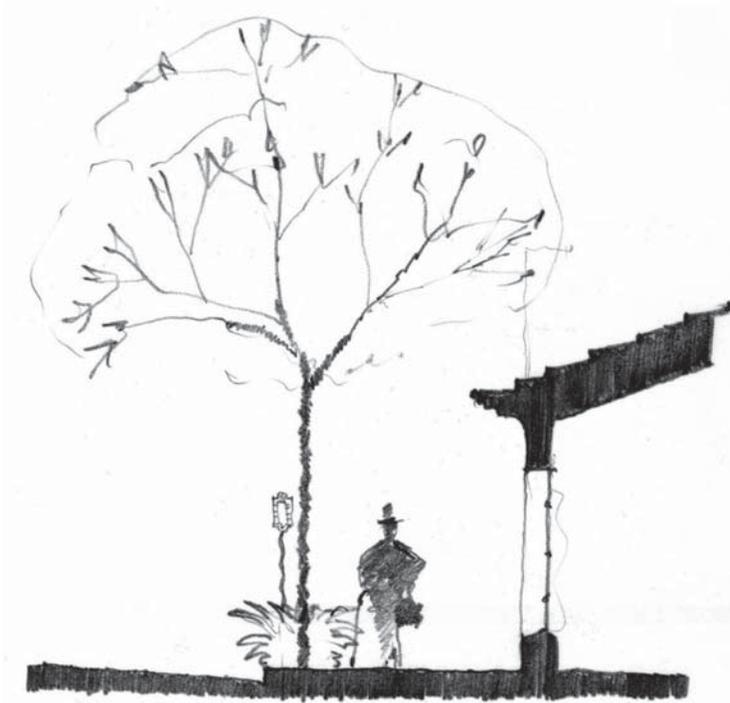
These guidelines shall not override existing design guidelines that pertain to specific pedestrian areas of the City (e.g. the State Street Landscaping Guidelines).

## **SIDEWALKS**

- 4.2.1 Look for opportunities to fill in gaps in sidewalks, using the current ordinance standards for pavement width as **minimum** dimensions. The width of new sidewalks should be appropriate to the level and type of pedestrian traffic the sidewalk is expected to accommodate. Historic sidewalk dimensions should be investigated and incorporated where appropriate. Look for opportunities to widen sidewalks that do not meet the minimum standards.
- 4.2.2 Preserve historic concrete sidewalk stamps (i.e. street names stamped into the concrete or contractor's stamps) to the greatest extent possible when renovating or replacing sidewalks.

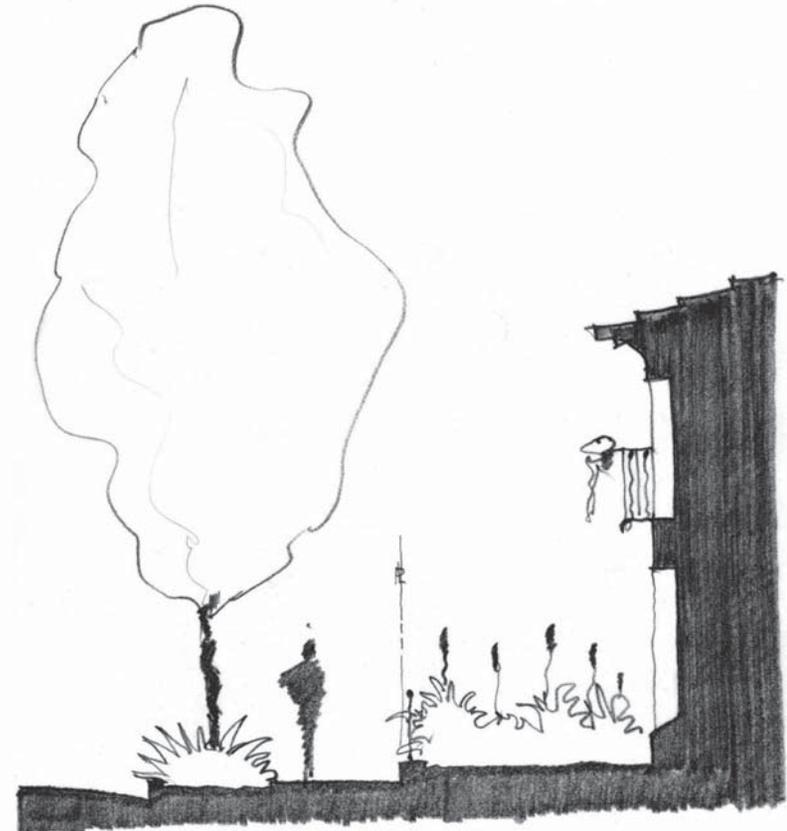
## **PARKWAYS AND LANDSCAPING**

- 4.2.3 Any proposed street or sidewalk improvement shall, where feasible, incorporate the installation of parkways or tree well planting sites. Landscape plans for parkways or tree wells shall be integrated into the general plan for improvements.



*This cross-section illustrates a possible sidewalk/parkway configuration for commercial areas outside of the Downtown core or other areas where single story buildings abut the right-of-way line. Reference Guidelines: 3.2.4, 4.1.4, 4.1.12, 4.1.17-19, 4.2.1, and 4.2.3-6.*

4.2.4 Maintain the use of parkways for landscaping. Parkway shall not be paved or developed with other impervious surfaces. Existing paving or impervious surfaces should be replaced with landscaping, whenever possible.



*This cross-section illustrates a possible sidewalk/parkway configuration for multi-family residential areas or other areas where building setbacks are required. Reference Guidelines: 3.2.5, 4.1.4, 4.1.12, 4.1.17-19, 4.2.1, and 4.2.3-6.*

4.2.5 Provide street trees at appropriate intervals to produce a desirable shade canopy for the sidewalk and visually narrow the street from the motorist's perspective. Parkway shall allow adequate space for tree growth. Minimum spacing requirements for planting specific tree species can be obtained from the City Arborist. Species designations are the responsibility of the City Parks Commission.

- 4.2.6 Provide groundcover and shrubs where appropriate to create a complete landscaping image for the street.
- 4.2.7 Preserve historic sandstone curbing to the greatest extent possible when making street or sidewalk improvements.

### **CROSSWALKS**

- 4.2.8 Crosswalks shall be designed to clearly confer the right-of-way to the pedestrian and minimize the crossing distance.
- 4.2.9 Encourage the development of mid-block crosswalks in areas with high pedestrian volumes. The location of mid-block crosswalks shall be determined by the Public Works Department.
- 4.2.10 Consider raised crosswalks where there are no traffic signals. The color and texture of paving materials shall be reviewed and approved by the City. The paving materials should enhance visibility and minimize hazards such as slipping and tripping.

### **4.3 PASEOS**

Existing City policy documents, including the Circulation Element of the City's General Plan, call for the protection and enhancement of the City's paseo system. Paseos have the potential to be attractive, well-designed, people-oriented places that provide desirable spaces for both the private and public sectors of the economy.

The public spaces (e.g. courtyards, plazas, and placitas) along paseo routes and the commercial activity within the paseos are nodes of activity where private business mixes with public uses. This rich mixture of activities is the key to the successful pedestrian environment in the grid.

Paseos confirm the intent of the City to develop a unique built environment, which contributes to Santa Barbara's economic prosperity. Paseos also enhance the City's network of pedestrian facilities and encourage walking, which can improve air quality, promote good health, and reduce noise throughout the grid. In addition, paseos can positively influence the commercial draw of the grid. Paseos support a larger mix of commercial activities by creating more opportunities for pedestrian access to exposed building frontages. The paseos themselves can become an attraction, drawing people to a commercial development and providing a more attractive atmosphere for building tenants.

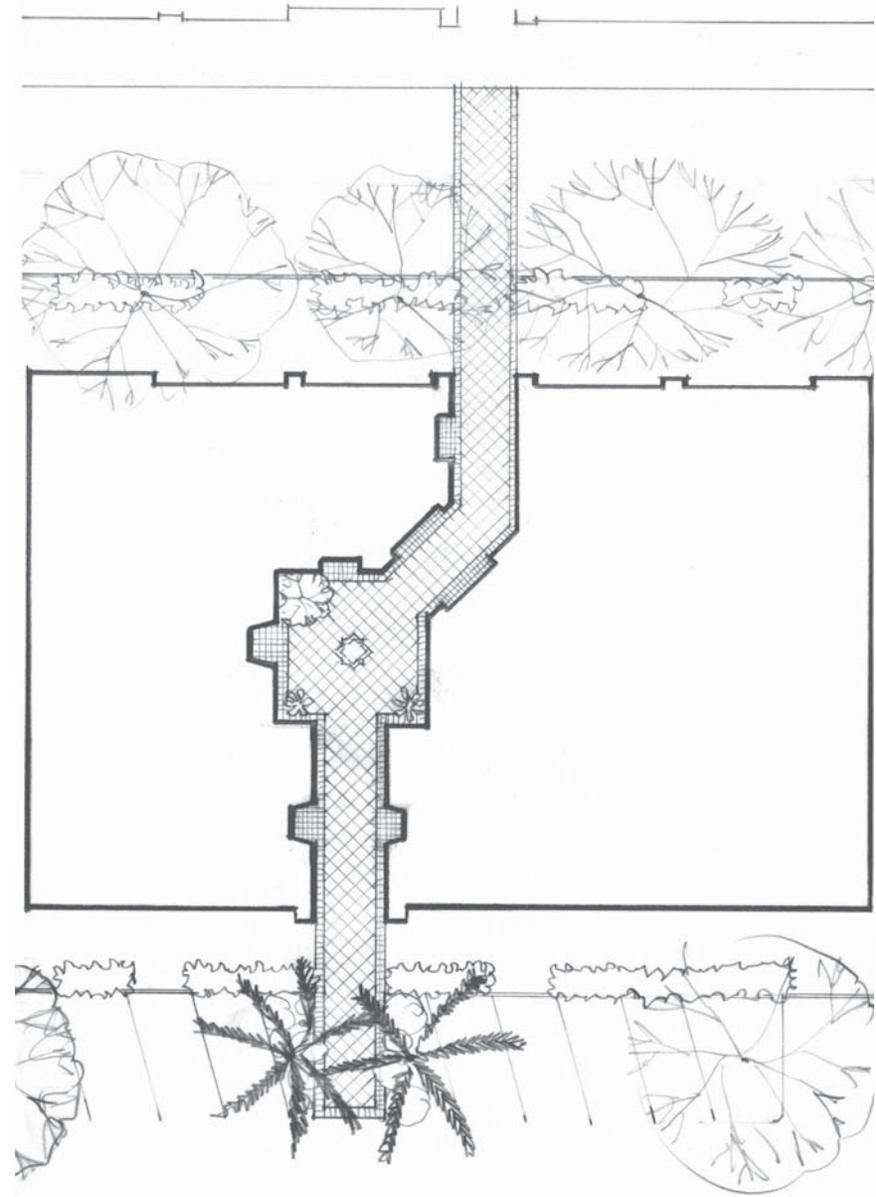
To encourage the protection and enhancement of the paseo system (including the development of new paseos), City staff, the design review boards, the Planning Commission, and the City Council will foster a spirit of cooperation with private developers by considering appropriate design flexibility.

The following guidelines apply specifically to paseos, in addition to the general guidelines outlined in section 4.1 of this document. Paseos, whether publicly or privately owned, shall be designed and maintained for general public use.

*The paseo at right connects an interior parking lot and the street frontage. A mid-block crosswalk continues the connection to another paseo across the street. The buildings along the paseo are articulated to provide pedestrian interest, and decorative paving materials have been used to provide visual continuity and draw people into the space. Reference Guidelines: 4.3.1, 4.3.4, and 4.3.8.*

## PLACEMENT/DESIGN

- 4.3.1 Paseos should be incorporated into new public or private developments where any of the following situations occur:
- ❖ A public area exists within the interior of a block that should be connected to the surrounding street frontage.
  - ❖ Pedestrians are required to walk out of their way to move between public areas on a block.
  - ❖ There is an opportunity to connect a new paseo to an existing paseo.
  - ❖ There is an opportunity to restore a paseo that has historically existed in a certain location.
- 4.3.2 Encourage paseos in the El Pueblo Viejo District wherever possible to enhance the Hispanic character of the area. Paseos are also encouraged wherever there are opportunities to make pedestrian connections between residential and commercial areas, educational facilities, and recreational facilities (e.g. Milpas Street and the Waterfront).



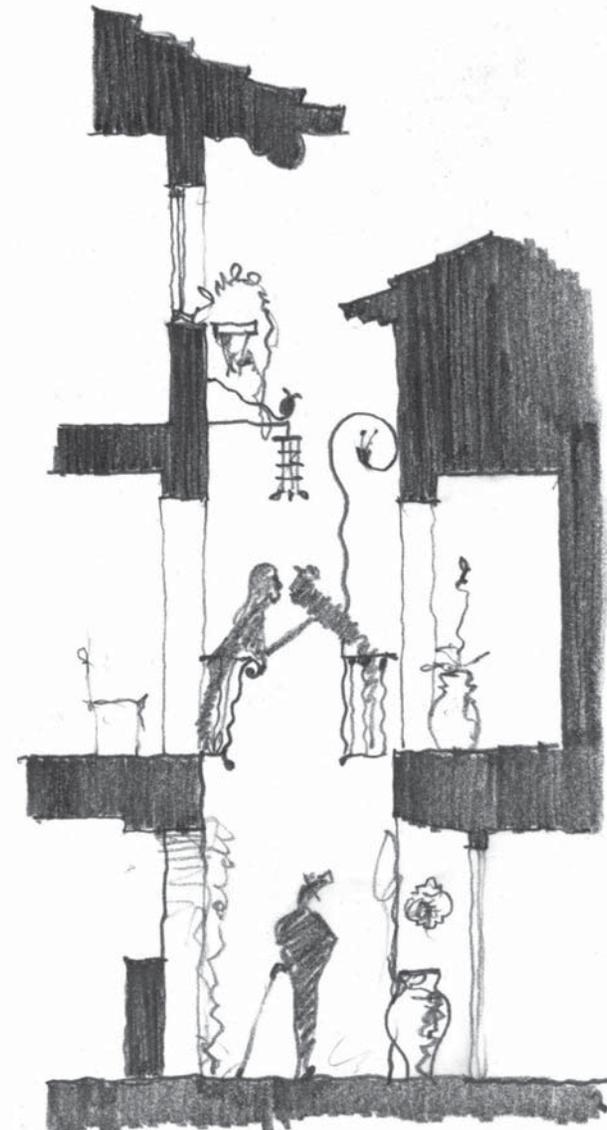
4.3.3 Paseos shall be inviting to pedestrians. The areas within a paseo should provide a varied spatial experience. Appropriate paseo dimensions will depend on a number of factors, including the following:

- ❖ The size, bulk, scale, and height of surrounding development;
- ❖ The level and type of pedestrian activity in the area; and
- ❖ Physical site constraints (e.g. a very small or narrow lot or existing natural features).

While pavement widths may vary, new paseos should be developed with a minimum width of ten feet (10') between building faces. However, a *callejon*, or narrow pedestrian street, may be an attractive alternative that provides variation in the scale of a paseo.

4.3.4 Encourage the development of mid-block crosswalks where streets intersect paseo routes. The placement of mid-block crosswalks shall be determined by the Public Works Department.

4.3.5 Other functions of paseos (e.g. merchandise delivery, trash collection, and fire access) shall be considered during the design and development review processes.



*A callejon, or narrow pedestrian street. Reference Guidelines: 4.3.3, 4.3.7-11.*

## PEDESTRIAN AMENITIES

- 4.3.6 Views of City landmarks (e.g. the Arlington tower, the Courthouse clock, and the Mission) should be created and/or maintained within paseos to provide locational reference points for pedestrians.
- 4.3.7 Maintain compatible architectural styles throughout the paseo to provide visual continuity for pedestrians.
- 4.3.8 Use architectural details to create a human scale environment within the paseo. Design building facades that back onto paseos to include pedestrian-friendly elements such as building entrances and display windows.
- 4.3.9 Use public art to provide visual interest for pedestrians.
- 4.3.10 Use decorative lighting that showcases adjacent building facades. All lighting fixtures shall conform to the City's Outdoor Lighting Design Guidelines.
- 4.3.11 Ensure that building projections and landscaping elements do not detract from the visibility of the paseo, create dark areas, or cause safety hazards.



*This drawing illustrates how views of landmark buildings or natural features can be captured within a paseo. Here, a human scale environment is achieved through the use of compatible architectural styles, pedestrian friendly elements, and landscaping. Reference Guidelines: 4.3.6-8.*

- 4.3.12 Place pedestrian oriented signage at paseo entrances that includes a map of the area surrounding the paseo and shows pedestrian connections to adjacent streets. Use signage throughout the paseo to make people aware of activities within and adjacent to the paseo. Signage shall be sensitive to the scale of the paseo, adding to its uniqueness and design appeal. All signs shall conform to the City's Sign Ordinance.

### LANDSCAPING

- 4.3.13 Integrate appropriate landscaping throughout the paseo and at its points of connection to other pedestrian facilities.

## 4.4 PATHWAYS

Incorporating pathways that complement the network of public sidewalks must be a priority when designing individual building sites. On-site pedestrian circulation and connections to adjacent sites must be considered with respect to the issues of privacy and security.

The following guidelines apply specifically to pathways, in addition to the general guidelines outlined in section 4.1 of this document.

### PLACEMENT/DESIGN

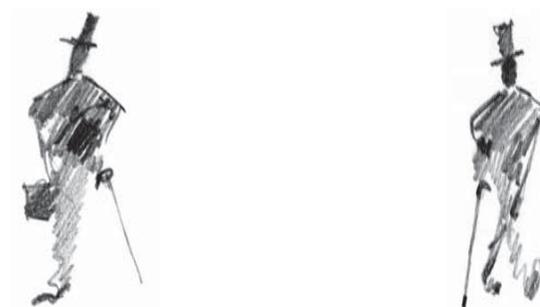
- 4.4.1 Establish direct pedestrian pathways between buildings to promote efficient on-site circulation.

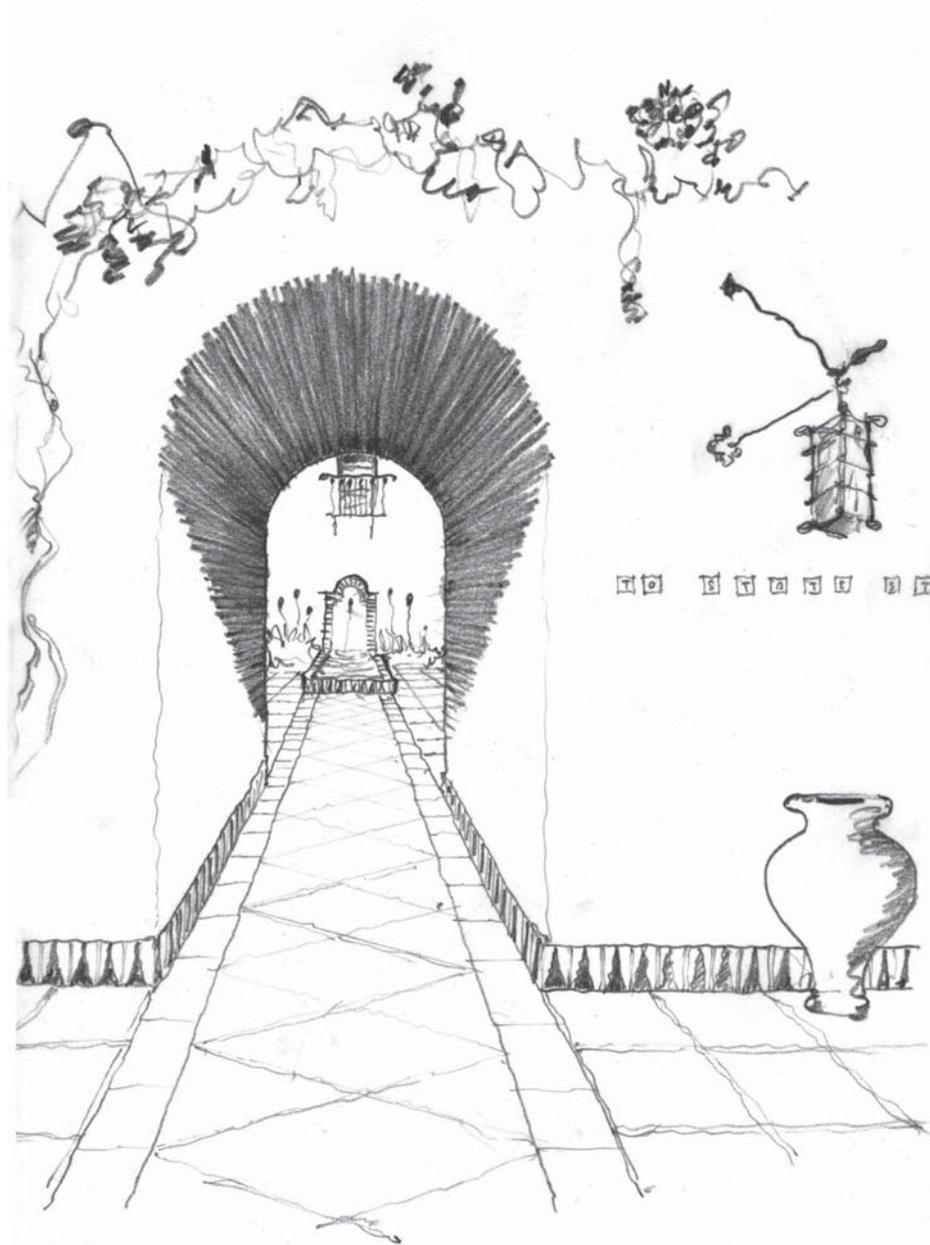
- 4.4.2 Look for opportunities to connect adjacent properties with pathways, where appropriate. Consider future site-to-site pedestrian connections so that they are not prevented by buildings, fences, or other permanent improvements.

## 4.5 LINKS TO TRANSIT AND BICYCLE FACILITIES

An important function of the network of pedestrian facilities is to link transit and bicycle facilities with a variety of destinations. Since every trip begins and ends with walking, effective pedestrian links increase the convenience of using transit and/or bicycles.

- 4.5.1 Where transit stops are in areas without sidewalks, extend sidewalks from the transit stops to the nearest improved sidewalk(s).
- 4.5.2 Provide transit route information in areas with high pedestrian volumes.
- 4.5.3 Connect pedestrian facilities with bicycle parking facilities to encourage bicyclists to park their bikes and walk to nearby destinations.





## CHAPTER 5: COURTYARDS, PLAZAS, AND PLACITAS

## 5. COURTYARDS, PLAZAS, AND PLACITAS

- ❖ *Encourage the provision of courtyards, plazas, and placitas throughout the City grid to create activity nodes, provide pedestrians with a more intimate gathering space away from the street, and maintain an inviting environment for pedestrians.*

### INTRODUCTION

Unlike a paseo, which is designed to provide pedestrian connections between destinations, courtyards, plazas, and placitas are spaces that allow people to congregate and interact away from the flow of pedestrian traffic. The different types of courtyards, plazas, and placitas can be categorized as follows:

**Public Spaces:** Public spaces are areas where the property is owned by a public agency and the public is allowed to enter and congregate. Examples include De la Guerra Plaza and Storke Placita.

**Semi-Public Spaces:** Semi-public spaces consist of areas where the public is allowed to enter and congregate but, unlike public spaces, are owned by a private interest. Examples include the La Arcada and El Paseo courtyards. *Note: Public access easements may be required by the Public Works Department for these types of spaces.*



*This plaza uses water and landscaping elements as a visual focal point. The orientation of the courtyard takes advantage of mountain views. Reference Guidelines: 5.2.1-5 and 5.2.7.*

**Private Spaces:** Private spaces are owned by a private interest for the use of adjacent building employees, tenants, or customers. Examples include the common areas of garden apartments or bungalow courts and restaurant dining courtyards. The design and intended function of such spaces may range from large public plazas that encourage active use, to small private courtyards that encourage quiet, passive reflection.

These guidelines are intended to be used as a menu of design options for courtyards, plazas, and placitas. The design and development review boards must consider the unique circumstances of individual projects when evaluating the location and design of these spaces.

## 5.1 LOCATION

- 5.1.1 Courtyards, plazas, and placitas shall be encouraged as components of new public and private developments to enhance the pedestrian experience in the City and provide connections to surrounding areas. The presence or absence of complementary pedestrian spaces in surrounding blocks should be considered when determining the appropriate location of a courtyard, plaza, or placita. The optimal placement of such a space will take advantage of proximity to or create views of landmark buildings or natural features.
- 5.1.2 Courtyards, plazas, placitas, or the paths leading to them should be connected to and perhaps be visible from the street, so that pedestrians will be encouraged to explore.
- 5.1.3 Consider the historic locations of courtyards, plazas, and placitas throughout the grid and look for opportunities to restore such spaces in their historic locations.
- 5.1.4 Encourage private interior courtyards or outdoor spaces in nonresidential or mixed-use developments for use by tenants, employees, or guests.
- 5.1.5 Encourage common outdoor areas in new multi-family developments for use by residents and their guests.

## 5.2 DESIGN AND LANDSCAPING

The design of courtyards, plazas, and placitas should provide interest and a sense of intrigue for pedestrians.

- 5.2.1 Use decorative paving materials that are reviewed and approved by the City to draw pedestrians down paths leading to interior courtyards.
- 5.2.2 Avoid blank walls and spaces without interest to pedestrians. Design buildings with several doors and windows that open into the pedestrian space and incorporate land uses that foster pedestrian activity (e.g. retail and residential uses).
- 5.2.3 Provide appropriate pedestrian amenities within courtyards, plazas, and placitas, taking into consideration the level of use, surrounding land uses, and existing amenities. See Chapter 4: *Pedestrian Facilities and Amenities* for more information regarding pedestrian amenities.
- 5.2.4 Use inviting landscape elements that provide shade, color, and texture. Landscape elements can be formal or informal (e.g. a proliferation of vines in a building recess) to reflect the overall character of the space.
- 5.2.5 Incorporate focal points into the design of courtyards, plazas, and placitas. Focal points may include sculptures, fountains, public art, architectural elements/features, or trees.



*This courtyard uses landscaping, decorative paving, and building details to provide visual interest for the seated pedestrian. The doors and windows opening onto the space encourage pedestrian activity. Reference Guidelines: 5.2.1-5.2.5 and 5.2.7.*

- 5.2.6 Provide sunny and shaded areas for seating. Shaded areas may be created using landscaping elements (e.g. canopy trees) or traditional Mediterranean architectural devices (e.g. arcades, colonnades, and pergolas).

- 5.2.7 Provide pedestrian-scale lighting that complements the surrounding built and natural environment. When determining the proper level of illumination for a courtyard, plaza, or placita, it is important to consider the quality of light versus the quantity of light. The lighting should be subtle and avoid overlighting while being bright enough to provide security and make the areas attractive for evening use. Consider a variety of lighting types, including footlighting, indirect lighting (wall washing), and overhead lamps. All lighting fixtures shall conform to the City's Outdoor Lighting Design Guidelines.

- 5.2.8 If private courtyards or outdoor spaces are to be secured at night, design the security device (e.g. the gate, wall, or fence) to minimize visual intrusion and complement the architecture and scale of surrounding buildings.

- 5.2.9 Drainage facilities for courtyards, plazas, and placitas shall be designed and located to minimize visibility and interference with pedestrian circulation.



**CHAPTER 6: TRANSIT STOPS**

## 6. TRANSIT STOPS

- ❖ *Design and locate transit stops to promote the increased use of transit, facilitate multi-modal travel, and reduce dependence on the automobile.*
- ❖ *Provide transit stops that are attractive, safe and convenient places in which to wait for a transit vehicle.*

### INTRODUCTION

Transit is an integral part of any balanced transportation network. In order to be a successful alternative to the automobile, transit service must be frequent, reliable, convenient, comfortable, and affordable. An effective transit system will not consist solely of a series of stops, but will also integrate complementary land uses and building design.

The guidelines in this chapter focus on designing and locating transit stops to enhance the convenience and comfort of transit. When designing a transit stop, early coordination with the various approving agencies is crucial. Any transit stop that is located in the public right-of way will require review and approval by the City Public Works Department, as well as design review approval from either the Architectural Board of Review or the Historic Landmarks Commission. The Metropolitan Transit District (MTD) must also be consulted to ensure that the stops will meet any applicable disabled access regulations and safely serve the different kinds of transit vehicles using the route.

For more information on disabled access regulations, vehicle setbacks, or other transit stop requirements, please refer to the MTD Passenger Accommodations Enhancement Plan.

### 6.1 LOCATION

- 6.1.1 Assessing appropriate locations for transit stops is an integral part of the development and design review process. Transit stops should be located to maximize convenience, provide pedestrian connections to nearby destinations, and be visible to potential users. Transit stops should not be located away from the public right-of way where they are not readily visible.

### 6.2 AMENITIES

- 6.2.1 When designing or improving a transit stop, there are certain amenities that must be provided for the stop to effectively accommodate transit passengers. These amenities are as follows:
- ❖ Bus stop markers/signs that are oriented to the pedestrian, rather than to passing vehicles;
  - ❖ Bus schedule and route map display areas;
  - ❖ Seating for transit passengers, placed so that waiting passengers are visible to the bus driver;
  - ❖ A shelter to shield passengers from the weather. An effective shelter can range from a canopy tree that provides shade to an architectural element with a solid top that protects passengers from sun, wind, and rain;
  - ❖ Pedestrian scale lighting to increase security and visibility for riders and transit operators;

- ❖ A trash container; and
- ❖ An improved hardscape surface that extends from the curb to the sidewalk. Hardscape surfaces may include paving materials other than poured concrete, as reviewed and approved by the applicable design review board and the Public Works Department. Where bus stops are located in areas without sidewalks, an improved hardscape surface shall be provided for passenger loading and unloading. The surface shall be large enough to accommodate both seated and standing passengers, extend to the street curb, and meet any applicable disabled access regulations. New sidewalk connections from the transit stop to the nearest improved sidewalk(s) shall be provided, whenever possible.

While these amenities should be present at each transit stop, the degree to which they are provided will vary based on several factors. These factors include:

- ❖ The space available for the stop;
- ❖ The location of the stop;
- ❖ The number of riders expected to use the stop; and
- ❖ The length of time passengers will spend at the stop.

For the purposes of these guidelines, transit stops are divided into three general categories, each of which has a list of suggested amenities. These categories can be used as a starting point for the design or upgrading of transit stops. However, the circumstances surrounding each

proposed stop will be different, and will need to be evaluated on a case-by-case basis. In addition, space constraints may prevent the inclusion of all of the amenities that are called for in the lists below.

**Minimalist Stop:** This type of stop is designed for minimal passenger use. This stop would generally be located on a route with low ridership (not a transfer point to other routes).



*This Minimalist Stop is recognizable to transit passengers, but is not an obtrusive presence on the street. The stop contains the following elements:*

- ❖ A transit stop sign and route map/schedule display area that are integrated into the design of the lamppost;
- ❖ A bench;
- ❖ A canopy tree to provide shelter from the sun;
- ❖ A trash container; and
- ❖ A decorative hardscape surface.

*Reference Guidelines: 6.2.1-2 and 6.3.1-3.*

**Neighborhood Stop:** This type of stop is designed for the higher density residential areas of the grid that surround the Downtown core. This stop would serve an active ridership that would be expected to either transfer busses or complete a portion of its trip on foot. Enhanced amenities, including increased bench space and route maps, are essential at these stops.



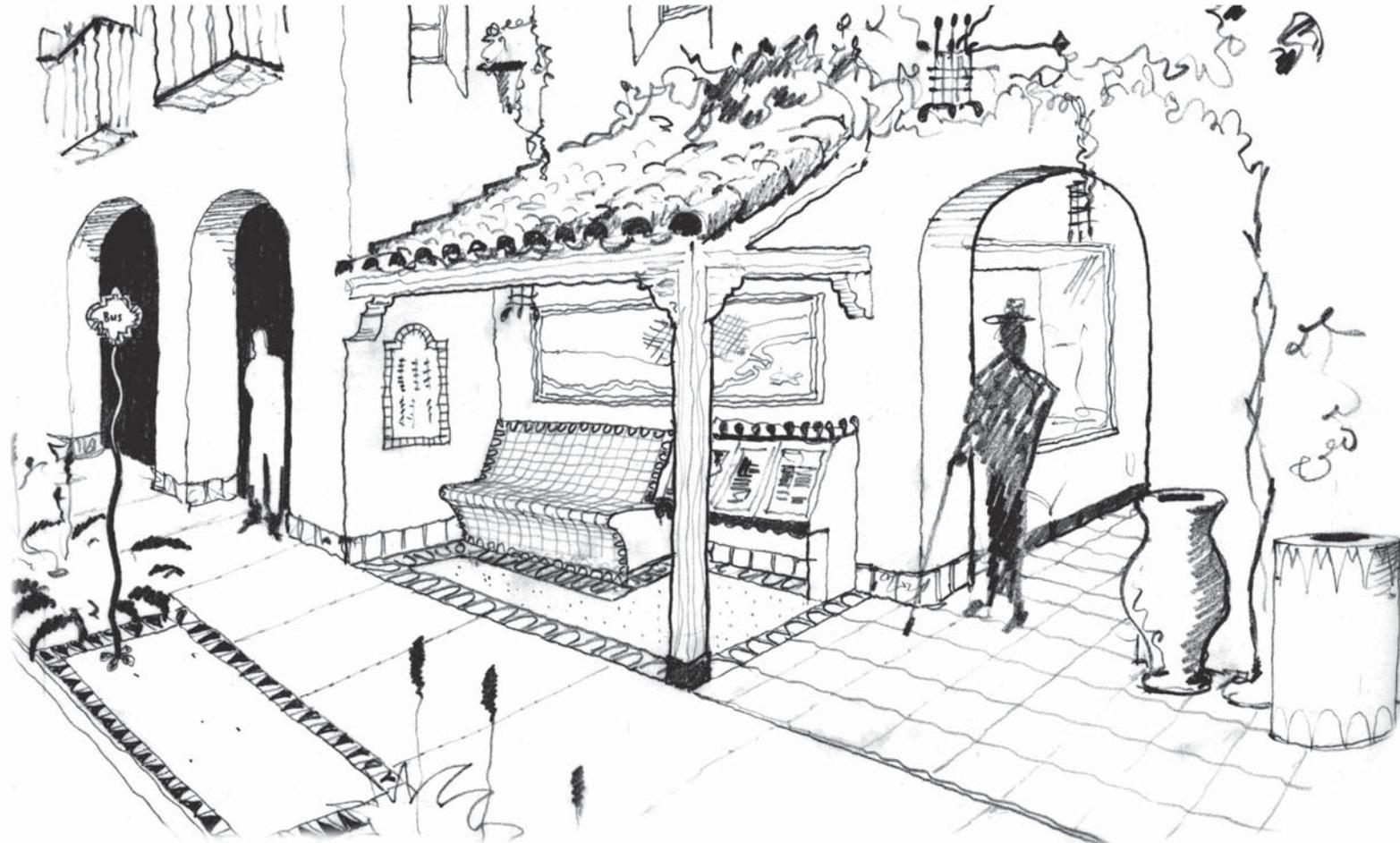
*This Neighborhood Stop contains the following elements:*

- ❖ *A transit stop sign and route map/schedule display area that are integrated into the design of the lamppost;*
- ❖ *A bench;*
- ❖ *A hard top shelter that blends with the surrounding architecture;*
- ❖ *A trash container; and*
- ❖ *A decorative hardscape surface.*

*Reference Guidelines: 6.2.1-2 and 6.3.1-3.*

**Commercial Stop:** This type of stop, located in the Downtown core, is designed for heavy passenger use by both residents and visitors. The higher volume of riders, the rate of visitor use, and the number of transfer passengers necessitate a high level of pedestrian amenities. Such amenities could include ample seating, increased route and schedule information, a larger sheltered area for passenger waiting, or other amenities listed in guideline 6.2.2. Commercial stops should be incorporated into the architecture of existing buildings, wherever possible (see reverse page for illustration).

In addition to the above stops, MTD's South Coast Transit Plan contains a conceptual description of stops called Pavilions. Pavilions would function as intermodal transportation centers within the Downtown core. Pavilions could potentially contain service elements for ticket sales, travel information, ATM machines, and landscaped plazas with benches, drinking fountains, newsstands, and other amenities. Should a Pavilion be proposed in the future, it would be reviewed and developed as a cooperative effort between MTD, City staff, and the appropriate development and design review boards.



*This example of a Commercial Stop is incorporated into the design of the adjacent building, and contains the following elements:*

- ❖ *A transit stop sign;*
- ❖ *Route map and schedule display areas;*
- ❖ *Enhanced signage showing nearby destinations/pedestrian amenities;*
- ❖ *A bench;*

- ❖ *A solid, hard top shelter;*
- ❖ *Lighting (incorporated into shelter design)*
- ❖ *A trash container;*
- ❖ *A newsrack; and*
- ❖ *Decorative hardscape surfaces.*

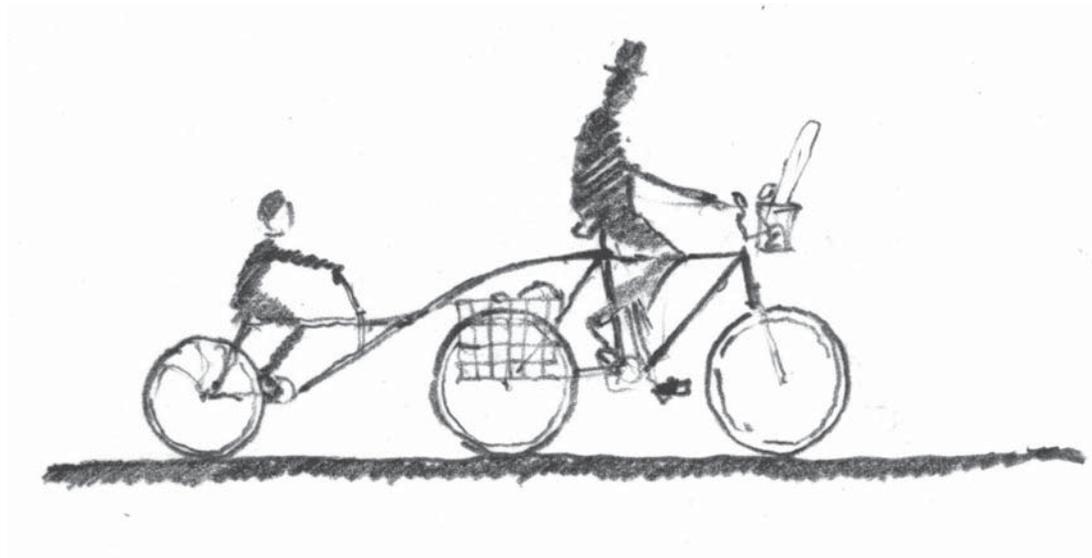
*Reference Guidelines: 6.2.1-2 and 6.3.1-3.*

- 6.2.2 Consider additional amenities to enhance the utility of transit stops, such as kiosks for information exchange, newsracks, clocks, recycling facilities, bicycle storage facilities (e.g. bicycle lockers), and enhanced signage showing the location of nearby destinations or amenities. The appropriateness of these amenities will depend on many factors, including the location of the transit stop, the level and hours of transit use, the composition of ridership, and the amenities of the transit vehicle (e.g. bicycle racks).
- 6.2.3 Ensure that transit facilities are designed for disabled access in compliance with the California Building Code.

### 6.3 DESIGN AND LANDSCAPING

- 6.3.1 The design of transit stops and related facilities should reflect the character of the surrounding neighborhood and complement the architectural styles of adjacent buildings and street furniture. The design of transit stops should be as simple as possible and not dominate the appearance of the streetscape.
- 6.3.2 The design of transit stops should incorporate unifying elements that make the stops recognizable to the public. Such elements might include enhanced sign posts or bench designs. When designing a transit stop in the grid, consider the design of stops on surrounding blocks.
- 6.3.3 Use landscaping to give the transit stop a park-like feeling without reducing clear access to the transit vehicle.





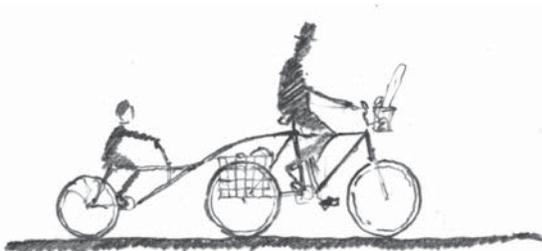
## CHAPTER 7: BICYCLE FACILITIES

## 7. BICYCLE FACILITIES

- ❖ *Provide bicycle facilities throughout the City grid to make bicycling a more viable and convenient mode of transportation.*
- ❖ *Design and locate bicycle facilities, both on private property and in the public right-of-way, to enhance the City's transportation network and facilitate multi-modal travel.*

### INTRODUCTION

Increasing bicycling as a mode of transportation serves a number of the City's transportation and planning goals, including decreasing dependence on the automobile, reducing overall parking demand, reducing traffic congestion, and decreasing levels of air pollution and noise. As with transit, the viability of bicycling depends on the consistent provision of bicycle facilities (including both parking and storage areas). The guidelines in this chapter focus on the optimal provision, location, and design of bicycle facilities.

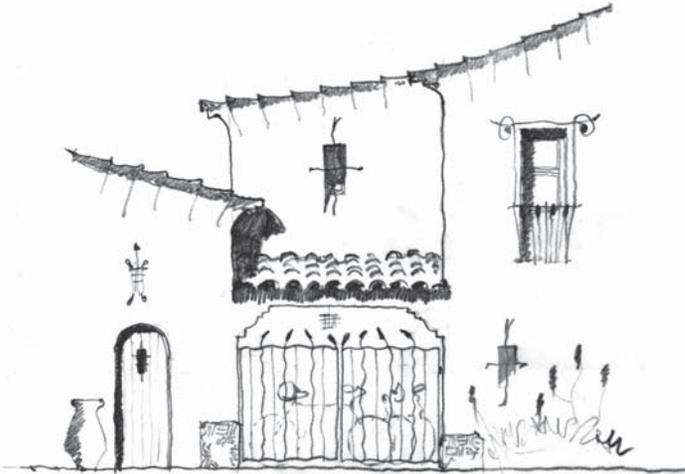


### 7.1 PROVISION OF BICYCLE FACILITIES

- 7.1.1 Bicycle parking and storage shall be provided for new development as required in Chapter 28.90 of the Municipal Code. Bicycle parking and storage for new developments in Parking Zones of Benefit are also encouraged.

### 7.2 LOCATION

- 7.2.1 Look for opportunities to provide short term public bicycle parking throughout the City grid. The placement of bicycle parking shall comply with the City of Santa Barbara Access and Parking Design Guidelines. The optimal placement of new bicycle parking facilities will take into account pedestrian circulation patterns and the location of existing facilities, and reduce the visual impacts associated with parked bicycles. Short term public bicycle parking should be visible from the street and nearby buildings to provide a measure of security and prevent theft.
- 7.2.2 Long term bicycle parking for employees and nonresidential tenants should be both easily identified and secure so that people can easily find the parking and be encouraged to ride. This may be achieved using a locked room, a bicycle corral (outdoor covered area enclosed by a fence with a locked gate), or bicycle lockers. Bicycle parking areas should be designed specifically for bicycle use, and shall not be converted for other uses.



*The design of this building incorporates a covered bicycle parking/storage facility. Reference Guidelines: 7.2.2, 7.3.1-2 and 7.3.5.*

- 7.2.3 Long term bicycle parking for residential buildings shall be covered and located away from the public right-of-way for security purposes.
- 7.2.4 Bicycle facilities should be provided where there are opportunities for pedestrian, transit, and train connections. Consider placing bicycle facilities near courtyard, plaza, and placita entrances, and in locations where riders can park their bicycles and walk to multiple destinations.

## 7.3 DESIGN

- 7.3.1 Long term bicycle parking and storage should be weather protected, whenever possible.
- 7.3.2 The design of bicycle facilities (including bicycle lockers) shall reflect the character of the surrounding neighborhood and complement the architectural styles of adjacent buildings and street furniture, without overpowering the streetscape. Such facilities shall be consistent with existing City design standards and guidelines.
- 7.3.3 Short term bicycle parking in the public right-of-way should be in the form of City standard hitching posts or other devices reviewed and approved by the Public Works Department. Substandard bicycle racks in public areas should be replaced with hitching posts or other approved devices, whenever possible.
- 7.3.4 Bicycle lockers should be clearly identifiable, incorporating written and/or pictorial signage.
- 7.3.5 Bicycle facilities should be well lit for night use, pursuant to the Outdoor Lighting Design Guidelines.



## CHAPTER 8: AUTOMOBILE PARKING FACILITIES

## 8. AUTOMOBILE PARKING FACILITIES

- ❖ *Design automobile parking facilities to reduce the visibility of automobiles and allow features of greater pedestrian interest to dominate the streetscape.*
- ❖ *Design automobile parking facilities to confer priority on the safety and convenience of pedestrians and encourage pedestrian activity on the street.*
- ❖ *Create links between automobile parking facilities and other transportation facilities to increase the convenience of walking, bicycling, and transit.*

### INTRODUCTION

Managing the growth of automobile parking facilities in the City poses a major challenge to planners and developers. Traditionally, Santa Barbara has developed and maintained a quality built environment within the grid, whose pedestrian orientation and aesthetic qualities are exemplary. However, the increasing numbers of automobile oriented developments threaten to rob the grid of its unique character, changing balanced and active streetscapes into spaces dominated by the automobile.

Automobile parking facilities can negatively impact the pedestrian orientation of the streetscape in the following specific ways:

- ❖ Automobile parking facilities can dominate the streetscape and detract from the attractiveness, interest, and individual character of business and residential areas. Poorly designed parking facilities are visually unappealing and create dead spaces in otherwise active streetscapes; and
- ❖ Automobile parking facilities can be significant impediments to pedestrian travel. Poorly located parking lots that separate buildings from sidewalks and other buildings interrupt the continuity of the pedestrian landscape by increasing the distance that pedestrians must travel from streets to building entrances, or from one building to another. In many cases, the most direct pedestrian routes are through the parking lots, making pedestrians uncomfortable and increasing the potential for conflicts. When pedestrians are forced to pass or traverse expanses of asphalt filled with cars, all reflecting heat and sunlight, the experience of walking is diminished.

These circumstances combine to affect the transportation choices that people make. Automobile parking facilities that fail to address pedestrian needs send the clear message that the automobile should be accommodated over the safety and convenience of pedestrians. As long as this continues to be the case, people will drive even for short trips, rather than walk.

In order to preserve and enhance the pedestrian character of the grid, every effort must be made to design automobile parking facilities so that they contribute to, rather than detract from, the pedestrian life of the street.

The guidelines in this chapter will address the location and design of two kinds of automobile parking facilities – **surface parking lots** and **parking structures**. The following guidelines should be used in addition to the City of Santa Barbara Access and Parking Design Guidelines and the existing Zoning Ordinance requirements regarding the provision, design, and landscaping of public and private parking facilities.

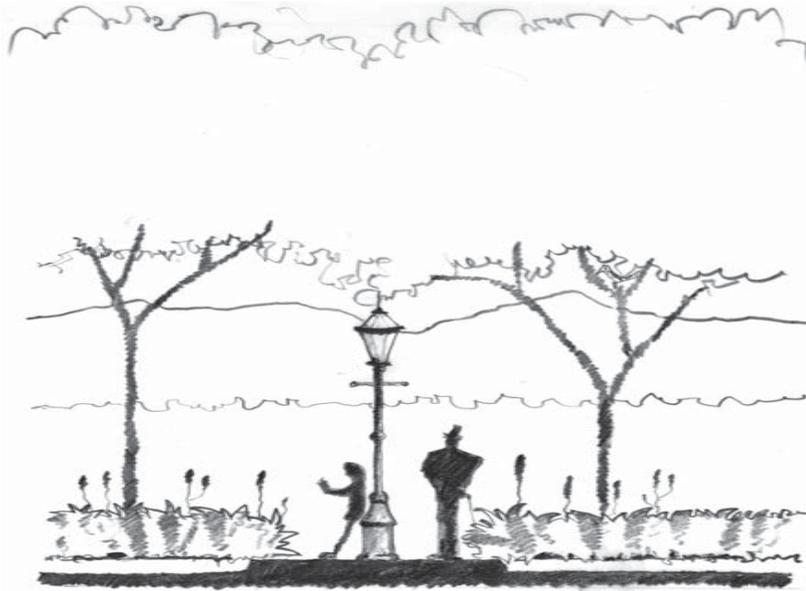
## 8.1 SURFACE PARKING LOTS

### LOCATION

- 8.1.1 Locate surface parking lots away from the street edge to minimize visual effects on the streetscape. Surface parking lots should be located behind habitable buildings and toward the interior of blocks.
- 8.1.2 If a surface parking lot must abut a street edge, minimize the amount of street frontage devoted to the lot by locating the lot to the side of the building, rather than between the building and the street.
- 8.1.3 Surface parking lots shall not abut a street intersection or terminate a major street vista.

### PEDESTRIAN AND AUTOMOBILE ACCESS

- 8.1.4 Shared parking between developments is encouraged to reduce the amount of space that must be devoted to surface parking lots.
- 8.1.5 Provide direct pedestrian access to building entrances from each exposed street frontage and/or adjacent pedestrian facility. Pedestrians entering from the sidewalk shall not be required to walk through a surface parking lot to enter a building. Where multiple buildings have rear entrances onto a shared parking lot, provide a pedestrian pathway that connects the entrances to the sidewalk or other adjacent pedestrian facility.
- 8.1.6 Avoid conflicts with pedestrian traffic by minimizing the width and number of curb cuts on main streets.
- 8.1.7 Where possible, use alleys or side streets for automobile access to surface parking lots. The use of alleys for parking access must be balanced with other customary functions of alleys, including service, utility, and loading/unloading areas. As an alternative to abandonment, consider retaining alleys for automobile parking or access.
- 8.1.8 Where surface parking lots must have automobile access from a main street, look for opportunities to share existing driveways with adjacent developments.



*In the parking lot at left, the light standard is accommodated beneath the mature tree canopies. Reference Guidelines: 8.1.11-13.*

## DESIGN AND LANDSCAPING

- 8.1.9 Surface parking lots and adjacent sidewalks shall be designed so that pedestrians are clearly visible to emerging automobiles. Incorporate design features such as articulated pavement, grade separation, and signage to clearly confer the right-of-way to pedestrians at points of conflict.
- 8.1.10 Design surface parking lots with multiple points of pedestrian access to exposed street frontage and/or adjacent pedestrian facilities. Use distinctive, City reviewed and approved paving materials, landscaping, lighting, and signage to clearly delineate pedestrian access routes within parking lots. To encourage pedestrian use, pedestrian access routes should be more visible, attractive, and convenient than a route through the parking stalls.
- 8.1.11 At a minimum, provide City reviewed and approved landscaping in the interior of surface parking lots consistent with the requirements of the Zoning Ordinance. In order to have any part of the ordinance requirements waived by a design review board, the applicant must demonstrate that compensating landscaped features will be present elsewhere in the parking lot.
- 8.1.12 Use canopy trees in the interior of surface parking lots to provide shade and greenery. For the purposes of these guidelines, palm trees shall not be considered canopy trees. Minimum spacing requirements for planting specific tree species can be obtained from the City Arborist. Mature tree canopies should have a vertical clearance of fifteen feet (15') in order to accommodate lighting fixtures (see 8.1.13 below).
- 8.1.13 The lighting of surface parking lots shall comply with the Outdoor Lighting Design Guidelines. Parking lot lighting shall be planned in conjunction with landscaping improvements for maximum utility. Lighting fixtures should be lower than the mature tree canopies.

8.1.14 Where a surface parking lot abuts a street edge, create a visual buffer to minimize the impact of parked cars and provide interest for pedestrians. Any improvements in the public right-of-way require the approval of the Public Works Department. At a minimum, landscaping and a screen wall shall be provided consistent with the Zoning Ordinance requirements. In addition, consider the following decorative and functional elements to further screen the parking:

- ❖ Increase the height of the screen wall where it will not cause a safety hazard for pedestrians or drivers;
- ❖ Create small plaza spaces along the street frontage, incorporating focal points such as fountains or public art. Include seating areas and other pedestrian amenities as appropriate. Where parking lots abut street intersections, consider incorporating other land uses (e.g. small flower shops) to anchor the corners;
- ❖ Create a gathering space around an adjacent transit stop; or
- ❖ Visually anchor the parking lot to the public right-of-way with strong architectural elements, such as arbors or pergolas.

8.1.15 Where disabled parking spaces must be located in front of buildings, the parking spaces shall be paved with decorative materials and landscaped to resemble open space areas.



*A low wall, landscaping, and canopy trees are used in this surface parking lot to minimize the visual effects of automobiles on the streetscape. Reference Guidelines: 8.1.11-12 and 8.1.14.*

## 8.2 PARKING STRUCTURES

For the purposes of these guidelines, parking structures include underground parking, parking on the ground floor of a multi-story building, and carports or garages that are associated with multiple-family residential developments. The identification and security of parking structures must be strongly considered during their design so that they can be readily identified by the public and the public will feel safe in using them.

**LOCATION**

- 8.2.1 Locate parking structures away from the street edge to minimize visual effects on the streetscape. This may be accomplished using one or more of the following techniques:
- ❖ Locate parking structures behind habitable buildings and toward the interior of blocks;
  - ❖ Design buildings so that the active use portion of the building faces the street and wraps around an interior parking structure; or
  - ❖ Place the parking underground.
- 8.2.2 If a parking structure must abut a street edge, minimize the amount of street frontage devoted to the structure by placing its shortest dimension(s) along the street edge.

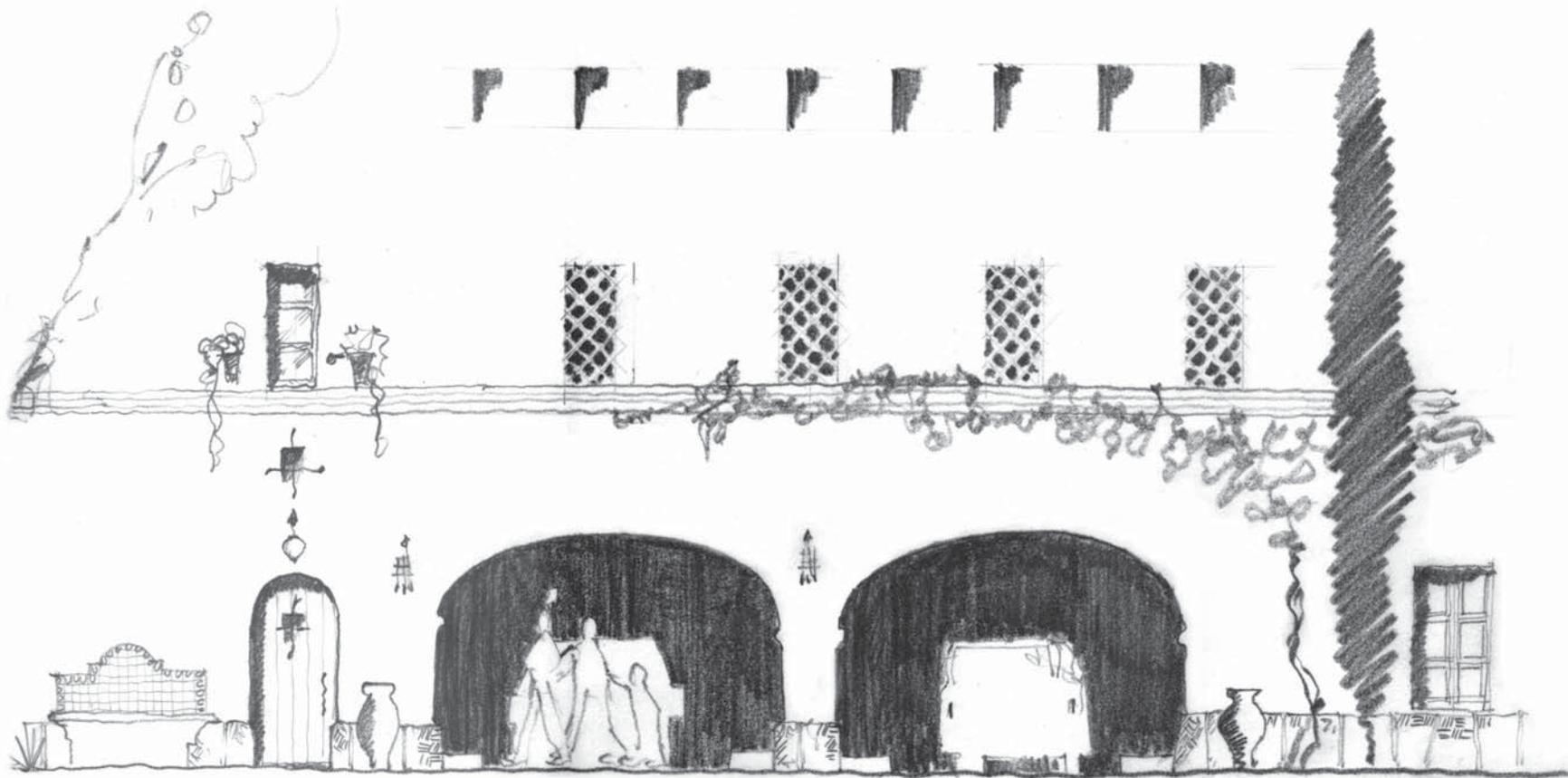
**PEDESTRIAN AND AUTOMOBILE ACCESS**

- 8.2.3 Avoid conflicts with pedestrian traffic by minimizing the width and number of curb cuts on main streets.
- 8.2.4 Where possible, use alleys or side streets for automobile access to parking structures. The use of alleys for parking access must be balanced with other customary functions of alleys, including service, utility, and loading/unloading areas. As an alternative to abandonment, consider retaining alleys for automobile parking or access.

- 8.2.5 Where parking structures must have automobile access from a main street, look for opportunities to share existing driveways with adjacent developments.
- 8.2.6 Parking structures and adjacent sidewalks shall be designed so that pedestrians are clearly visible to emerging automobiles. Incorporate design features such as pavement articulation, grade separation, and signage to clearly confer the right-of-way to pedestrians at points of conflict.
- 8.2.7 Design parking structures with multiple points of pedestrian access to adjacent pedestrian facilities. Use distinctive, City reviewed and approved paving materials, lighting, and signage to clearly delineate pedestrian access routes within parking structures.

**DESIGN**

- 8.2.8 Parking structures should be designed to be compatible with both adjacent developments and the overall neighborhood. Please see Chapter 1: *Compatibility of New Development with the Existing Environment*, for a discussion of how new parking structures can be made compatible with their surroundings.
- 8.2.9 Design and articulate the exposed portions of parking structures to suggest the appearance of a habitable building. Simplicity of design is encouraged. Provide other visual enhancements (e.g. public art) for pedestrians.



*While this parking structure abuts the street edge, it has many features that minimize its visual effects on the streetscape:*

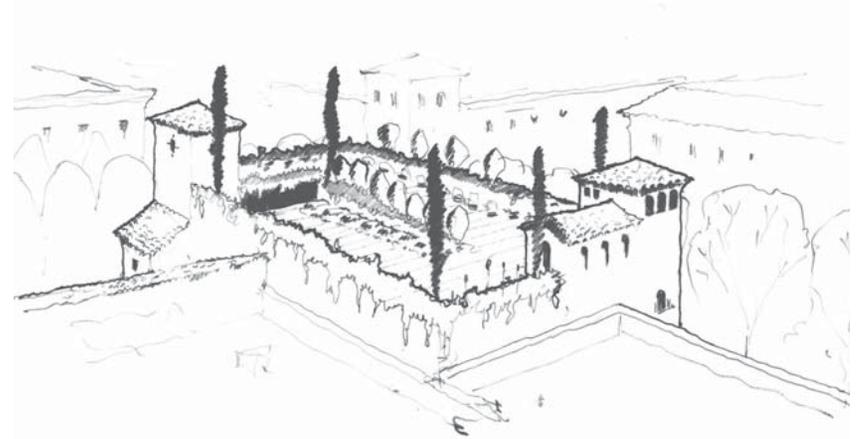
- ❖ *The width and number of curb cuts have been minimized;*
- ❖ *The structure is designed and articulated to resemble a habitable building;*
- ❖ *The structure is compatible with the architectural styles of existing surrounding developments;*

- ❖ *Other land uses have been incorporated along the ground floor of the structure;*
- ❖ *The width and height of ingress and egress openings have been minimized; and*
- ❖ *“Windows” in the parking structure are screened to prevent light spillage at night.*

*Reference Guidelines: 8.2.3, 8.2.8-10, and 8.2.12-13.*

*Landscaping and architectural elements soften views of this parking structure from higher elevations and allow it to blend with adjacent rooftops. Reference Guideline: 8.2.14.*

- 8.2.10 Incorporate land uses along the ground level street frontage that encourage pedestrian activity. Such uses could include retail, entertainment, office, restaurant, or residential uses. Transit stops and public restrooms could also be incorporated. If the incorporation of other land uses is not possible, provide a landscaped patio space between the structure and the street.
- 8.2.11 Incorporate the above design elements in parking structures adjacent to paseos or other public spaces. The parking structure should contribute to, not detract from, the pedestrian oriented character of such spaces.
- 8.2.12 Minimize the width and height of openings for automobile ingress and egress (openings must be designed for disabled access in compliance with the California Building Code). Use architectural elements to divide the openings, minimize the appearance of structure utilities (e.g. ducts, vents, pipes, and lighting), and reduce other visual impacts on the streetscape.
- 8.2.13 Where cars are exposed to the street via “windows” in the parking structure, use screening elements to reduce the visual effects of parked cars and light spillage at night. Screening treatments could include tracery, ironwork, or landscaping.



- 8.2.14 Design the top level of parking structures to blend with adjacent rooftops and minimize light and glare. The following elements should be incorporated:
- ❖ Use landscaping to soften views of the structure from higher elevations and provide shade;
  - ❖ Use architectural elements that provide visual interest (e.g. trellises and towers);
  - ❖ Choose hardscape colors that blend with adjacent rooftops; and
  - ❖ Provide lighting at the lowest illumination level possible, consistent with IES standards. All lighting must also conform to the Outdoor Lighting Design Guidelines. Indirect lighting should be used to minimize light spillage.

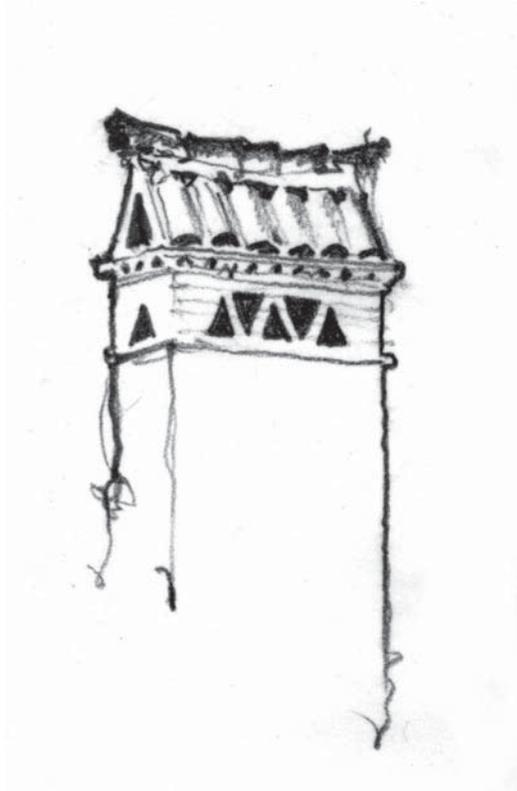
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## **8.3 SIGNAGE**

- 8.3.1 Provide signage in public parking lots and structures to orient users to the Downtown area and show nearby transportation facilities (e.g. “You Are Here”). The signage shall not be an advertisement for adjacent businesses. Similar signage is also encouraged for private parking structures.
- 8.3.2 Provide address identification at the entrances to private parking lots and structures.

## **8.4 LINKS TO OTHER TRANSPORTATION FACILITIES**

- 8.4.1 Locate bicycle parking and storage facilities within parking structures, where appropriate.
- 8.4.2 Integrate transit stops into the design of large parking structures, where appropriate.



## CHAPTER 9: BUILDING EQUIPMENT AND SERVICE AREAS

## 9. BUILDING EQUIPMENT AND SERVICE AREAS

- ❖ *Design and locate building equipment and service areas so that they do not dominate the appearance of the site or interfere with pedestrian or vehicular circulation.*

### INTRODUCTION

The location and design of building equipment and service areas is important to the overall appearance of a site. Proper location will minimize unattractive views from adjacent properties, the street, and hillside areas, and minimize potential conflicts with pedestrians. Good design will allow the equipment and service areas to present the best possible appearance within the limits of their function. The location and design of building equipment and service areas should be studied early in the design process to facilitate the review of the project.

Building equipment and service areas include, but are not limited to, rooftop equipment, mechanical and electrical equipment and conduits, HVAC ducts and piping, fire equipment, water backflow devices (e.g. fire protection, landscape, and domestic water systems), trash facilities, recycling facilities, utilities, satellite dishes, antennas, loading/unloading areas, and site building and drainage facilities. For the purposes of these guidelines, drainage facilities shall not include creeks or other natural watercourses.

### 9.1 LOCATION

- 9.1.1 Design buildings to accommodate all necessary building equipment. Initial site plans and elevation drawings that are submitted to the design review boards shall fully show the location of existing and proposed utilities.
- 9.1.2 Locate service and equipment areas to minimize visibility from public spaces, including streets, hillsides, and neighboring properties. Combine equipment (e.g. vents) where possible to reduce visibility.
- 9.1.3 Locate loading/unloading and utility areas to minimize conflicts with pedestrian and vehicular circulation.
- 9.1.4 On sites served by an alley or street with low traffic, locate all facilities such as service or delivery entrances, loading/unloading areas, and trash collection facilities for access from that alley or street. Other functions of the alley or street, such as parking or pedestrian access, shall be considered when deciding on the appropriate placement of service and delivery entrances.
- 9.1.5 In the event of a conflict, the convenience of a utility or delivery provider shall not take precedence over the safety or circulation needs of pedestrians or the aesthetics of the street.



*Trash and recycling containers are screened in a manner that complements the architecture of adjacent buildings.*

## 9.2 DESIGN AND LANDSCAPING

- 9.2.1 Building equipment shall be as small as possible to reduce visibility.
- 9.2.2 Incorporate building equipment into the design of the building by placing utilities in wall or roof recesses to reduce visibility from public areas. Exposed and surface mounted utilities are not desirable.

- 9.2.3 If building equipment and service areas must be visible from the street or neighboring properties, screen them from public view with a City reviewed and approved material or dense landscaping.
- 9.2.4 Solar collectors, satellite dishes, communications equipment, and other rooftop equipment should be located out of view from public streets and neighboring properties. Visible equipment shall be constructed of non-reflective material and screened to the greatest extent feasible.
- 9.2.5 Screening materials and detailing should be chosen to blend with and complement the architectural style of the existing building(s) on site.
- 9.2.6 Roof equipment should be completely concealed within the roof structure to avoid visibility from hillside areas of the City. Where roof equipment is visible, consider the following screening methods:
- ❖ Use a parapet wall or roof equipment well.
  - ❖ Paint roof equipment the same color as the roof so that it will disappear when viewed from hillside areas of the City.
  - ❖ If a vent pipe or other equipment will extend above the roofline and be visible from adjacent areas, creatively incorporate it into the design, (e.g. combining pipes into a false chimney structure or adding caps to vents).

## **GLOSSARY AND REFERENCES**

## GLOSSARY

### **ARCADE**

A wall composed of arches and their supporting columns.

### **ARCHITECTURAL BOARD OF REVIEW (ABR)**

A nine member committee established by the City Charter and appointed by the City Council to review and approve, conditionally approve or deny projects according to ordinance and based on guidelines related to design.

### **ARTICULATE**

To give character or interest; to define.

### **BICYCLE FACILITY**

Any bicycle related structure, such as a bicycle parking area or bicycle path, that is designed to improve or encourage bicycle use.

### **BICYCLE PARKING**

A facility or piece of equipment designed to hold and lock a bicycle.

### **BUILDING**

Any structure having a roof supported by columns or walls for the shelter, housing or enclosure of persons, animals, or property of any kind.

### **BUILDING COMPOSITION**

The putting together of various design elements to produce the building form.

### **BUILDING EQUIPMENT AND SERVICE AREAS**

Any equipment or services intrinsic to the function of a building. These include, but are not limited to, rooftop equipment, mechanical and electrical equipment and conduits, site building and drainage facilities, HVAC ducts and piping, fire equipment, water backflow devices, trash facilities, recycling facilities, utilities, satellite dishes, antennas, and loading/unloading areas.

### **BUILDING HEIGHT**

The maximum vertical height of a building or structure at all points measured from natural grade. Architectural elements that do not add floor area to a building, such as chimneys, vents, antennae, and towers, are not considered a part of the height of a building, but all portions of the roof are included.

### **BUILDING ORIENTATION**

The placement of a building, structure, or object on a site in relation to natural features, property lines, and other parts of the built environment (e.g. the street).

### **BUILT ENVIRONMENT**

Any buildings, structures, infrastructure, improvements or other man-made development in an area.

### **BULK**

The dimensional volume of a building.

### **CALLEJON**

A narrow pedestrian street.

### **CANOPY TREE**

A tree that has significant upper mass and provides shade, and whose mature tree canopy is greater than or equal to twenty feet (20') in diameter.

**CANTILEVER**

A beam or truss with an unsupported end projecting past the bearing; may support any kind of projecting element including a building overhang or balcony.

**CHARACTER**

The nature or “personality” of a building or area.

**CIRCULATION SYSTEM**

A network of roads, sidewalks, bikeways, and other paths used for travel.

**CITY GRID**

See Grid.

**COMMERCIAL STOP**

A transit stop that is located in the Downtown core and designed for heavy passenger use by both residents and visitors.

**COMPATIBILITY**

Presentation of a harmonious character between new developments and adjacent structures or the surrounding neighborhood.

**CORNICE**

A molded projection at the top of a wall or column.

**DESIGN REVIEW BOARD**

The Architectural Board of Review or the Historic Landmarks Commission.

**DETAILING**

The use of small features or elements to give character or definition to a space or building.

**DEVELOPMENT**

Any buildings, structures, infrastructure, landscaping or other man-made or natural elements associated with the improvement of a site.

**DISTRICT**

An area with designated boundaries, set apart by some inherent characteristic or designated use.

**DOWNTOWN CORE**

Otherwise known as the Central Business District (CBD), the area roughly bounded by Arrellaga Street to the north, Garden Street to the east, Highway 101 to the south, and De la Vina St. to the west (*Source: Santa Barbara Municipal Code Section 28.90.100*).

**EASEMENT**

A right, such as a right-of-way, afforded to a person or entity to make use of another person’s real property.

**ELEVATIONS**

Drawings to scale that show the appearance of the exterior of a building.

**EL PUEBLO VIEJO DISTRICT**

A district intended to preserve and enhance Santa Barbara’s historic architectural character. All new buildings and exterior changes to existing buildings in the district must be designed to be compatible with one of several Hispanic architectural styles, as defined in the Historic Structures Ordinance (S.B.M.C. Chapter 22.22).

**FORM**

The shape of a building.

**FRONTAGE**

The part of a lot that forms an edge with the street.

**GRID**

Also referred to as the City grid, the area roughly bounded by Portesuello Avenue and Alamar and Constance Streets on the north, a line following Constance Street, Olive Avenue, Milpas Street, Alameda Padre Serra, and Salinas Street on the east, Cabrillo Boulevard on the south, and a line following the eastern slope of the Mesa Hills to Clearview Road on the west.

**HARDSCAPE**

In landscape architecture, the non-living components of the design, especially walls, walks, overhead structures, stones, benches, and similar objects.

**HILLSIDE DESIGN DISTRICT**

An area defined by S.B.M.C. Section 22.68.110.A.2 that is generally greater than 20% in slope and subject to review by the ABR.

**HISTORIC LANDMARKS COMMISSION (HLC)**

A nine member committee established by the City Charter and appointed by the City Council to review and approve, conditionally approve or deny projects according to ordinance and based on the El Pueblo Viejo Design Guidelines.

**HUMAN SCALE**

A scale that is comfortable and relates proportionately to human size.

**IMPROVEMENTS**

Street work, flood and drainage work, utilities and other desirable facilities to be installed or agreed to be installed for public or private streets.

**INFILL DEVELOPMENT**

New development on scattered vacant or underdeveloped sites in a built up area.

**INFRASTRUCTURE**

Improvements or structures, such as streets, water pipes, or storm drains, bicycle facilities, transportation facilities, or other public right-of-way improvements that are typically intended to serve the public.

**LANDSCAPING**

Planted areas or areas that are covered with permeable material. The placement of trees, shrubs, vegetative and organic or inorganic materials in a prescribed area. Organic and inorganic materials include gravel, cinders, and rock and bark materials.

**MASSING**

The appearance of heaviness or weight, or lack thereof.

**METROPOLITAN TRANSIT DISTRICT (MTD)**

The designated authority for transit services in the South Coast, and the agency authorized to receive transit funding from state and federal sources. For the purposes of this document, references to MTD shall include all transit providers within the City grid.

**MINIMALIST STOP**

A transit stop that is designed for minimal passenger use. This type of stop would generally be located on a route with low ridership and would not be a transfer point to other routes.

**MOLDING**

A raised bump or projection used to add interest to an otherwise flat or insignificant surface; often used at intersections of planes (walls, floors, ceilings) and around openings.

**MULTI-MODAL TRANSPORTATION**

Transportation that involves connections between various forms of travel. For example: bike to train.

**NEIGHBORHOOD**

Properties located in close proximity to each other with land uses that sometimes share physical similarities or distinctive characteristics.

**NEIGHBORHOOD STOP**

A transit stop that is designed for the higher density residential areas of the grid that surround the Downtown core. This type of stop would serve an active ridership that would be expected to either transfer busses or complete a portion of its trip on foot.

**ORDINANCE**

A municipal regulation or law that is passed by the City Council.

**OUTDOOR LIGHTING**

The night time illumination of an outside area or object, or any man-made light emitting object located outdoors.

**PARAPET**

A wall that rises above a flat roof.

**PARKWAY**

Any of the following:

- ❖ The area between the curb and the sidewalk within a fully improved street right-of-way;
- ❖ That area extending six feet from the curb towards the nearest right-of-way line in an area with no sidewalk; or
- ❖ Any area within a street right-of-way in which an official or parkway tree is located. *(Source: Santa Barbara Municipal Code)*

**PASEO**

A connecting walkway that joins streets, open plazas, courtyards, cafes, and shops through the central portions of City blocks. A paseo sometimes serves as a connector between parking facilities, commercial street frontage, and other popular destinations. Paseos may be either publicly or privately owned and maintained, and are intended for general public use.

**PATHWAY**

A type of pedestrian facility that is located on private property. Pathways can serve a variety of functions, including linking separate buildings on a single site, linking buildings on adjacent sites, and connecting private buildings to sidewalks or paseos.

**PEDESTRIAN**

A person who walks or uses a means of conveyance propelled by human power (other than a bicycle).

**PEDESTRIAN AMENITIES**

Items that enhance the walking experience for the pedestrian. Examples include seating areas, canopy trees or other landscaping elements, lighting, drinking fountains, newsracks, trash containers, and telephones.

**PEDESTRIAN FACILITIES**

Improved walkways that are designed to carry pedestrian traffic between destinations. See entries for *Sidewalk*, *Paseo*, and *Pathway*.

**PEDESTRIAN FRIENDLY DESIGN**

Development that is designed with an emphasis primarily on pedestrian access to the site and building, rather than on automobile access and parking.

**PERGOLA**

A trellis supported by columns.

**PERMIT**

A written allowance that must be obtained from a governing body in order for a certain project to be undertaken.

**PLACITA**

A small plaza (see below).

**PLANNING COMMISSION**

A seven member commission established by the City Charter and appointed by the City Council to review and approve, conditionally approve, or deny projects based on Zoning and Subdivision Ordinance requirements. The Planning Commission also advises the City Council on changes to the Zoning Ordinance, issues related to the General Plan, and other development policies of the City.

**PLAZA**

An open space that occurs along a circulation route; a meeting or gathering place.

**PROPORTION**

The relationship of height to width and depth.

**PUBLIC RIGHT-OF-WAY**

Includes, but is not limited to, any street, avenue, boulevard, lane, mall, highway, sidewalk or other pedestrian pathway, bike path, trail, or similar place that is owned or controlled by a public entity.

**RETURN**

A surface that adjoins and recedes from the main face of a building (e.g. the side of a molding that trims a window or a small portion of a wall at a right angle to the main façade).

**REVEAL**

A recessed edge, especially the exposed masonry surface between a window jamb and the main face of the wall.

**ROOFLINE**

The highest point of a structure including parapets, but not including spires, chimneys, or heating or cooling mechanical devices.

**SCALE**

The relative dimensions or size of a building or related feature.

**SETBACK**

The required distance between the edge of a building and the public right-of-way.

**SIDEWALK**

A type of pedestrian facility that is located in the public right-of-way and owned and maintained by the City for public use at all times of the day. In the grid, sidewalks are generally provided around the perimeter of blocks and connect the street frontage of private lots.

**SITE**

A parcel or lot that is occupied or will be occupied by a use or structure.

**STORY POLES**

Temporary devices erected on a site to depict the actual size, bulk, scale, and height of a proposed building.

**STREET**

Any existing or proposed street, road, avenue, boulevard, land, parkway, place, public alley, bridge, viaduct or easement for public access. A street includes all land within the street right-of-way, whether improved or unimproved, and includes such improvements as pavement, shoulders, curbs, gutters, sidewalks, drainage appurtenances, parking space, bridges, and viaducts.

**STREETSCAPE**

The visual appearance of the neighborhood as seen from the street.

**STRUCTURE**

Anything constructed or erected and the use of which requires more or less permanent location or attachment to something having a permanent location on the ground.

**TRELLIS**

A frame put together by crossing wood strips or beams.

**TRANSIT**

Forms of group transportation such as busses, shuttles, trains, watercraft, vanpools, and carpools.

**TRANSIT AMENITIES**

Items that are provided to accommodate waiting transit passengers, including seating, shelters, route maps, and trash containers.

**TRANSIT STOP**

A designated area where passengers can wait for a transit vehicle.

**URBAN DESIGN**

The large scale organization of a city, dealing with the massing and organization of buildings and the spaces between them, but not with the architecture of individual buildings.

**URBAN SPRAWL**

The decentralization of development, resulting in low density construction away from traditional urban centers.

**VERANDA**

An open sided, raised sitting area with thin columns that support its roof. Typically a veranda will extend along an entire side wall or wrap around a building.

**ZONES OF BENEFIT**

Designated areas where buildings do not have to provide the entire amount of vehicular parking required by the Santa Barbara Municipal Code, Parking Section. These areas are located near a public parking lot that provides the required parking for the uses.

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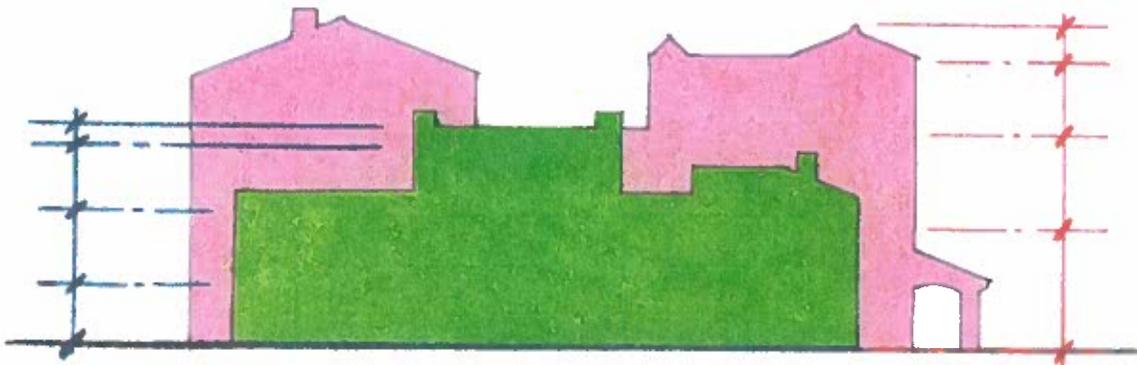
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A COMPARATIVE ANALYSIS  
OF  
**THREE STORY BUILDINGS**  
for  
DOWNTOWN SANTA BARBARA  
WITH RESPECT TO  
*SIZE, MASS, BULK AND SCALE*

By  
William Mahan, AIAE  
2005





## Acknowledgement

The buildings compared in this study were all designed by Santa Barbara architects, except the Bothin Building. Drawings were generously provided to me, and without them this study would not have been possible.

Eight buildings are compared for height, length, elevations, perspective (photographic), floor to floor heights and scale of architectural elements. Those drawings provided, which were not dimensioned, have been scaled.

The architects of the buildings are as follows:

- |                                       |                                     |
|---------------------------------------|-------------------------------------|
| 1. 727 Garden Street                  | Edwards & Pitman                    |
| 2. 801 Garden Street                  | Berkus Design Studio                |
| 3. 2323 DeLaVina Street               | Sharpe, Mahan & Associates          |
| 4. The Bothin Building                | Lionel Pries ('25-26)& Cernal Arch. |
| 5. 1123 Chapala Street                | Sharpe, Mahan & Associates          |
| 6. 1111 Chapala Street                | Lenvik & Minor                      |
| 7. 1021 Anacapa Street                | Cernal Architects, Inc.             |
| 8. Chapala Lofts, Chapala & Gutierrez | Berkus Design Studio                |

The section of the Cathedral of Florence is from A History of Architecture on the Comparative Method, by Sir Bannister Fletcher.

## Introduction

The purpose of this study is to understand what elements contribute to the height and size of 3 story buildings and to define and understand the meaning of *size, mass, bulk and scale*. Furthermore, this study will generate visual and comparative tools which the reviewing boards and commissions can use to evaluate proposed new designs. These tools will include Setback Evaluation Analysis, Vertical Envelope Analysis, Elevation Area Analysis and Perspective Analysis, and will be discussed in greater length starting on page 30.

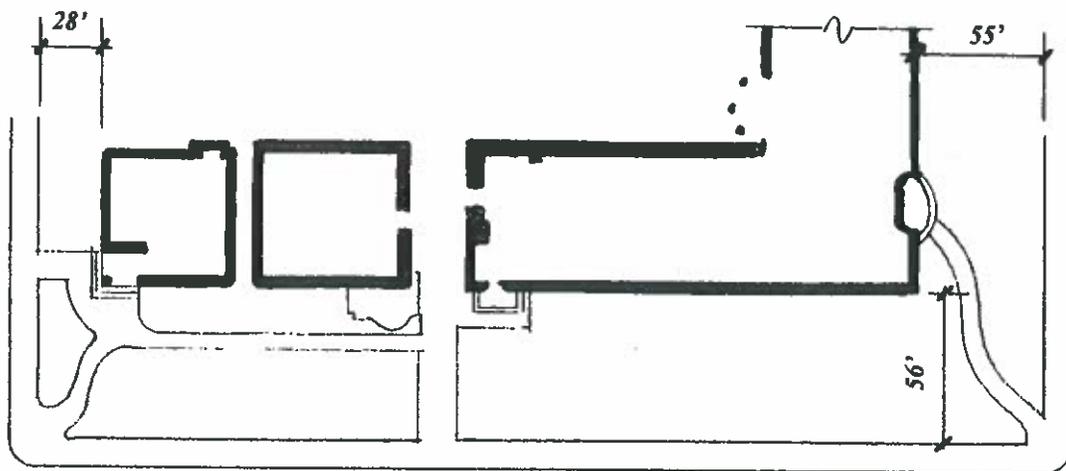
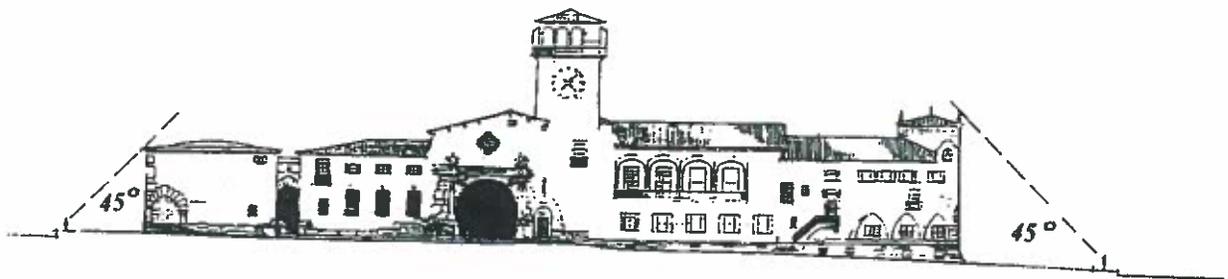
Because of land costs and a shortage of building area, it is believed that 3 story structures will be increasingly popular, and therefore it is important to establish some methods of comparison. The *size, mass, bulk and scale* of buildings is determined by the architect, but what elements cause them to be as they are and how are they interrelated? It is believed that an understanding of how these elements work will assist the design reviewers in evaluating the architectural proposals before them.

In measuring the eight buildings in this study some dimensions are scaled and some finished floor elevations or sidewalk elevations are averaged. It is not the purpose of this report to judge each building exactly, but rather to establish a general spectrum from small to large, and to try to understand what makes that happen. Heights of ridges, etc. given are for the elevation shown. Other parts of the building could be higher but are not considered in this study.

## Size

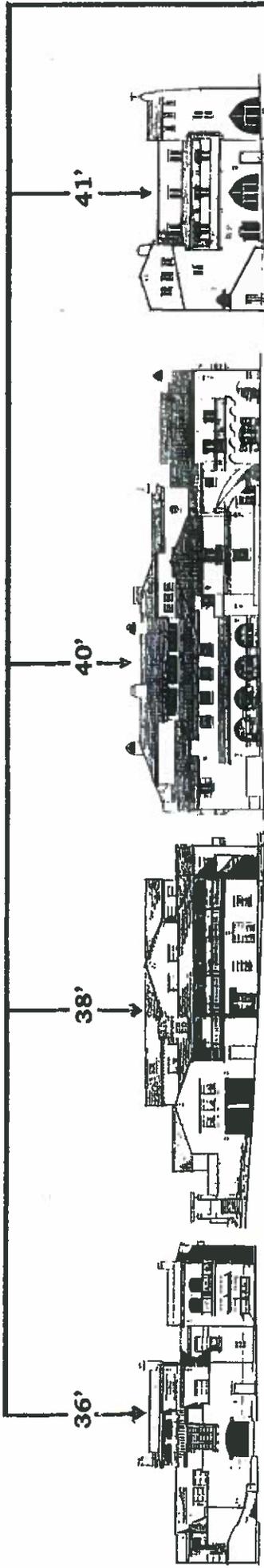
The eight buildings listed previously are shown in elevation at 1/32 scale on pages 5 through 8, to form a spectrum of *size*. They are also shown on page 3 at 1"=50' and compared to the Courthouse. They are organized in ascending order of their ridge heights from 36' to 57' and the Courthouse highest ridge which is also 57'. They are all 3 story buildings, but the highest ridge varies by 21' from the lowest ridge, a difference of 158%. Why is there such a difference in their heights? That question is what this study attempts to explore.

The apparent size of a building can be greatly mitigated by providing yard setbacks, as is amply demonstrated by the Santa Barbara Courthouse. Its apparent length and height are successfully reduced by its generous setbacks as shown below. Notice how a viewer's line of sight never exceeds 45 degrees. This view of the Courthouse is successful because of its setback from the property line, but its façade is essentially vertical. Another successful approach is step-backs in the building façade. As shown on page 14 a series of step-backs in the architectural elements function to reduce the building's size as seen from the street.



ANACAPA STREET

Avg. Fin. Fl. to Highest Ridge

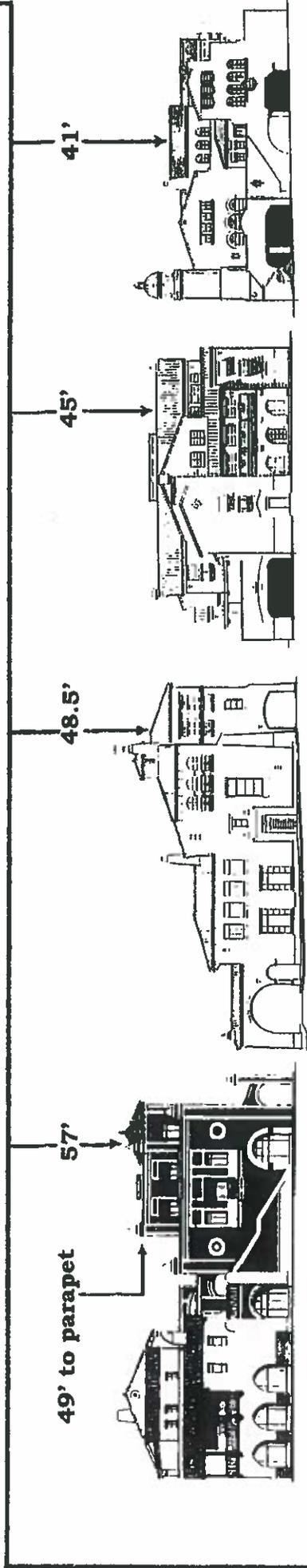


727 Garden Street

801 Garden Street  
De La Guerra St. Elevation

2323 De La Vina Street

Bothin Building  
De La Guerra Plaza  
Elevation

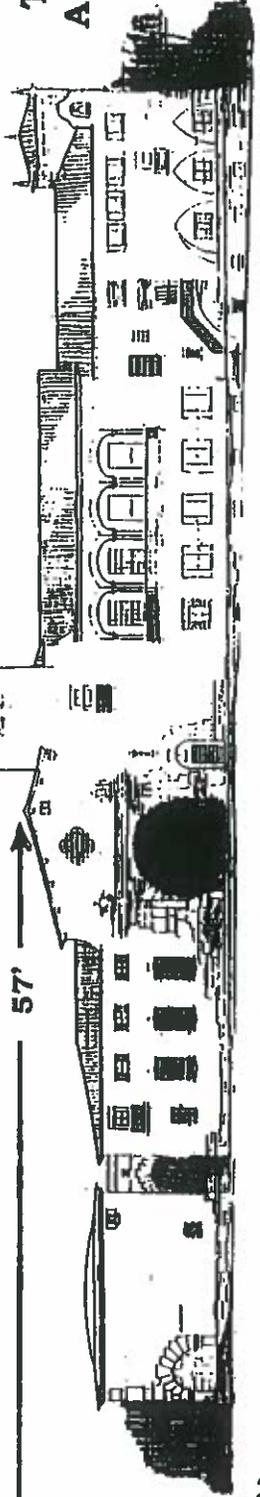


Chapala Lofts  
Gutierrez Street Elevation

1021 Anacapa Street

1111 Chapala Street

1123 Chapala Street



Santa Barbara Courthouse

Three Story Buildings  
A Comparative Analysis

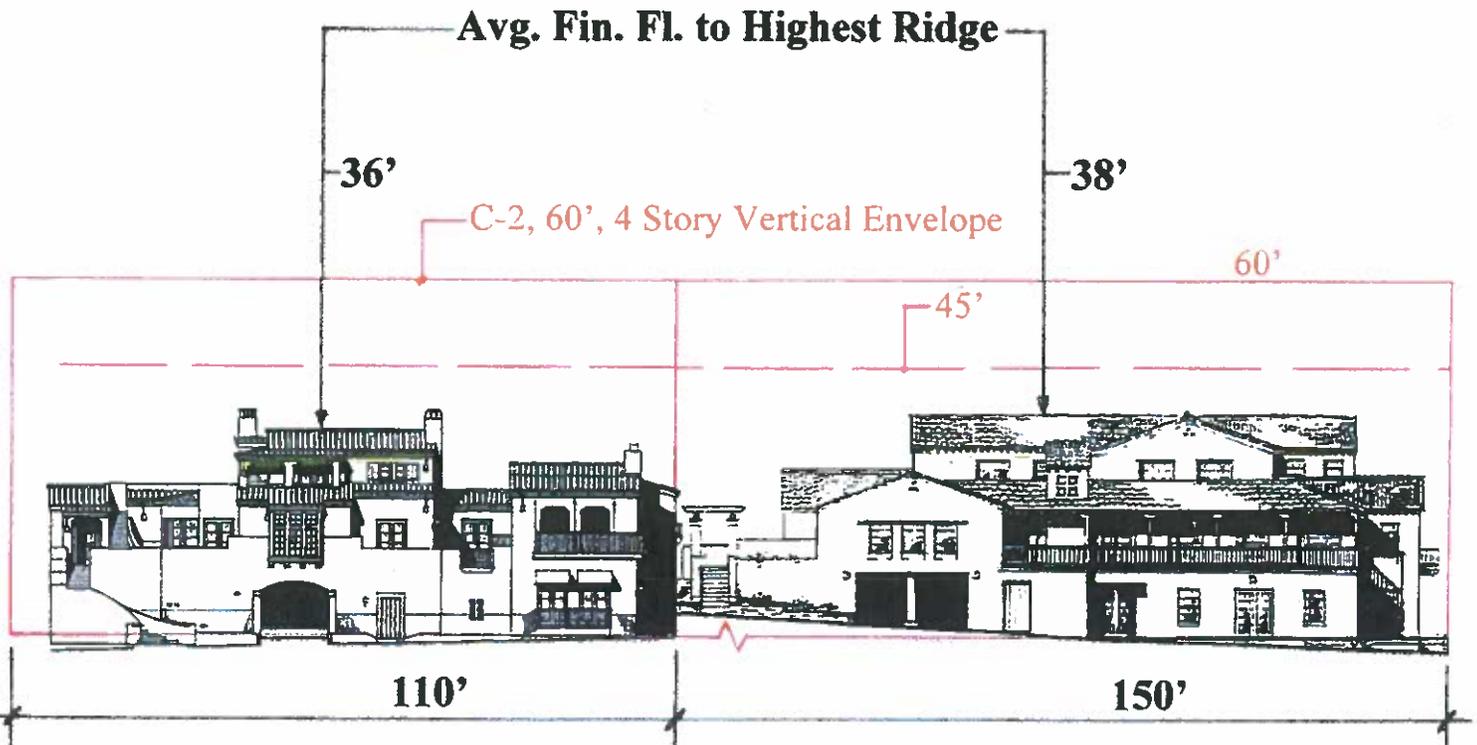
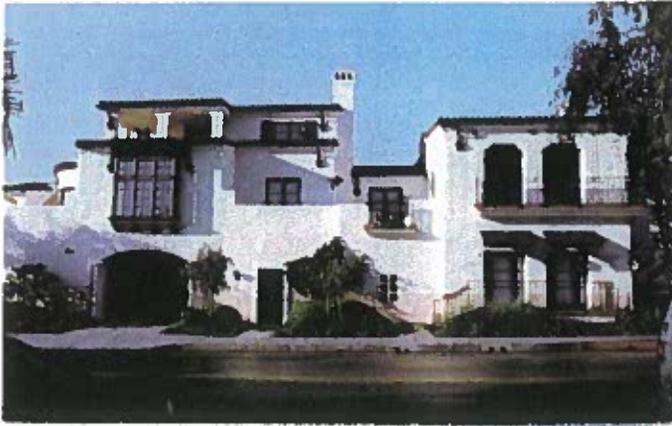
Scale 1" = 50'

On pages 5 through 8 these eight elevations are shown at approx. 1"= 32' scale. The average finished first floor to the highest ridge is given as well as the length of the buildable envelope facing the sidewalk. Where the finished floors vary and their elevations are complex, the average sidewalk elevation was scaled. Some buildings are broken up in an attempt to shorten their visual length, such as 2323 DeLaVina and some buildings are divided into two distinct architectural styles, such as the Chapala Lofts, shown on pages 13 and 18 respectively. These two examples demonstrate that visual length, as well as actual length, is important when evaluating size.

Generally, it must be recognized that length is site specific. There is no correlation between ascending heights and corresponding lengths of buildings. However, length does contribute to *mass and bulk* as will be discussed later.

Pages 5 through 8 also give the fl. to fl. dimensions of the 1<sup>st</sup> and 2<sup>nd</sup> floors and their total height, as well as the highest plate and the highest ridge, and also the EAR calculation described below. Also shown in yellow is the height of the major eave, which usually is similar to the highest plate. It should be observed that the buildings with parking on the 1<sup>st</sup> floor have the lowest 1<sup>st</sup> fl. to fl. heights which contributes to their being at the low end of the spectrum. Commercial use seems to result in more height than office use. Residential use cannot be evaluated in this study because only two of the samples have residential use and they position themselves at opposite ends of the spectrum. However, fl. to fl. height of residential space is very discretionary so these two examples demonstrate the degree of discretion that the architect has in determining the final height of a building with residential function.

Also illustrated on pages 5 through 8 are the areas of the Vertical Building Envelopes shown by the red lines. The ratio of the area of the elevation to the area of the Vertical Building Envelope is the Elevation Area Ratio (EAR). Expressed as a percentage it represents the degree to which the building façade fills up its buildable space. The EAR percentage is a tool which will be discussed later starting on page 30. The dashed line represents the 45' high envelope which is the maximum height for 3 story buildings in the C-O and R-3 zones. Although 3 story buildings can legally be 60' tall in the C-2 zone the eight examples shown in this study vary from 36' to 57', which illustrates the wide variation above and below the 45' mark. When a 3 story building exceeds 45' (in the C-2 zone) it should be carefully considered by the reviewing boards and commissions for its qualities of neighborhood compatibility and human scale.



**727 Garden Street**

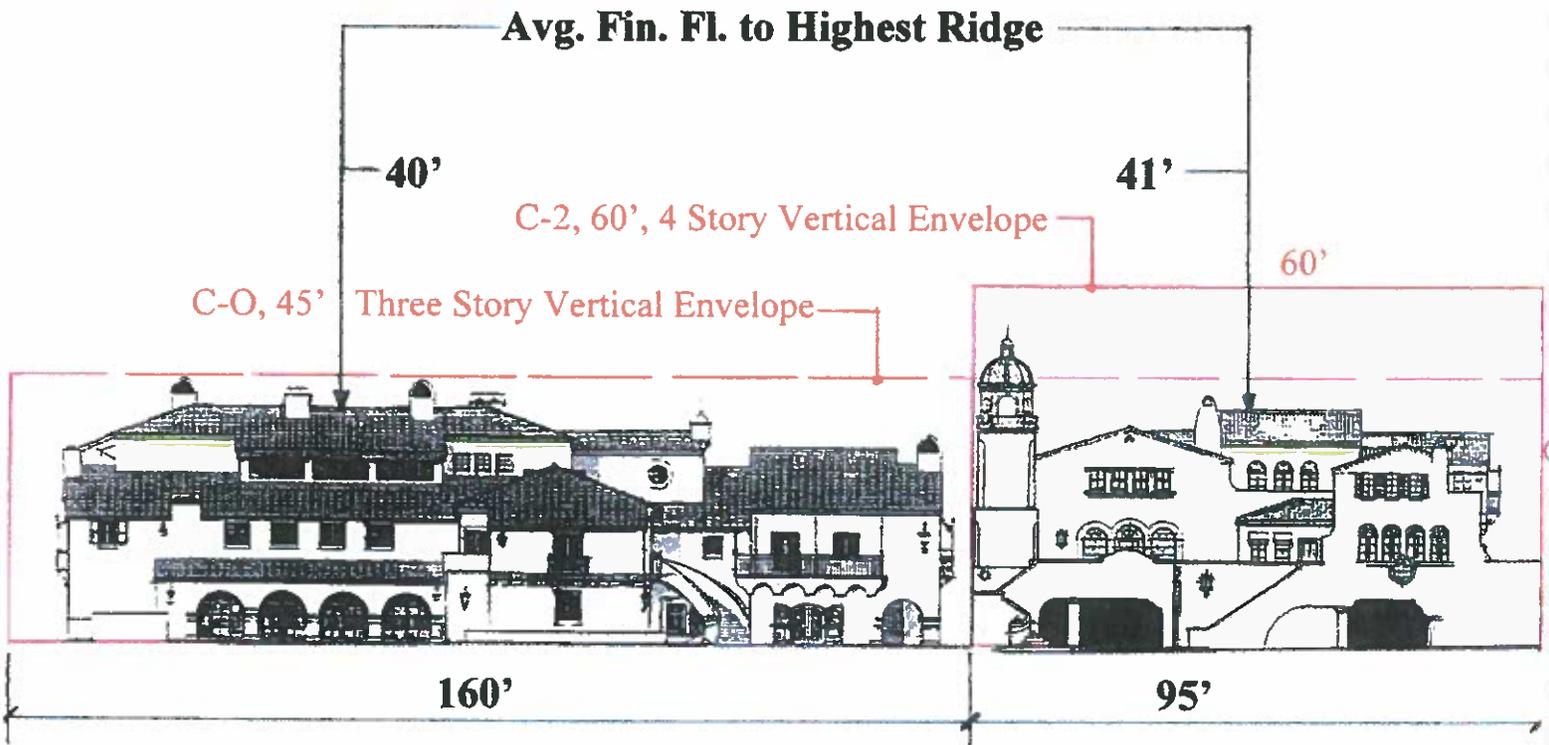
1<sup>st</sup> fl to fl=12.5'  
 2<sup>nd</sup> fl to fl=11'  
 Highest Plate=31'  
 Highest Ridge=36'  
 Façade area=3060 sf  
 EAR=3060/(60X110)=0.46

**801 Garden Street**

1<sup>st</sup> fl to fl=12.5'  
 2<sup>nd</sup> fl to fl=11.5'  
 Highest Plate=32'  
 Highest Ridge=38'  
 Façade area=3011 sf  
 EAR=3011/(60x150)=0.33

## Three Story Buildings A Comparative Analysis

Scale 1"=32'



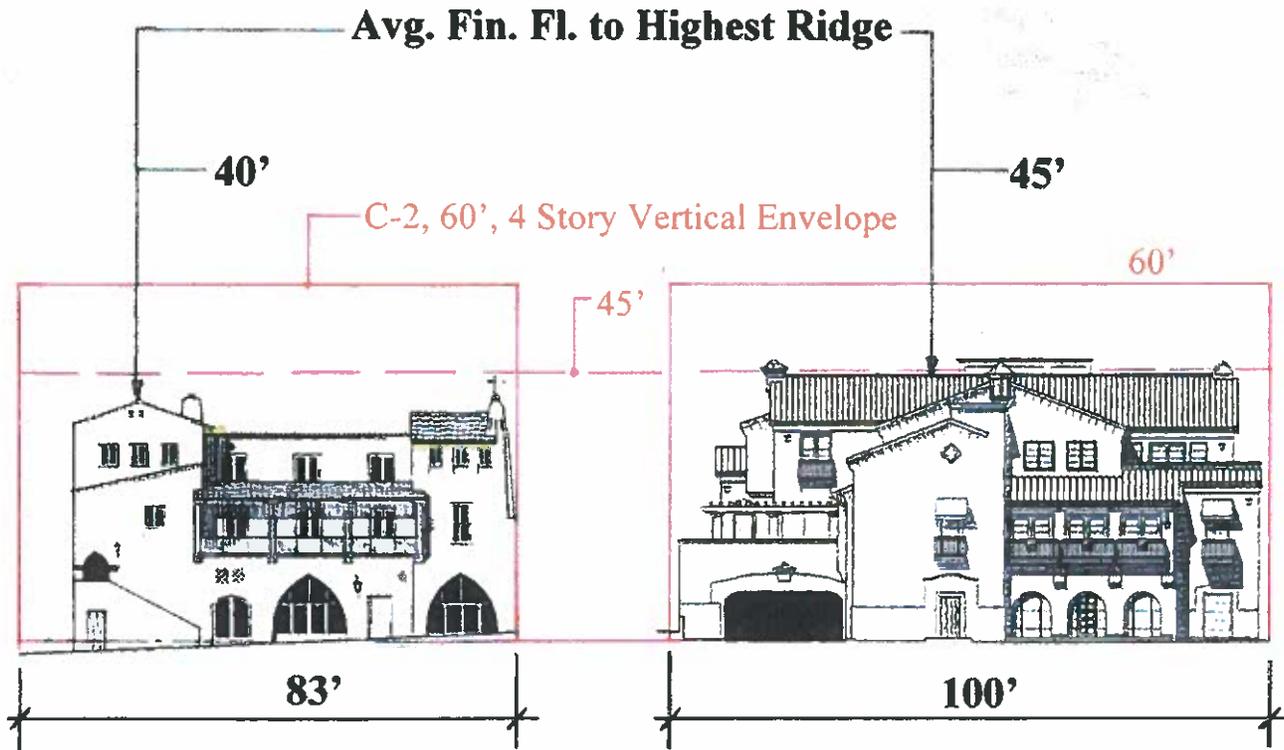
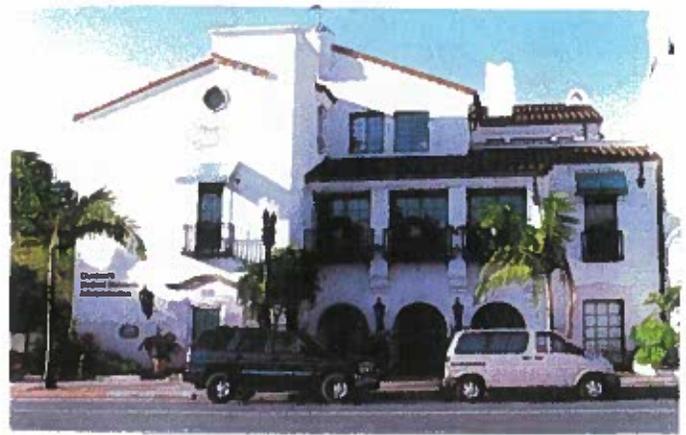
**2323 De La Vina Street**

1<sup>st</sup> fl to fl=12'  
 2<sup>nd</sup> fl to fl=12'  
 Highest Plate=33'  
 Highest Ridge=40'  
 Façade area=5149 sf  
 EAR=5149/(60X160)=0.54  
 (adj. for C-O, 5149/(45x160)=0.71)

**1123 Chapala Street**

1<sup>st</sup> fl to fl=11.5'  
 2<sup>nd</sup> fl to fl=12'  
 Highest Plate=34'  
 Highest Ridge=41'  
 Façade area=3293 sf  
 EAR=3293/(60x95)=0.58

**Three Story Buildings  
 A Comparative Analysis**  
 Scale 1"=32'



**Bothin Building, 1926**  
(De La Guerra Plaza Elevation)

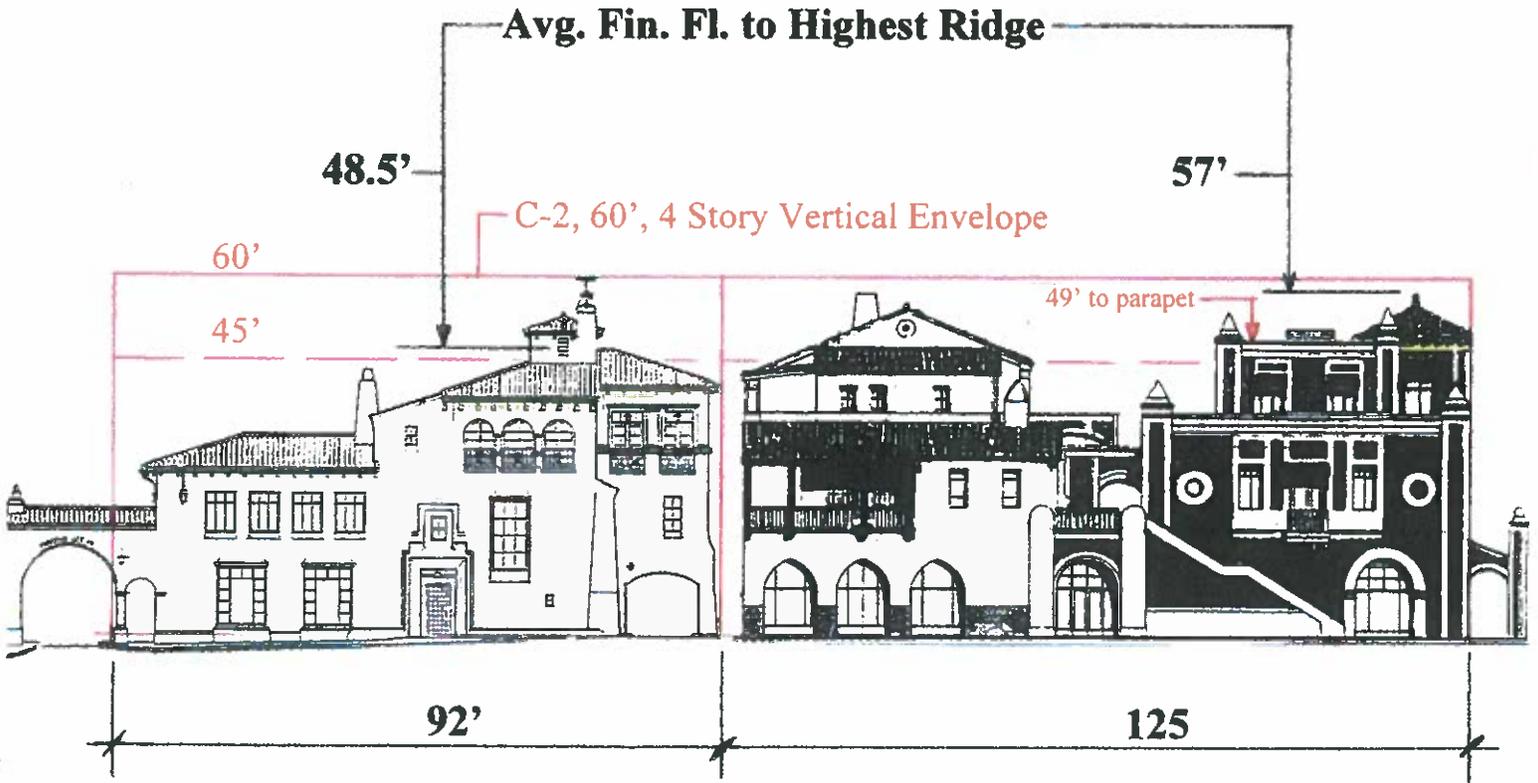
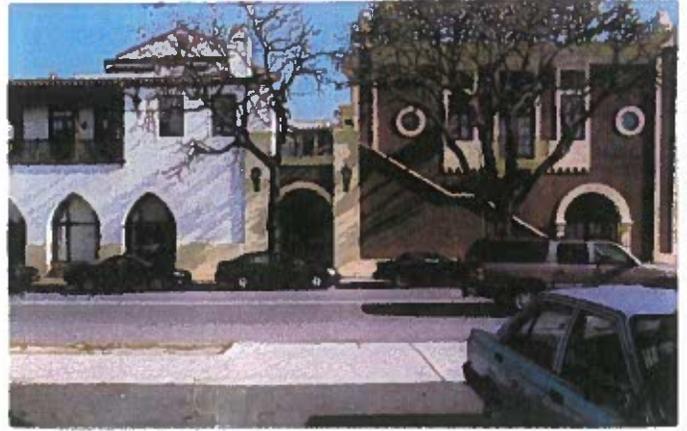
**1123 Chapala Street**

1<sup>st</sup> fl to fl=10'  
 2<sup>nd</sup> fl to fl=15'  
 Highest Plate=32'  
 Highest Ridge=41'  
 Façade area=2599 sf  
 EAR=2599/(60X83)=0.52

1<sup>st</sup> fl to fl=13.5'  
 2<sup>nd</sup> fl to fl=13.5'  
 Highest Plate=36'  
 Highest Ridge=45'  
 Façade area=4073 sf  
 EAR=4073/(60x100)=0.68

## Three Story Buildings A Comparative Analysis

Scale 1"=32'



**2323 De La Vina Street**  
 1<sup>st</sup> fl to fl=14'  
 2<sup>nd</sup> fl to fl=14'  
 Highest Plate=40'  
 Highest Ridge=48.5'  
 Façade area=3624 sf  
 EAR=3624 / (60X92)=0.59

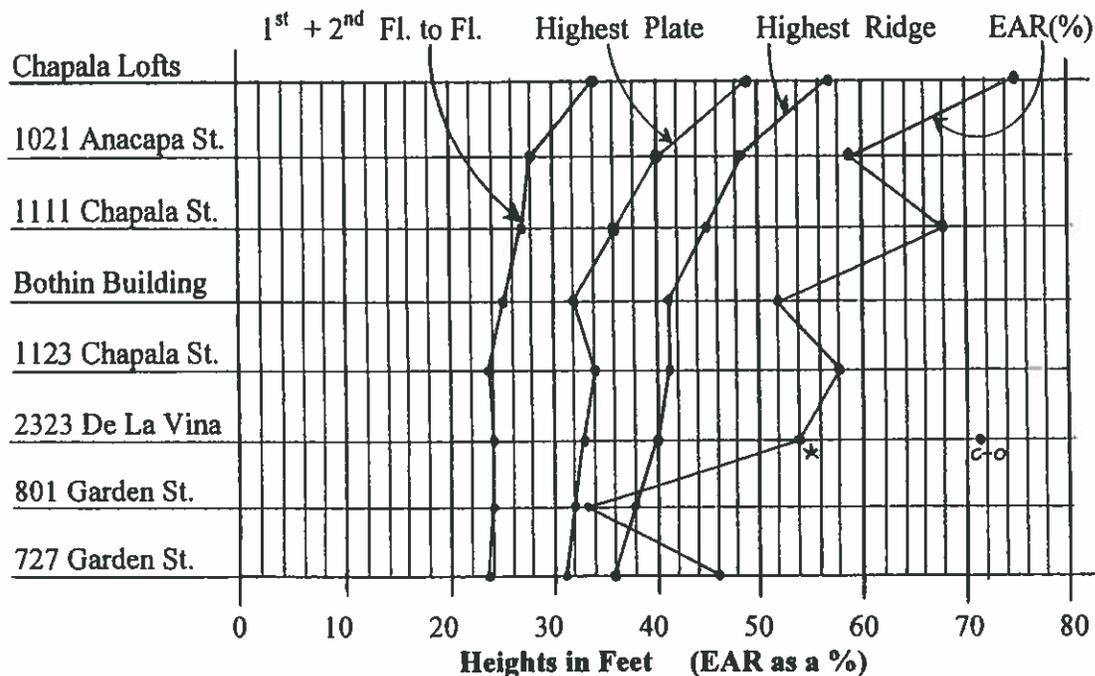
**1123 Chapala Street**  
 1<sup>st</sup> fl to fl=18'  
 2<sup>nd</sup> fl to fl=16'  
 Highest Plate=49'  
 Highest Ridge=57'  
 Façade area=5666 sf  
 EAR=5666 / (60x125)=0.76

## Three Story Buildings A Comparative Analysis

Scale 1"=32'

On pages 5 through 8 the major eave is shown in yellow. In the elevation drawings on these pages the highest ridge dominates. But in actuality as shown on pages 11 through 18 the major eave may often appear to be the highest element, and in some instances a major gable, such as seen at 1123 Chapala Street on page 14, essentially obscures the highest ridge. Models and perspective drawings as discussed beginning on page 30 are better at evaluating apparent height, rather than elevations, which only show measured heights.

Graph 1 below shows the relationship between 1<sup>st</sup> and 2<sup>nd</sup> fl. to fl. heights, highest plate and highest ridge. The correlation between the fl. to fl. heights and the plate and ridge heights demonstrates how these dimensions are related. However, it should be observed that the designer has considerable discretion in the result of the final heights. Notice how the first four lowest fl. to fl. totals are all about the same; 23.5'; 24' and 23.5'. However, the highest plate and ridge for those four examples increase from 31' through 36' to 34' through 41', respectively. Decisions on the height of the 3<sup>rd</sup> floor plate and the pitch of the roof matter dramatically. Also The EAR is given for each building. If a proposed new 3 story building's EAR and dimensions are inserted into the graph, the reviewing boards and commissions will understand where it fits in the spectrum of small to large. It is not suggested that small is good and large is bad, because variety is important, but the decision makers should know how a building compares with similar buildings and how it relates in terms of compatibility with its neighbors.



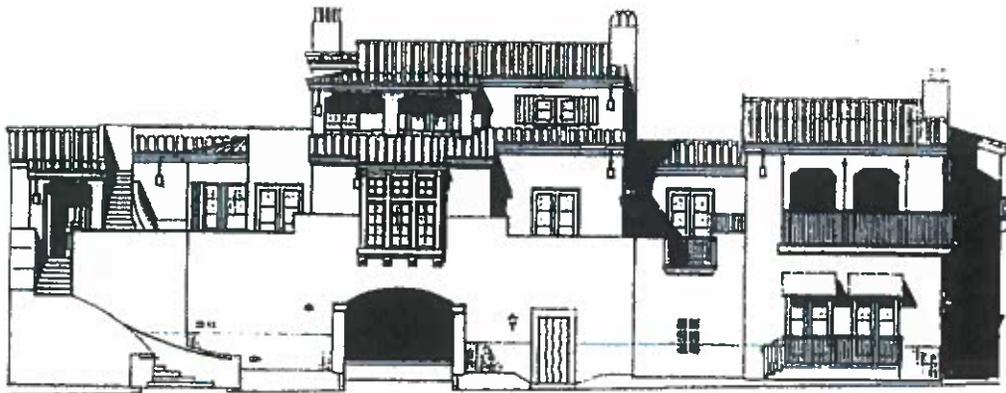
**Graph 1**  
**Fl. to Fl., Plate Hights, Ridge Hights and EAR%**  
 \* EAR adjusted for a C-2 Zone

On page 14 the elevation of 1123 Chapala is color coded to display the step-backs from the property line of the different elements of the elevation. One should take note that the highest ridge of 41' is on a section of building that sets back 28'. A visual tool such as color coding will help the reviewers better perceive the various step-backs of an elevation drawing.

In conclusion, *size* is defined by plate and ridge heights, which are driven by fl. to fl. totals that range widely based on function and discretion. *Size* is also a function of length and this dimension is more site specific, but it can be altered visually by breakup or sectioning with different styles. *Size*, as determined by an EAR is another tool to be used. Step-backs are very effective in reducing the apparent *size* of a building, and providing open balconies on the third story at the corners will give the design a two and three story look. So it is clear that a building cannot properly be evaluated by quantitative analysis alone. It must also be reviewed as a three dimensional piece of architecture and compared with like buildings so that it is understood where it fits into the spectrum of *size*, and how its fl. to fl., plate and ridge heights compare in Graph 1. Finally, the building needs to be compatible in its neighborhood.



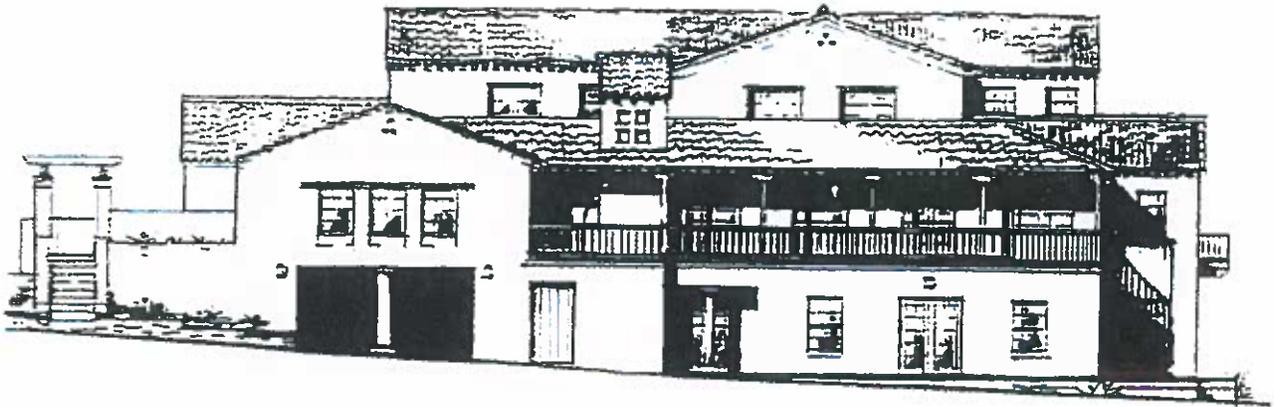
The highest ridge and the three story mass is effectively blocked by the two story mass on the corner. This building is a beautiful example of a two and three story composition.



**727 Garden**  
**Garden Street Elevation**  
Scale 1"=20'



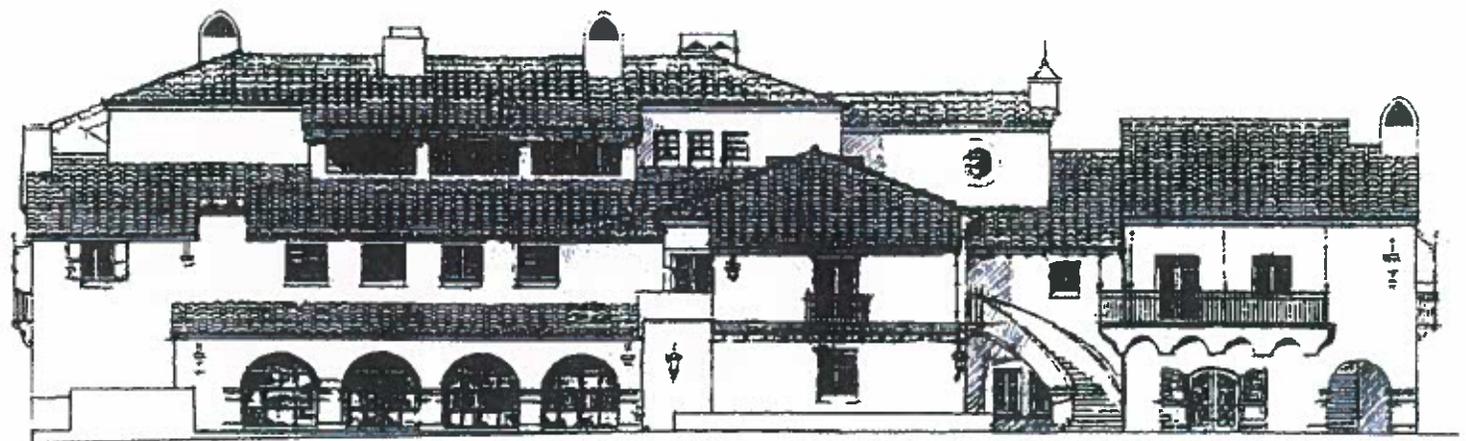
The long ridge is screened by the double gables and the trees help to soften the composition. The step-back at the corner and the Monterey Style balcony help to give this design human scale.



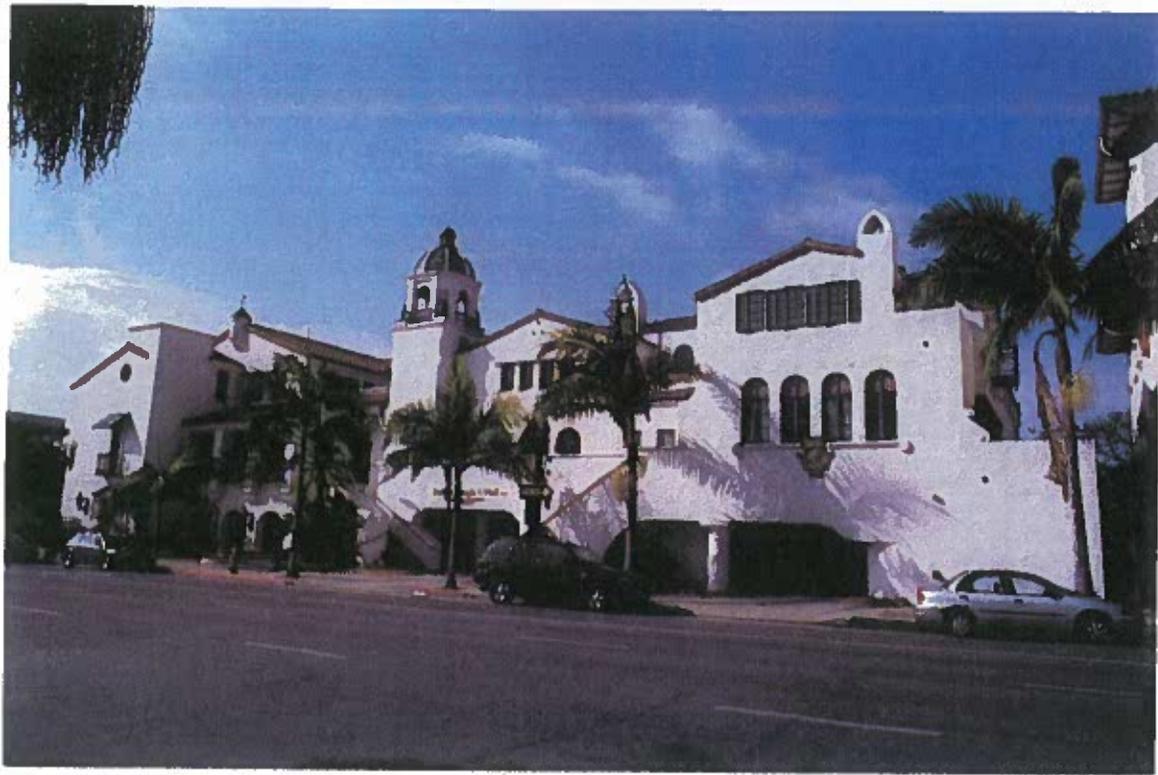
**801 Garden**  
**De La Guerra Street Elevation**  
**Scale 1"=20'**



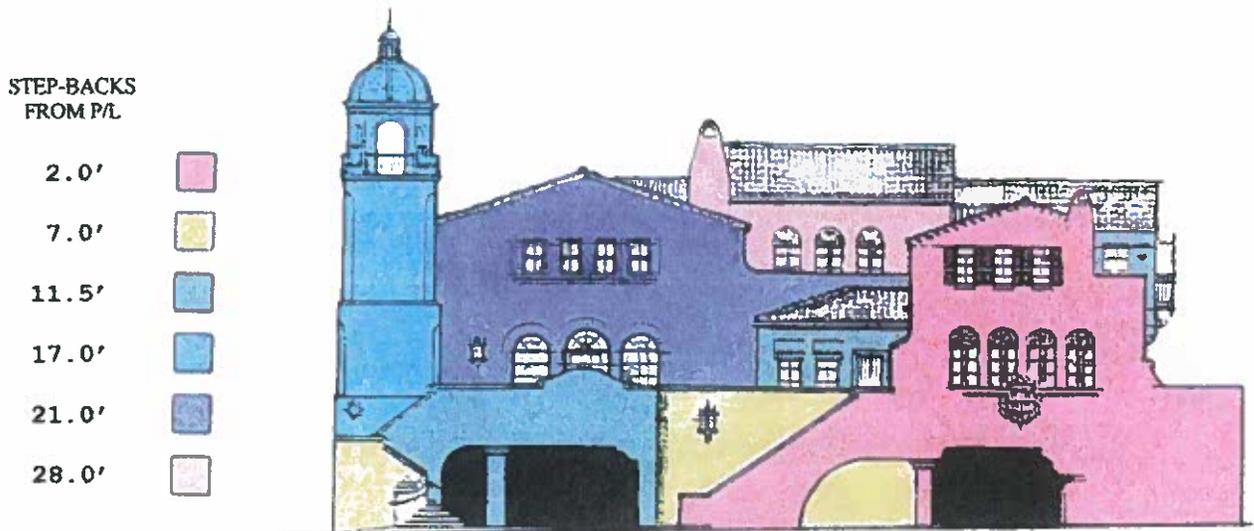
The three story element is screened by the two story element and the trees help to break up the masses. This building is almost a half of a block long, but the two story element on the corner is set back 28' from the De La Vina sidewalk, and separated from the main three story mass by a 16' wide patio which visually separates the two elements of the building.



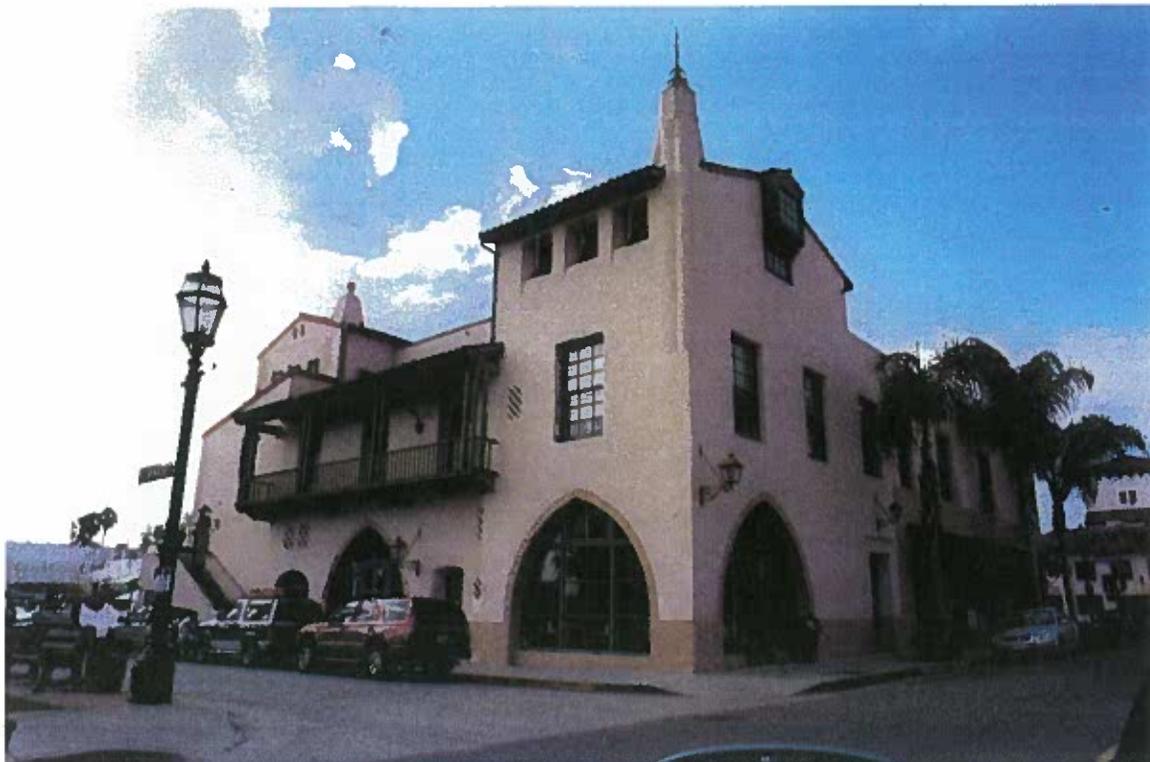
**2323 De La Vina**  
**De La Vina Street Elevation**  
**Scale 1"=20'**



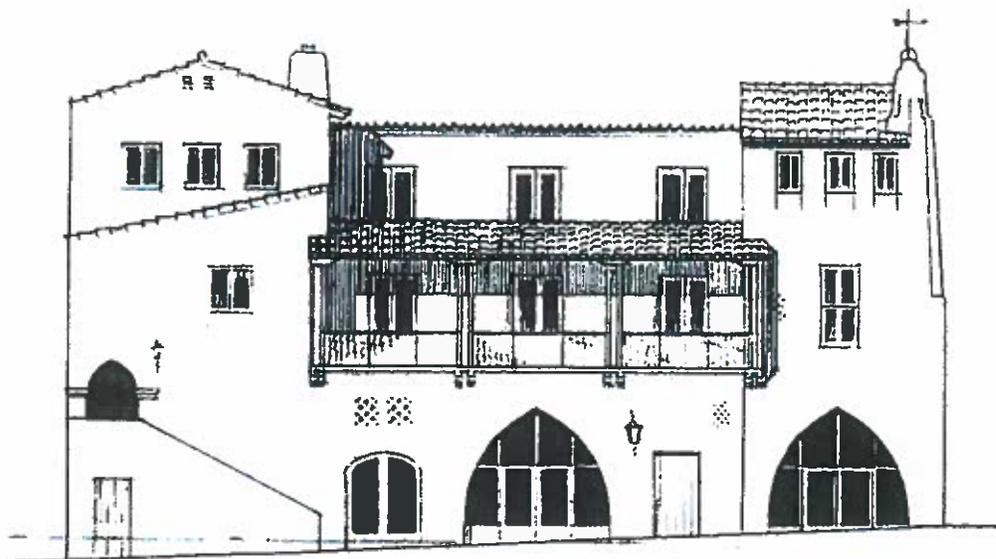
The highest ridge is screened by the lower three story gabled element. Notice how the three story gable in the photo appears much larger than it does in the elevation drawing. The stepped corner balconies help to soften the three story element shown in red.



**1123 Chapala**  
**Chapala Street Elevation**  
 Scale 1"=20'



There is some photographic distortion, but the three story mass looms. Once again, however, the major eave rather than the highest ridge dominates. The elevation demonstrates a pleasing composition, and the Monterey Style balcony works well to break up the massing. This building benefits by borrowing open space from De La Guerra Plaza



**Bothin Building 1926**  
**De La Guerra Plaza Elevation**  
Scale 1"=20'



Notice how the elevator tower is the highest mass in the photo, but hardly noticeable in the elevation drawing. The highest ridge is barely apparent as it is screened by the forward elements. This building is a good example of how elevation drawings are less reliable than perspectives and models.



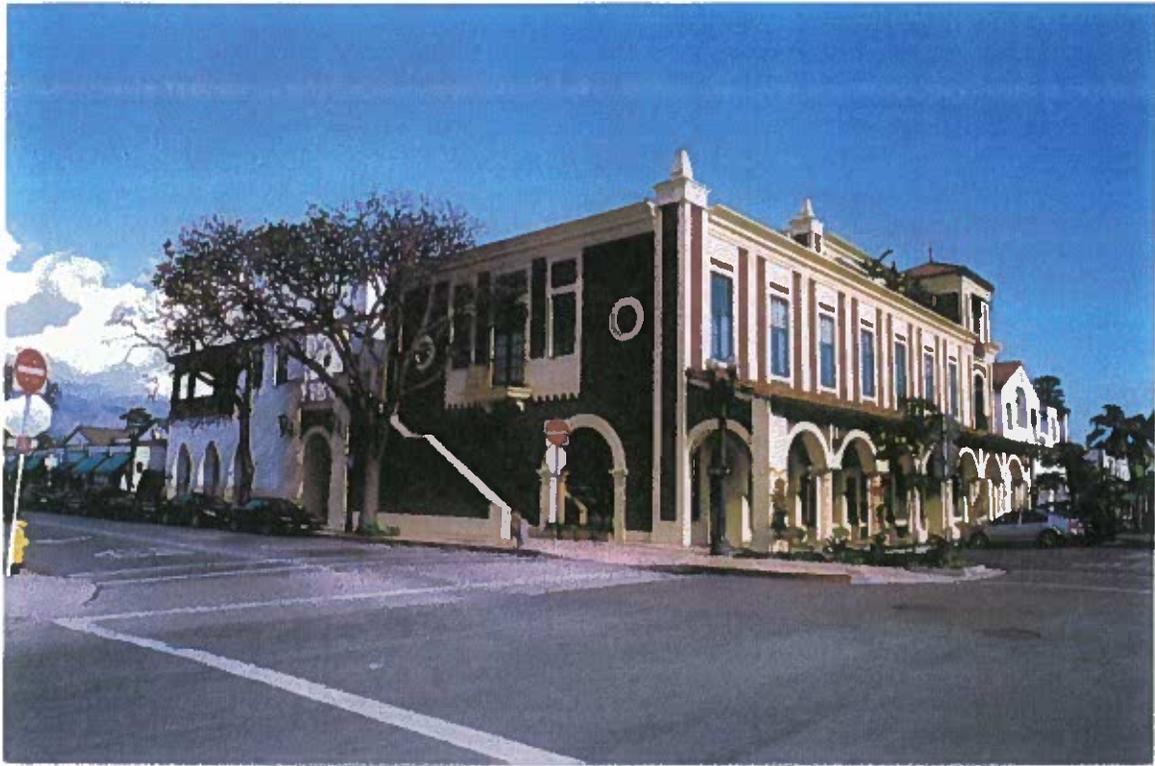
**1111 Chapala**  
**Chapala Street Elevation**  
Scale 1"=20'



The highest ridge is screened by the highest plate and major eave. Stepped masses are more effective from across the street and down Anacapa, looking up street. So this massing works better for the pedestrian than the driver. If the porch on the third story were not roofed over the massing of this three story element would be less imposing.



**1021 Anacapa**  
**Anacapa Street Elevation**  
Scale 1"=20'



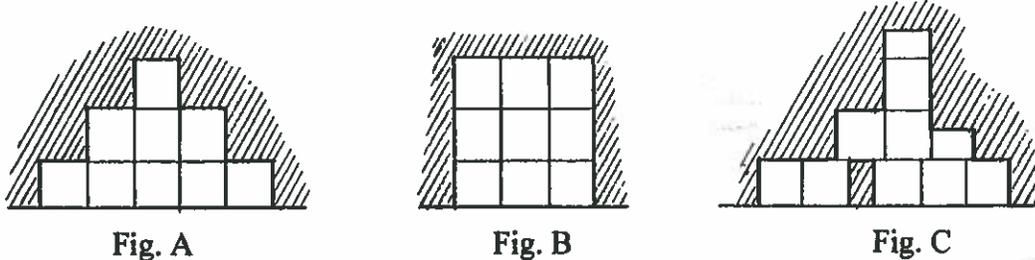
The highest ridge is less important with flat roofs, where highest plate or parapet govern. Trees help, and the total mass is broken up with two different architectural styles. The arcade, which is built over the public sidewalk helps to reduce the apparent height of the façade.



**Chapala Lofts**  
**Gutierrez Street Elevation**  
Scale 1"=20'

## Mass and Bulk

In considering the architectural qualities of mass and bulk it will be helpful to try to define these two terms more clearly.



*Mass* is a quantitative term and represents the amount of something. For example, the area of the 9 squares in Figures A, B and C, or the length and height of the elements of a building elevation represent its *mass*. The analytical eye will observe that the *mass* (area) of Figure A, B and C are the same.

*Bulk*, on the other hand is the qualitative term and represents the composition of the *mass*. For example, the 9 squares in Figure B appear bulkier than Figure A even though Figure A is wider, and bulkier than Figure C even though Figure C is both higher and wider than Figure B. Using our Courthouse, a similar example is shown in Fig. 2 and Fig. 3 below. Assume that the *mass* (area) of the tower approximately equals that which it would take to fill in the grand archway. The *mass* of Fig. 2 would be equal to the *mass* of Fig. 3. However, the *mass* of Fig. 2 seems to possess a less bulky composition. Whether the *mass* of the Courthouse is too great or not is not the issue. The issue is that the composition shown in Fig. 2 is more acceptable than Fig. 3 because it appears less *bulky*. Although the Courthouse is a massive building, it is graced with beautiful composition and generous setbacks.

If a building's functions require a *mass* equaling the dimensional heights illustrated in the upper portions of the curves shown in Graph 1 on page 9, it will be important for the design review boards and commissions to carefully consider the composition of that *mass* and the appropriateness of ample setbacks.



Fig. 2



Fig. 3

A Comparative Analysis of Bulk  
The Courthouse w. and w/o. the Grand Arch and Tower

## Scale

In architectural design, *scale* is the proportions of a building or its parts, with reference to a definite unit of measure.<sup>1</sup> For the architecture of Santa Barbara and especially the Pueblo Viejo the definite unit of measure is the height of a human being. This is why we use the term "human scale."

Of course, there are exceptions to this in our city because architecture has a number of function and ideals to express. For example, the grand archway of our Courthouse expresses with its grand scale the power of "government of the people". And the grand scale of the Arlington Theater expresses the glamour and influence of the 1920 movie industry with a building that proclaims "the sky's the limit". But generally, Santa Barbara is a city of buildings whose *scale* is decidedly human and possessed of great charm, as is best exemplified by our El Paseo.

The problem is that the architectural elements of a building such as doors, windows, archways, towers, etc. must be properly proportional to fit the architecture as a whole. And, as a building gets bigger, so must its elements. When this happens they tend to lose their human scale.

Look at the proportions of the windows and arches of the Cathedral of Florence on page 21 in comparison with the windows and arches of our Courthouse. Even though we recognize the Courthouse as one of our largest buildings, its elements seem small by comparison. Because buildings in Santa Barbara are getting larger, we must be concerned about which existing buildings we accept as standards for comparing *size, mass, bulk and scale*. If our goal is to retain the human scale in our architecture we must struggle to reduce the *size* of larger buildings. *Size* determines *mass*. Composition determines *bulk*. Architecture with a given *mass* must be composed of elements that are to scale with that *size* and *mass*. Whether or not those elements are human in *scale* will be dependent on the *size* and *mass* of the elements that compose the architecture.

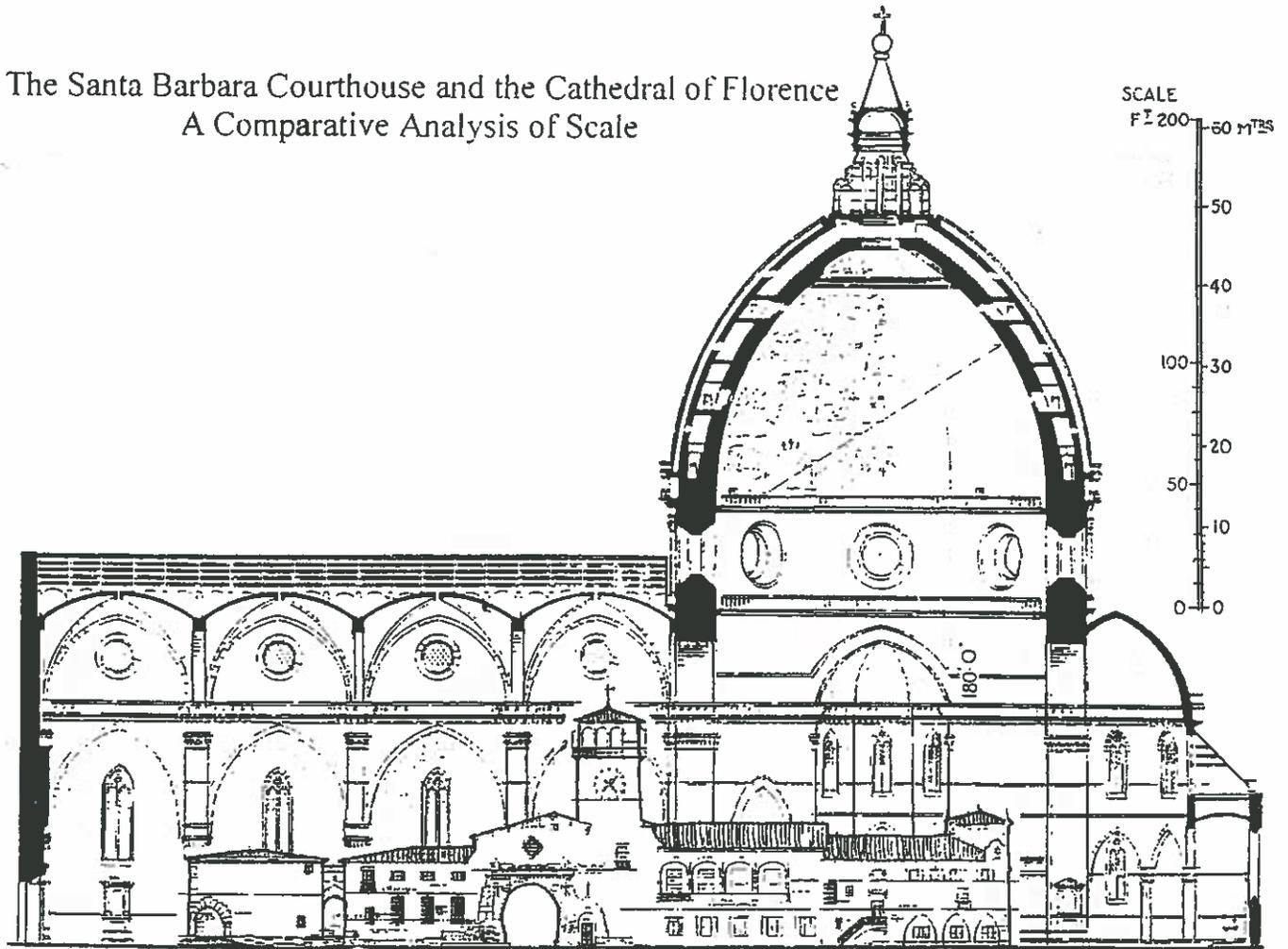
Following, on pages 22 through 29, are vignettes of our eight SCALE STUDY examples with an average height man shown. One can compare his height (5'-8") with the size of the architectural elements which the building possesses.

In conclusion, it can be seen from these pages that there is a reasonable range of *size* (ie. fl. to fl., plate and ridge heights) which accommodate door and window sizes (and corresponding other architectural elements) which are closely related to the human height, and thus will be human in scale. But, buildings of greater *size* and corresponding *mass* and *scale* require elements of greater size. These greater sizes depart proportionately from human scale, and consequently from the charm and character that has epitomized the architecture of Santa Barbara, which we all admire and seek to sustain.

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<sup>1</sup> Paraphrased from Sturgis' Dictionary of Architecture.

The Santa Barbara Courthouse and the Cathedral of Florence  
A Comparative Analysis of Scale



This Elevation of the Courthouse  
and the  
Section through the Cathedral  
are shown at the same scale

## Scale Study

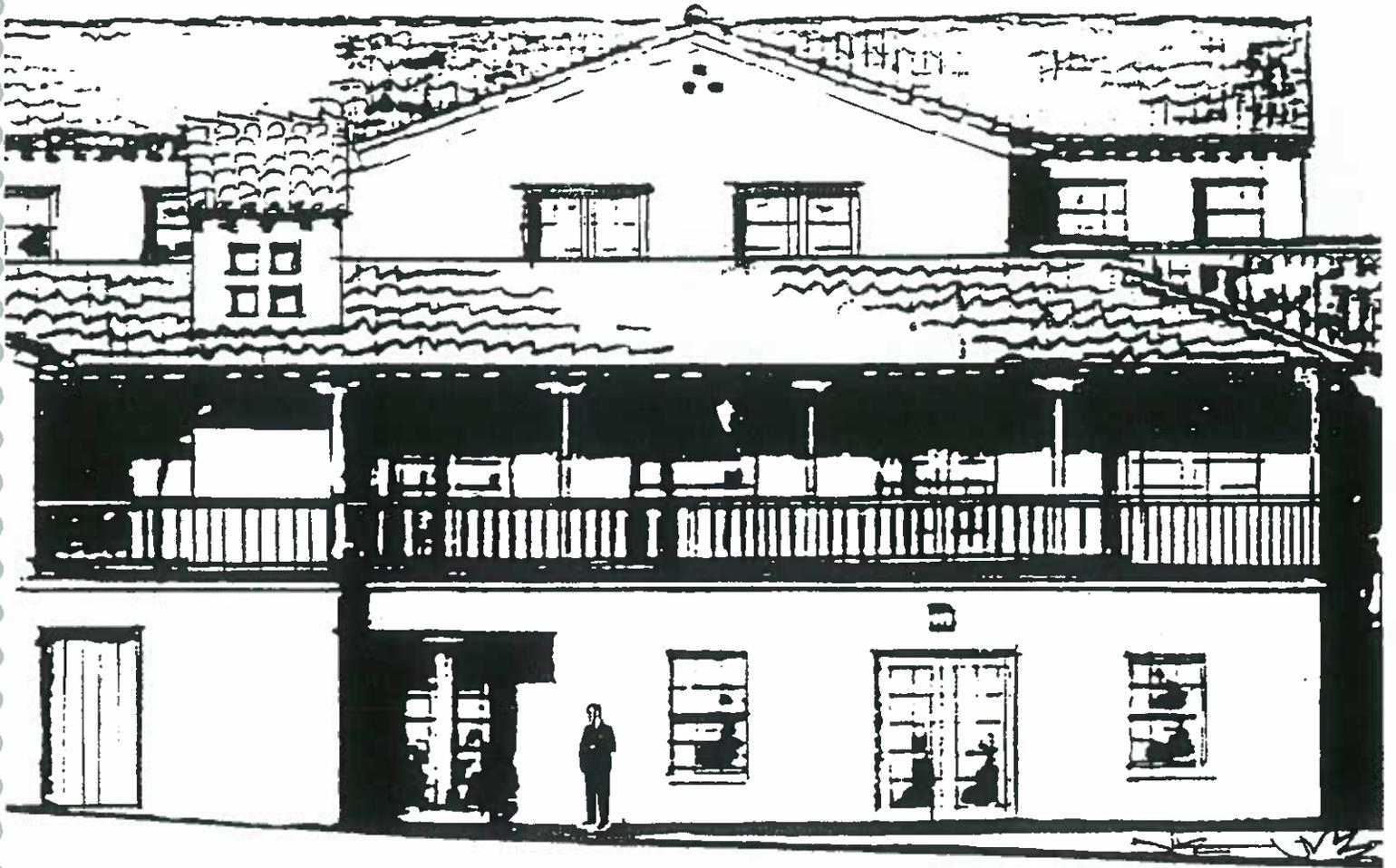
5'-8" man compared with 7' high door at grade and  
8' high french doors at the upper balcony.



727 Garden Street  
1/8"=1'-0"

## Scale Study

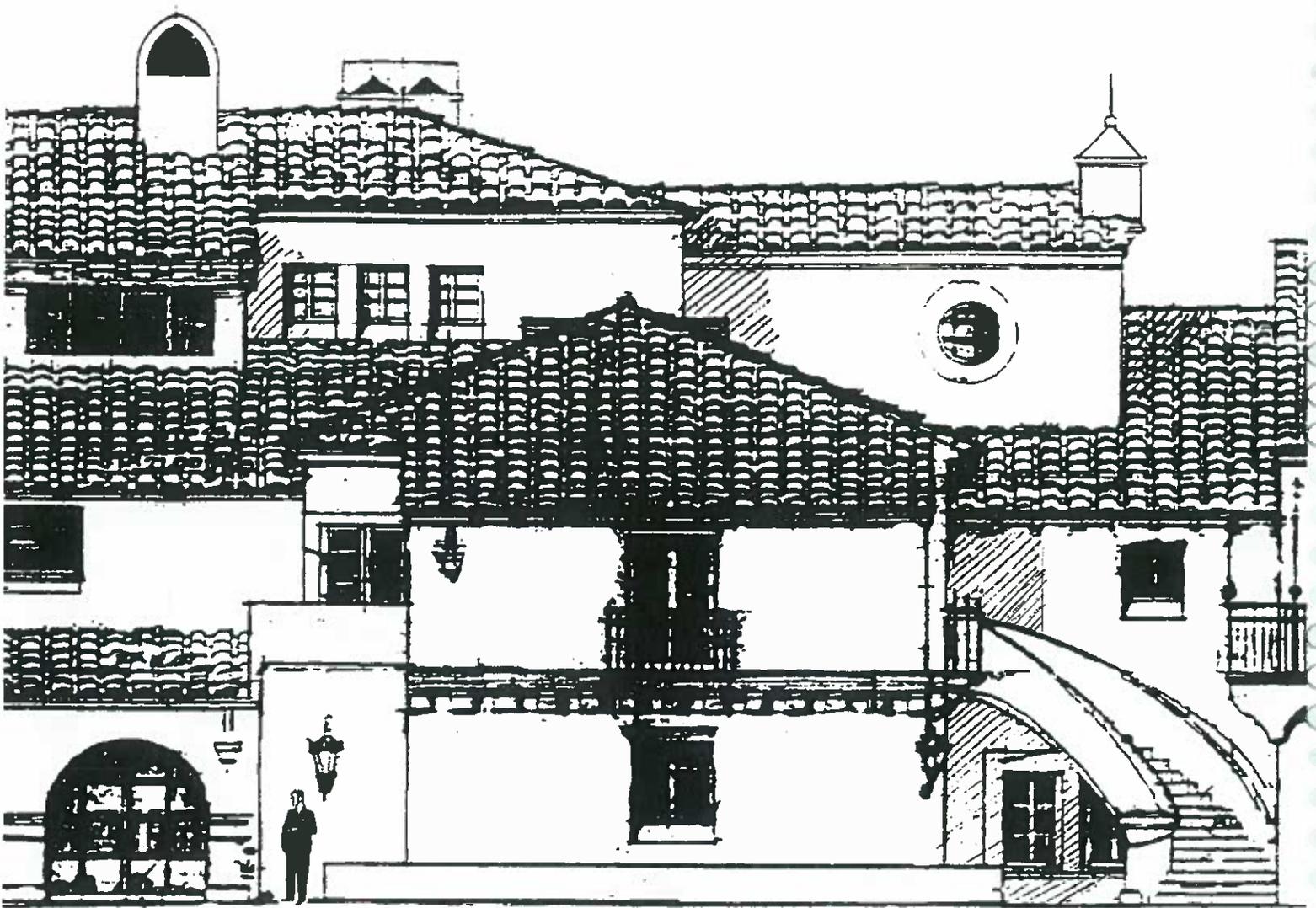
5'-8" man compared with 8' high doors on the terrace  
and the 9'-6" high balcony above.



801 Garden Street  
De LA Guerra St. Elevation  
1/8"=1'-0"

## Scale Study

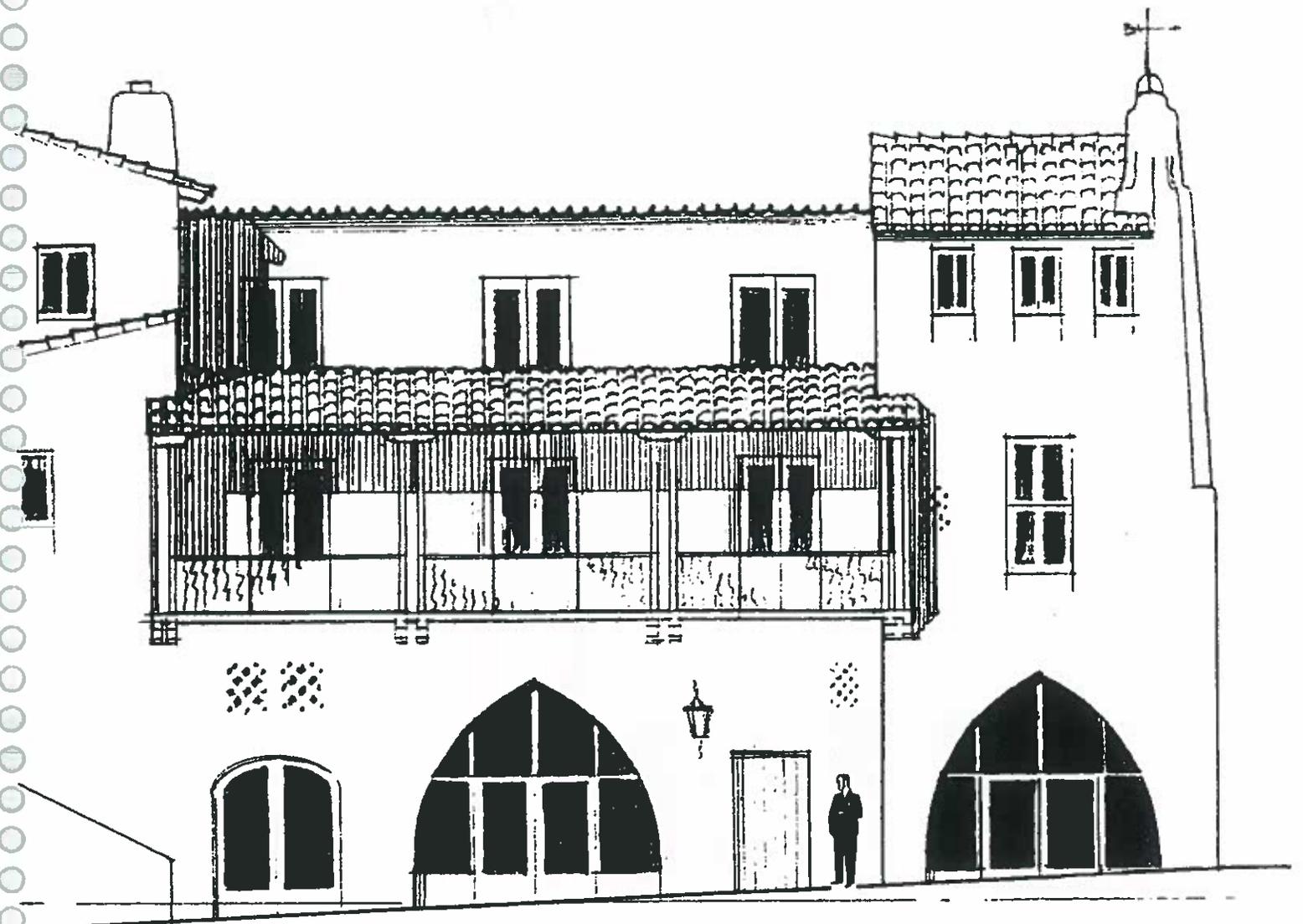
5'-8" man compared with 8'-6" high arch  
and 7' high french doors at the balcony above



2323 De La Vina Street  
1/8"=1'-0"

## Scale Study

5'-8" man compared with 7' high door  
at grade and the 11' high arches.



Bothin Building 1926  
De La Guerra Plaza Elevation

1/8"=1'-0"

## Scale Study

5'-8" man compares with 8'-6" high garage opening and the 10' high tri-arches above.

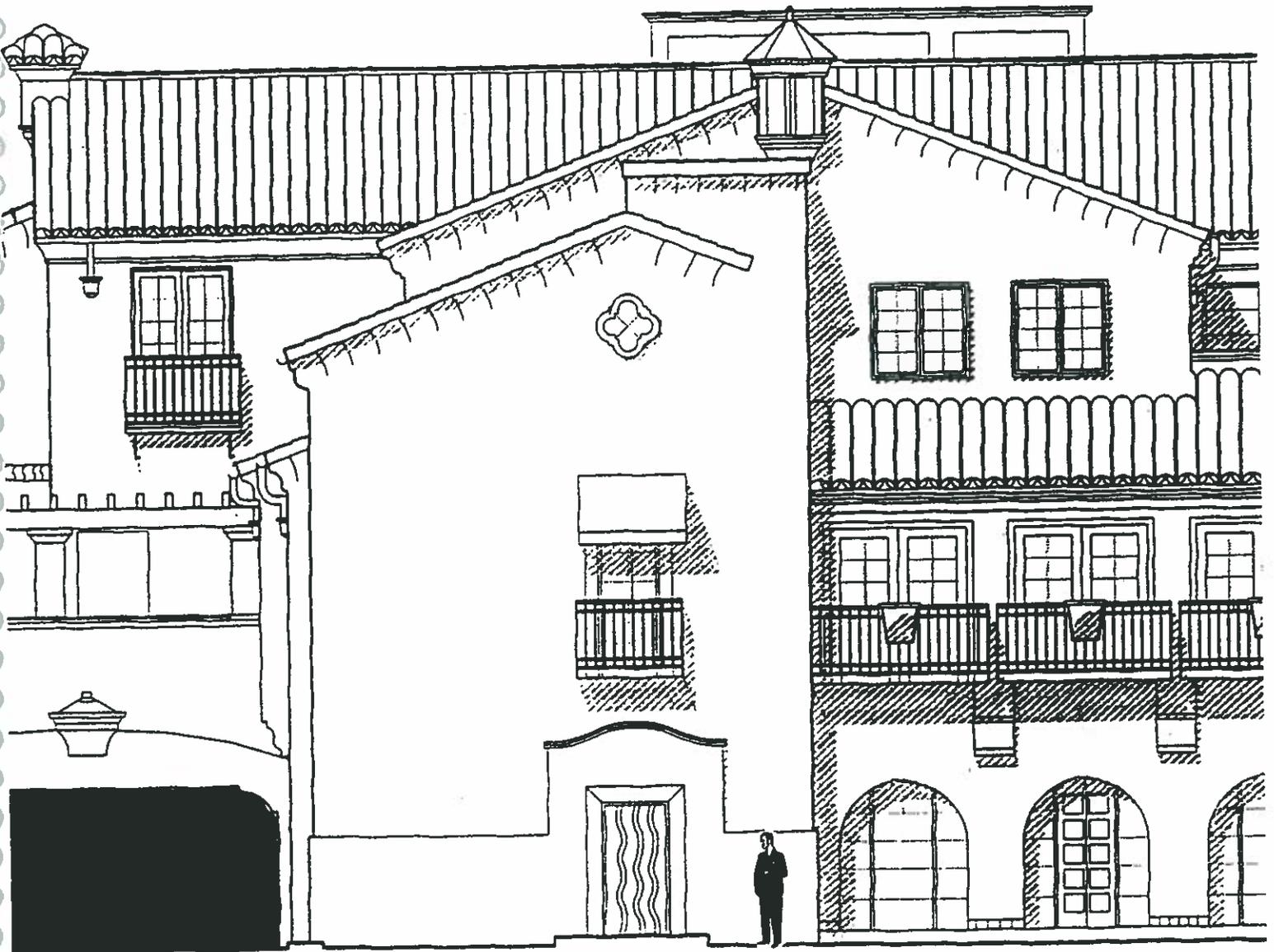


1123 Chapala Street

$1/8'' = 1'-0''$

## Scale Study

5'-8" man compared with a 7' high "river of life" door  
and the 8' arches.



1111 Chapala Street

1/8"=1'-0"

# Scale Study

5'-8" man compared with the 9' high main] entry door and the 12' high window above.



1021 Anacapa Street  
18"=1'-0"

# Scale Study

5'-8" man compared with the 13'-6" archway flanked by 22' high pilasters



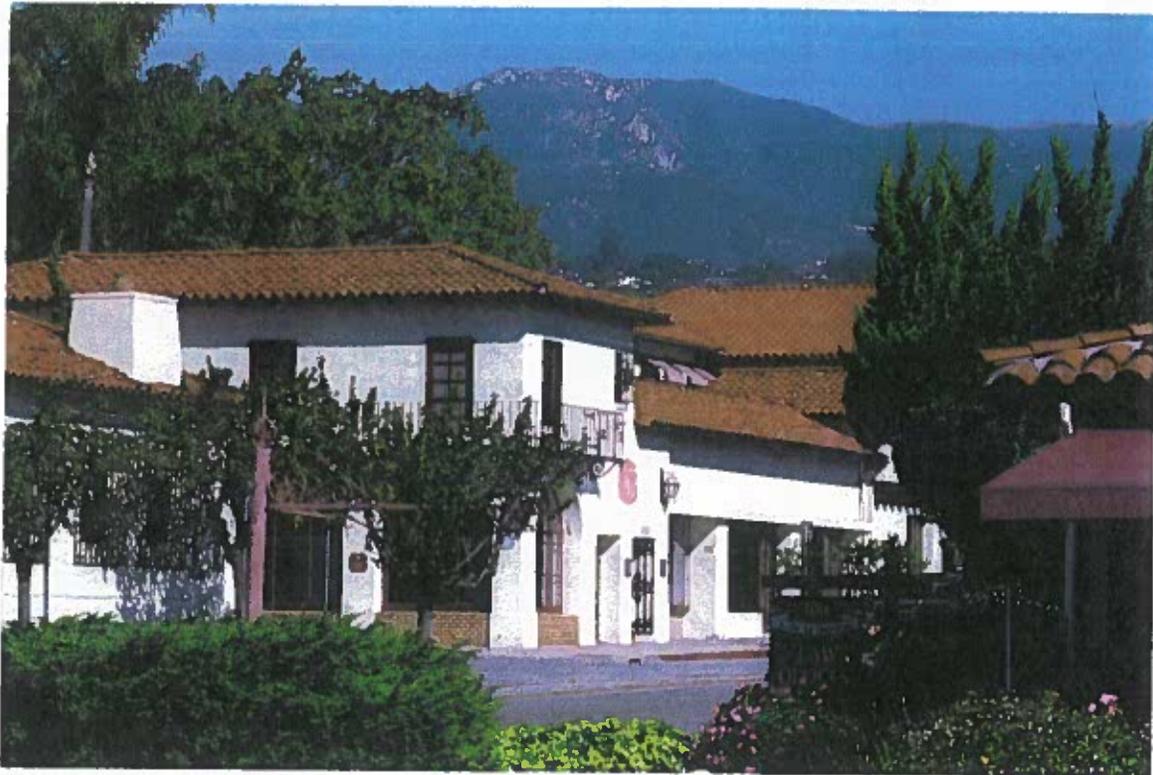
Chapala Lofts  
Gutierrez Street Elevation  
1/8"=1'-0"

### **Mountain View Blockage**

Views of the mountains can be blocked by one story buildings, depending on the location of the observer. However, views of the mountains may not be blocked by a two story building as shown in photo, fig. 4 below.

This view is seen from the east side of Anacapa across from the city hall and looking north toward Presidio Ave.

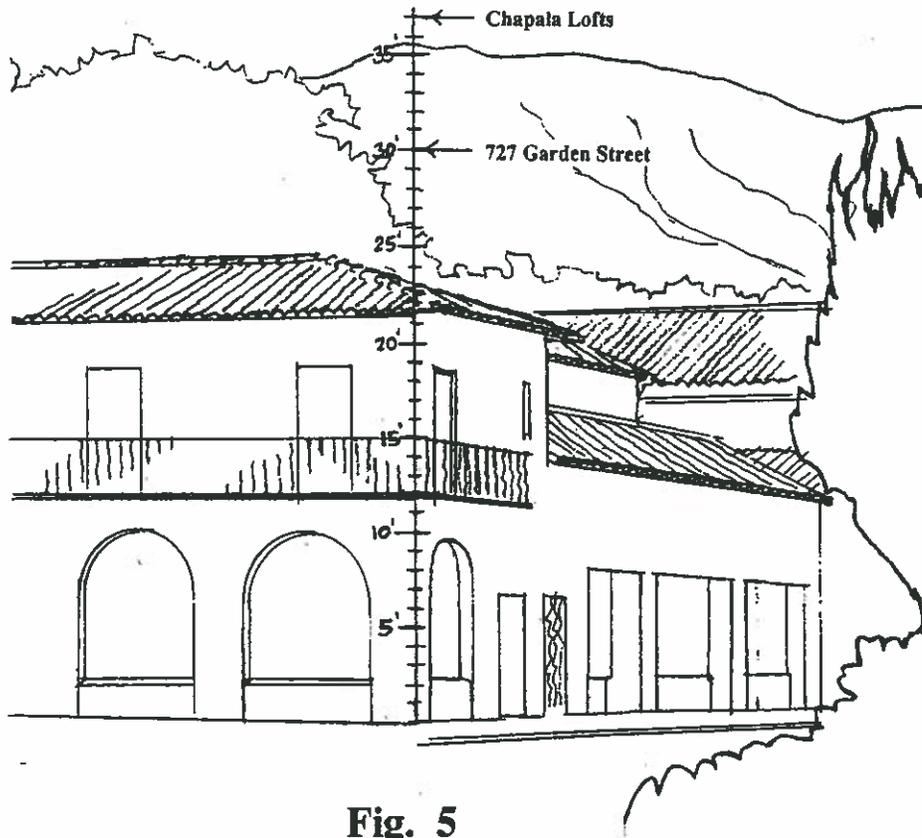
The two story Presidio Ave. building is very modest, having an approximate upper plate height of 22 ft. and a ridge of approximately 26 ft. Both of these dimensions are much smaller than those of the 8 buildings which we have examined in this study.



**Fig. 4**

Shown in fig. 5 is an exact drawing of the previous photo, enlarged to a scale of 1"=10'. A scale has been placed at the corner of this 2 story building so that the heights of other buildings in this study can be compared.

For the comparison I have chosen 727 Garden shown on page 11 and the Chapala Lofts shown on page 18. Both of these buildings are 2 story at their foremost corner, the former having its second story plate height at 30' and the latter having a second story plate height of 37'. Both are marked on the scale and it can be observed that 727 Garden would obscure about half of the existing mountain view and Chapala Lofts would obscure most of the existing mountain view. The other six buildings in this study would fall somewhere in between.



**Fig. 5**

In conclusion, it is granted that the Presidio Ave. building is unusually low and probably obsolete by today's design standards. But, giving it 3 extra feet in height for new duct work, etc. it still would protect most of the views. It seems that a greater effort should be expended by our architects and design reviewers to minimize the height of buildings where significant mountain views would be impacted.

## Tools

### 1. **Graph Analysis**

A graph similar to Graph 1, shown on page 9 will be helpful in assisting the design reviewers and the public in evaluating the size of the proposed building in relationship to other existing buildings and to its vertical envelope. The proposed design's fl. to fl., plate and ridge heights and its EAR should be calculated and inserted into the graph, so that they can be compared with the existing statistics from this study, or statistics from neighboring buildings.

### 2. **Step-back Evaluation Analysis**

Color coding elevations similar to the one shown on page 14 will be helpful in assisting design reviewers and the public in evaluating the architectural offsets and step-backs of the proposed project. The purpose of this analysis is to better understand the degree to which the elements of the design are stepped back from one another. These step-backs are not readily apparent in a typical elevation drawing. The *bulk* of a building is greatly mitigated by adequate architectural step-backs, as viewed from the public street. This analysis tool is especially important when evaluating larger buildings, where *size*, *bulk*, *mass* and *scale* are issues of concern.

### 3. **Vertical Envelope Analysis**

The Vertical Envelope is defined as the area resulting from multiplying the height from average finished grade to the height limit (45 or 60 feet depending on the zone) and the length of the site on which the architectural elevation is drawn, as shown on pages 5 through 8. The area of the building elevation is calculated and divided by the Vertical Envelope area. This calculation produces a ratio or percentage of the buildings area to the Vertical Envelope area. This quantity is known as the Elevation Area Ratio (EAR), and one below 0.40 is low while one above 0.60 is high.

The purpose of calculating an EAR is to better understand to what degree a design is filling up its buildable space. It is recognized that excessive *bulk* occurs when a design tends to fill its available Vertical Envelope. It should be noted that the 45' height limit was intended for three story buildings, and the 60' height limit was intended for four story buildings. When a three story building in the 60' height zone exceeds a 45' height, concern by the design reviewers is warranted. The reasons for this additional height should be carefully analyzed and understood, in relation to *size*, *bulk* and compatibility to the neighboring buildings.

The EARs for the eight buildings examined in this study are shown on pages 5 through 8. The calculations of the façade areas do not include the areas of chimneys, towers and other architectural amenities, and were scaled from the 1/32" drawings.

### 4. **Perspective Analysis and Streetscape Elevations**

As demonstrated by the photographs with their respective drawn elevations, shown on pages 11 through 18, the photographs are much more telling and truthful in representing readily visible composition of a proposed project. Of course, photographs cannot be taken

of a building that has not yet been built. But, models can be photographed or perspectives can be drawn, both of which are superior to elevation drawings, which do not represent a building as we actually see it.

Photos of models and perspective drawings are tools necessary so that the design reviewers can visually experience the building accurately. These pictures should illustrate the building as it would be seen as viewed from eye level and across the street. If people and autos are shown in the photos or renderings they should be in scale with the building, so as not to distort the true *size*, *bulk* and *scale* of the building being represented. The *size* of a building will appear smaller when oversized cars and people are placed in front of it.

It is also a very helpful tool to provide a streetscape elevation, in which the proposed project is shown in elevation between its neighboring buildings. Furthermore, it is helpful to see these streetscape elevations with and without trees.

**In conclusion**, the four tools described above and the definitions and analysis used in this report should be helpful in assisting design reviewers and the public in better visualizing and evaluating proposed architecture. It should also help architects and their clients by providing a review process that is clearly defined and equally administered for all.

## Glossary

**Bulk** The qualitative readily visible composition and perceived shape of the structure's volume, ie. the design of its architectural composition, shape and scale, including setbacks and stepbacks.

**Elements** 1. Portions of a building which appear separated from other portions of the same building. 2. The elements of a facade, ie. the doors, windows, arches, details, etc.

**Elevation** The flat scale drawing of the facade of a building.

**Finish Floor** The plane of the floor or average of floor planes from which a vertical measurement is taken.

**Floor to Floor** A vertical measurement of the distance from one floor to the next floor above.

**Highest Plate** 1. The top of a wall or parapet shown in an elevation drawing. 2. The top of the wall on which the lower portion of a sloping roof rests.

**Highest Ridge** The highest horizontal edge of a roof shown on an elevation drawing or seen in perspective.

**Human Scale** The aspect of architecture in which its elements are in proportion to the height of an average human. (See **Scale**)

**Major Eave** The highest eave shown on an elevation drawing or seen in perspective.

**Mass** The quantitative characteristics of a building, ie. the measure of its height, length, openness and solidity. (See **Volume**)

**Massing** The arrangement of the elements that make up a structure's bulk, including openness and solidity.

**Neighborhood Compatibility** See the Architectural Board of Review's Guidelines and the Single Family Residential Design Guidelines.

**Perspective** A picture or drawing of a building that shows it as it appears to the eye.

**Pitch** The slope of a roof as expressed by the ratio of the vertical height in feet to 12' of horizontal length, ie. 4 to 12, 8 to 12, etc.

**Scale** The proportions of a building or its elements, with reference to a definite unit of measure. (See **Human Scale**)

**Size** The length and height of a building, or elements of a building, measured from its elevation, and excluding towers, chimneys and other architectural appendages.

**Stepbacks** The varying distances of the different elements of a structure's facade, measured from the property line.

**Vertical Building Envelope** The vertical height set by the Zoning Ordinance, nominally, 45' for three story buildings and 60' for four story buildings times the width of the site.

**Volume** A structure's quantitative measurement of height, width and depth.

Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<b>Issue</b>	<b>WHAT TYPES OF LAND USE SHOULD BE PERMITTED IN THE CITY OF REDONDO BEACH?</b>
<b>Goal 1A General</b>	Provide for the types and mix of land uses necessary to serve the needs of existing and future residents.
<b>Objective 1.1</b>	Ensure that lands are designated to accommodate the housing, commercial, employment, educational, recreational, cultural, social, and aesthetic needs of the residents and that they are developed to maintain and enhance the quality and character of the City awareness, efficient planning, and effective response to these continually evolving issues.
<b>Objective 1.2</b>	Provide for the continuation of existing and new development of housing to meet the diverse economic and physical needs of the City's residents.
<b>Policies 1.2.3</b>	Allow for the development of housing types intended to meet the special needs of senior citizens, the physically challenged, and low and moderate income households in areas classified as Multi-Family Residential (“R-2,” “R-3,” “RMD,” and “RH”), Mixed Use (“MU-1,” “MU-2,” and “MU-3”) and Commercial Regional (“CR”) on the Land Use Plan map provided that they are designed to be compatible with adjacent residential structures and other areas designated for other categories of use provided that no substantial adverse impacts will occur ( <i>II.1</i> ).
<b>Policies 1.2.4</b>	Allow for the development of housing for senior citizens by permitting such housing to vary from the development standards in the zone in which it is located (subject to approval of a Conditional Use Permit and Planning Commission Design Review) in areas classified as Multi-Family Residential (“R-3,” “RMD,” and “RH”), Commercial (“C-2,” “C-3” and “C-4”), Mixed Use (“MU-1,” “MU-2,” and “MU-3”) and Commercial Regional (“CR”) on the Land Use Plan map provided that a) it is appropriate at the proposed location; b) it is located within a reasonable walking distance of commercial retail, professional, and social and community services patronized by senior citizens, or has its own private shuttle bus that will provide daily access to these services, or be within a reasonable walking distance of a bus or transit stop providing access to these services; and c) the project includes units affordable to lower-income or moderate-income households to the extent feasible.

Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<p><b>Goal 1H</b> <b>General Corridor</b></p>	<p><b>Artesia Boulevard:</b> Continue and enhance existing commercial districts which contribute revenue to the City and are compatible with adjacent residential neighborhoods.</p> <p>In addition to establishing policies to assure quality design, the principal strategy for Artesia Boulevard is to divide the corridor into four sub-areas. This has been done in an attempt to change Artesia Boulevard from a long, largely undifferentiated corridor into distinct Sub-areas, each with its own functional and design emphasis. Each sub-area was developed to be compatible with the prevailing character of existing development and to enhance trends that were already occurring.</p> <p>The implementation of these sub-areas should allow future development in each area to be more coordinated and compatible, while creating a discernible pattern of diversity as one travels the length of the corridor.</p>
<p><b>Objective 1.15</b></p>	<p>Provide for the evolutionary development of Artesia Boulevard into four distinct sub-areas which reflect and reinforce the existing primary activity areas and adjacent land uses, are oriented and accessible to the needs of nearby residents, and differentiated by use, density/intensity, and physical form and character.</p>
<p><b>Policies 1.15.1</b> <i>Function and Permitted Uses</i></p>	<p>Accommodate land uses and provide for a physical form and scale of development which differentiates Artesia Boulevard into the four following sub-areas: Blossom Lane to west of Flagler Lane: developed as a mixed-use node, integrating residential with community-serving commercial uses (Sub-Area 3);</p>
<p style="text-align: center;"><b>Artesia Boulevard Corridor</b></p> <p style="text-align: center;"><b>Sub-Area 3: Mixed-Use Corridor-Blossom to West of Flagler Lane</b></p> <p>This is one of several areas within the City that has been designated for "mixed use." The mixed use designation permits commercial development by itself (and is therefore a commercial designation), but also permits the option of constructing residential units on the upper floors of a development with commercial uses on the ground floor. To complement the incorporated residential units, an emphasis is placed on a "pedestrian-oriented" character of the commercial component as described under the preceding sub-area.</p> <p>The concept of mixing commercial and residential uses has been gaining in popularity in many cities. Traditional planning practice has dictated that residential uses should be physically separated and buffered from other types of "conflicting" uses. More recent experience, however, has shown that when properly planned and designed, mixed use</p>	

## Existing General Plan Goals-Objectives-Policies for Mixed Use Development

developments can create a unique and positive environment for residents and businesses alike.

In mixed use developments, residential units are located and designed to provide sufficient privacy and security, while commercial uses are located and designed to provide easy accessibility and good visibility to the public.

While separated in this manner, the two types of uses also enjoy the benefits of their mutual proximity. For residents, they have the convenience and added dimension of having desirable retail businesses within a short walk. For businesses, they can draw vitality from having a "round-the-clock" source of patronage. This adds a type of "energy" to a development that would not exist if it were strictly commercial.

There are also several other potential benefits of mixed use development. These include (1) enhancing the opportunities for redevelopment of an area that may be currently lacking in vitality; (2) introducing a new and interesting form of development into the city; (3) increasing affordable housing opportunities and providing an alternative type of housing; and (4) helping to curb traffic congestion by decreasing the need for automobile trips.

This segment of Artesia Boulevard was designated for mixed use primarily because it is in substantial need of revitalization, and mixed use is viewed as a viable means of achieving this. Within this area only ("MU-1"), an option has also been provided for strictly residential development, provided that the entire side of a block is developed for this use. This is intended to provide yet another option for the revitalization of this area.

<b>Objective 1.18</b>	Provide for the development of local-serving pedestrian-oriented commercial uses and integration of multi-family residential on the upper floors or in intervening clusters along the corridor, provided that they are compatible with adjacent commercial uses.
<b>Policies 1.18.1 <i>Permitted Uses</i></b>	Accommodate the development of pedestrian-oriented retail, professional office, and other related land uses as permitted by Policies 1.16.1 and 1.17.2 on parcels designated as "MU-1" ( <i>II.1</i> ).
<b>Policies 1.18.2 <i>Permitted Uses</i></b>	Accommodate residential uses on the second floor or higher of structures developed with commercial uses on the lower levels on parcels designated as "MU-1" ( <i>II.1</i> ).
<b>Policies 1.18.3 <i>Permitted Uses</i></b>	Allow for the development of multi-family residential uses where the entirety of the block frontage is developed for this use on parcels designated as "MU-1" ( <i>II.1</i> ).
<b>Policies 1.18.4 <i>Density/ Intensity and Height</i></b>	Permit development of sites exclusively for commercial uses to a maximum intensity of a floor area ratio of 0.5 and height of two stories (30 feet) ( <i>II.1</i> ).

Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<p><b>Policies 1.18.5</b> <i>Density/ Intensity and Height</i></p>	<p>Permit the development of mixed-use structures integrating residential with commercial uses to a maximum intensity of a floor area ratio of 1.5 and three stories (45 feet), providing that:</p> <ul style="list-style-type: none"> <li>a. all floor area exceeding the ratio of 0.7 is developed for residential units;</li> <li>b. the maximum residential density does not exceed 35 units per net acre; and</li> <li>c. a minimum floor area ratio of 0.3 is developed for commercial uses <b>(II.1)</b>.</li> </ul>
<p><b>Policies 1.18.6</b> <i>Density/ Intensity and Height</i></p>	<p>Permit the development of sites exclusively for residential uses to a maximum density of 35 units per net acre and three stories (45 feet) provided that the entire designated block frontage is developed for this use <b>(I 1.1)</b>.</p>
<p><b>Policies 1.18.7</b> <i>Design and Development</i></p>	<p>Require that commercial and mixed-use structures be designed to promote pedestrian activity in accordance with Policy 1.17.5 <b>(II.1, II.7, I 1.18)</b>.</p>
<p><b>Policies 1.18.8</b> <i>Design and Development</i></p>	<p>Require that mixed-use (commercial and residential) structures be designed to mitigate potential conflicts between the commercial and residential uses (e.g., noise, lighting, security, and automobile access) and provide adequate amenities for residential occupants <b>(II.1, II.7, II.18)</b>.</p>
<p><b>Policies 1.18.9</b> <i>Design and Development</i></p>	<p>Require that building elevations above the second floor be set back in accordance with Policy 1.16.3 <b>(II.1)</b>.</p>
<p><b>Policies 1.18.10</b> <i>Design and Development</i></p>	<p>Require that sites developed exclusively for residential use incorporate elements to ensure their compatibility with adjacent commercial uses, including the following:</p> <ul style="list-style-type: none"> <li>a. buffer the residential from the commercial use by the use of walls, landscape, horizontal and vertical setbacks;</li> <li>b. adequately mitigate the noise, traffic, and lighting impacts of adjacent commercial uses;</li> <li>c. provide passive recreation open space on-site;</li> <li>d. provide adequate security; and</li> <li>e. prevent impacts on the integrity and continuity of other commercial uses <b>(II.1, II.7, II.8)</b>.</li> </ul>
<p><b>Policies 1.18.11</b> <i>Design and Development</i></p>	<p>Require that projects developed exclusively for residential use be designed and sited to convey a high quality character in accordance with Policy 1.13.3 <b>(II.1, II.7, II.18)</b>.</p>

Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<b>Policies 1.18.12</b> <i>Design and Development</i>	Require that sites exclusively developed for residential use provide on-site open space amenities which are designed and sized to be accessible to and usable by tenants <b>(II.1)</b> .
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Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<p><b>Goal 1H</b> <b>General Corridor</b></p>	<p><b>Pacific Coast Highway:</b> Continue and enhance existing commercial districts which contribute revenue to the City and are compatible with adjacent residential neighborhoods.</p> <p>Pacific Coast Highway is South Redondo's main commercial street and north-south artery. Previously, almost the entire length of Pacific Coast Highway was under one commercial zoning designation. In the development of the General Plan, one of the objectives for Pacific Coast Highway was to differentiate sections of the corridor in terms of both the types and intensities of uses. The intent of this strategy was to (1) provide aesthetic relief and contrast along this long linear corridor, and (2) enhance the economic vitality of the corridor by "breaking up" the supply of land for different uses.</p> <p>The economic study prepared for the General Plan showed that there was too much commercial development capacity in relation to forecasted demand. By changing some portions of Pacific Coast Highway to multiple-family residential, this creates a better balance between the supply of land and the economic demand for commercial and residential uses</p>
<p style="text-align: center;"><b>Sub-Area 1: Mixed-Use Node-Palos Verdes Boulevard and South</b></p> <p>For a general discussion of mixed use development, see Artesia Boulevard: Sub-Area 3.</p> <p>This area was designated for mixed use development ("MU-3") primarily because of its physical suitability for development of this scale. In particular, this area features lot depths in excess of 300 feet and is adjoined to the rear by high density apartment complexes situated at a higher elevation. Because of these factors, this area is more capable of supporting larger scale, higher intensity development without creating undue impacts. This fairly large area also provides a significant opportunity for the production of new affordable multiple-family housing.</p> <p>The standards for this area include a few differences from other mixed use areas. These are (1) residential units can be developed on the ground floor of buildings located behind buildings with ground floor commercial uses; and (2) the minimum commercial floor area ratio of 0.3 applies only the first 130 feet of property depth. These standards were instituted since it was not felt to be economically or physically practicable to extend commercial uses entirely to the rear of these relatively deep sites.</p>	
<p><b>Objective 1.21</b></p>	<p>Provide for the development of community-serving retail and office commercial and mixed-use projects integrating residential with commercial uses southeast of Palos Verdes Boulevard as a primary activity center of the City.</p>

Existing General Plan Goals-Objectives-Policies for Mixed Use Development

<p><b>Policies 1.21.1</b> <i>Permitted Uses</i></p>	<p>Accommodate the development of pedestrian-oriented retail, professional office, and related land uses as permitted by Policies 1.16.1 and 1.17.2 on parcels designated as “MU-3” (II.1).</p>
<p><b>Policies 1.21.2</b> <i>Permitted Uses</i></p>	<p>Accommodate residential uses according to the following standards: a. along the street frontage: on the second floor or higher of structures developed with commercial uses on the lower levels; and b. structures located behind street-facing mixed retail and residential buildings: on any floor (including the ground floor) or on the second level or higher with retail or parking located on the ground floor (II.1).</p>
<p><b>Policies 1.21.3</b> <i>Density/Intensity and Height</i></p>	<p>Permit development of sites exclusively for commercial uses to a maximum intensity of a floor area ratio of 1.0 and height of two stories (30 feet) (II.1).</p>
<p><b>Policies 1.21.4</b> <i>Density/Intensity and Height</i></p>	<p>Permit the development of mixed-use structures integrating residential with commercial uses to a maximum intensity of a floor area ratio of 1.5 and three stories (45 feet), providing that: a. all floor area exceeding the ratio of 0.7 is developed for residential units; b. the maximum residential density for “market-rate” units does not exceed 35 units per net acre; c. residential densities exceeding 35 units per net acre shall be developed for units affordable for low and moderate income households; and d. a minimum floor area ratio of 0.3, applied to the first 130 feet of property depth from Pacific Coast Highway, is developed for commercial uses. (This shall not be interpreted to limit the siting of commercial uses to the first 130 feet of lot depth) (I 1.1).</p>
<p><b>Policies 1.21.5</b> <i>Design and Development</i></p>	<p>Require that commercial and mixed-use structures be designed to promote pedestrian activity in accordance with Policy 1.17.5 (II.1, II.7, I 1.18).</p>
<p><b>Policies 1.21.6</b> <i>Design and Development</i></p>	<p>Require that mixed-use structures be designed to mitigate potential conflicts in accordance with Policy 1.18.8 (II.1, II.7, II.18).</p>
<p><b>Policies 1.21.7</b> <i>Design and Development</i></p>	<p>Require that new development be sited and designed to convey a “village” character, including the: a. siting of structures on common pedestrian walkways, courtyards, and other open spaces; b. incorporation of arcades and other setbacks along the street frontage;</p>

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	<p>c. use of multiple building volumes and masses to reduce the “sense” of large scale “boxes” and create a visual fabric of multiple buildings;</p> <p>d. incorporation of extensive facade modulation and articulation and design details;</p> <p>e. use of roofline and height variations to break up massing and provide visual interest;</p> <p>f. use of unified architectural design styles;</p> <p>g. clear identification of building entrances;</p> <p>h. extensive use of landscape (planting beds, raised planters, containers, or window boxes) which provides a three-dimensional character; and</p> <p>i. use of pedestrian-oriented signage (e.g., projecting signs) (<i>II.1, II.7, I 1.18</i>).</p>
<p><b>Policies 1.21.8</b> <i>Design and Development</i></p>	<p>Require that building elevations above the second floor be set back in accordance with Policy 1.16.3 (<i>II.1</i>).</p>

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**Sub-Area 7: Mixed-Use Node-Torrance Boulevard Intersection**

For a general discussion of mixed use development, see Artesia Boulevard: Sub-Area 3.

Pacific Coast Highway and Torrance Boulevard is the most prominent and highly trafficked intersection in South Redondo. Taking advantage of this, this area was designated for mixed use ("MU-3") to make this location into a focal point of activity within South Redondo. Special attention will be given to encourage the type of design and uses that will make the area distinctive in terms of both appearance and activity.

Reference should also be made to the Harbor/Civic Center Specific Plan, Pacific Coast Highway Sub-Area, Zone 6, which establishes additional standards and policies for this area.

<b>Objective 1.27</b>	Provide for the development of a higher intensity pedestrian-oriented activity node containing community-oriented commercial uses and/or mixed-use development projects, integrating residential with commercial uses, as a primary activity area of the City.
<b>Policies 1.27.1 Permitted Uses</b>	Accommodate the development of pedestrian-oriented retail, professional office, and related land uses as permitted by Policies 1.16.1 and 1.17.2 on parcels designated as "MU-3" (II.1).
<b>Policies 1.27.2 Permitted Uses</b>	Accommodate residential uses in accordance with Policy 1.18.2 (II.1).
<b>Policies 1.27.3 Density/Intensity and Height</b>	Permit development of sites exclusively for commercial uses to a maximum intensity of a floor area ratio of 1.0 and height of two stories (30 feet) (II.1).
<b>Policies 1.27.4 Density/Intensity and Height</b>	Permit the development of mixed-use structures integrating residential with commercial uses to a maximum intensity of a floor area ratio of 1.5 and three stories (45 feet), providing that: a. all floor area exceeding the ratio of 0.7 is developed for residential units; b. the maximum residential density does not exceed 35 units per net acre; and c. a minimum floor area ratio of 0.3 is developed for commercial uses (I 1.1).
<b>Policies 1.27.5 Design and Development</b>	Require that commercial and mixed-use structures be designed to promote pedestrian activity in accordance with Policy 1.17.5 (II.1, II.7, I 1.18).
<b>Policies 1.27.6 Design and Development</b>	Require that mixed-use structures be designed to mitigate potential conflicts in accordance with Policy 1.18.8 (II.1, II.7, II.18).

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<b>Policies 1.27.7</b> <i>Design and Development</i>	Require that building elevations above the second floor be set back in accordance with Policy 1.16.3 (II.1).
<b>Policies 1.27.8</b> <i>Design and Development</i>	Require that any development projects involving multiple parcels site and design buildings to convey a “village” character, in accordance with Policy 1.21.7(II.1, II.7, II.18).
<b>Policies 1.27.9</b> <i>Design and Development</i>	Implement streetscape improvements in the public areas at the intersection of Pacific Coast Highway and Torrance Boulevard including, but not limited to the use of decorative/aesthetic materials and colors for crosswalks and/or sidewalks, distinctive public signage, street trees, street furniture, and similar elements (II.17).

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\* **CodeAlert:** This topic has been affected by Ordinance No. [3146-15](#). To view amendments and newly added provisions, please refer to the [CodeAlert Amendment List](#).

**10-2.900 Specific purposes, MU-1, and MU-3 mixed-use zones, and CR regional commercial zone.**

In addition to the general purposes listed in Section 10-2.102, the specific purposes of the MU-1 and MU-3 mixed-use zones and the CR regional commercial zone regulations are to:

- (a) Encourage residential uses in conjunction with commercial activities in order to create an active street life, enhance the vitality of businesses, and reduce vehicular traffic;
- (b) Provide appropriately located areas consistent with the General Plan for a full range of neighborhood and community-oriented retail sales, services, professional offices, and other commercial uses;
- (c) Strengthen the City's economic base, and provide employment opportunities close to home for residents of the City;
- (d) Ensure that commercial and residential uses in a development are designed to be compatible with each other;
- (e) Ensure that the appearance and effects of buildings and uses are harmonious with the character of the area in which they are located;
- (f) Accommodate the development of regional-serving commercial uses in areas designated CR (Regional Commercial);
- (g) Ensure that the primary character of mixed-use developments should be commercial in nature so as to integrate with and enhance the quality of the surrounding business districts;
- (h) Ensure that high quality, usable, public open spaces are provided within mix-use developments for purposes of aesthetics, social interaction, internal and external connectivity.

(Ord. 2756 c.s., eff. January 18, 1996, as amended by § 3, Ord. 3076 c.s., eff. July 7, 2011)

** 10-2.910 Land use regulations: MU-1, MU-3, MU-3A, MU-3B, and MU-3C mixed-use zones, and CR regional commercial zone.**

In the following schedule the letter "P" designates use classifications permitted in the specified zone, the letter "C" designates use classifications permitted subject to approval of a Conditional Use Permit, as provided in Section 10-2.2506, and the letter "A" designates use classifications permitted subject to approval of an Administrative Use Permit, as provided in Section 10-2.2507. Where there is neither a "P," a "C," nor an "A" indicated under a specified zone, or where a use classification is not listed, that classification is not permitted. The "Additional Regulations" column references regulations located elsewhere in the Municipal Code.

<b>Use Classifications</b>	<b>MU-1</b>	<b>MU-3</b>	<b>MU-3A</b> <b>MU-3B</b> <b>MU-3C</b>	<b>CR</b>	<b>Additional Regulations</b> <b>See Section:</b>
<b>Residential Uses</b>					
Multi-family residential	C	C	C	C	10-2.911(b)
Condominiums	C	C	C	C	10-2.911(b)
Family day care home, small	P	P	P	P	
Family day care home, large	P	P	P	P	
Residential care, limited	P	P	P	P	
<b>Commercial Uses</b>					
Animal sales and services:					
Animal feed and supplies	P	P	P	P	
Animal grooming	C	C	C	C	10-2.911(a)
Animal hospitals	C	C	C	C	10-2.911(a)
Animal sales	C	C	C	C	10-2.911(a)
Artist's studios	P	P	P	P	
Banks and savings and loans with drive-up service	P C	P C	P C	P C	10-2.911(a)
Bars and cocktail lounges	C	C	C	C	10-2.1600
Business and trade schools	C	C	C	C	
Commercial printing, limited	P	P	P	P	
Commercial recreation	C	C	C	C	10-2.1600
Communications facilities	C	C	C	C	
Drive-up services	C	C	C	C	10-2.911(a)
Food and beverage sales:					
30,000 sq. ft. or less floor area	P	P	P	P	
More than 30,000 sq. ft. floor area	C	C	C	C	10-2.911(c)
Hotels	C	C	C	C	
Liquor stores	C	C	C	C	10-2.1600
Maintenance and repair services	P	P	P	P	
Offices	P	P	P	P	10-2.911(d)
Personal convenience services	P	P	P	P	
Personal improvement services:					Except music studios 2,000 sq. ft. or less

Use Classifications	MU-1	MU-3	MU-3A MU-3B MU-3C	CR	Additional Regulations See Section:
1,000 sq. ft. or less floor area – max. 10 occupants	P	P	P	P	floor area require an AUP
1,001 - 2,000 sq. ft. floor area	A	A	A	A	10-2.2507
2,001 sq. ft. or greater floor area	C	C	C	C	
Plant nurseries	C	C	C	C	
Recycling collection facilities:					10-2.1616
Reverse vending machines	P	P	P	P	10-2.911(a)
Small collection facilities	C	C	C	C	10-2.911(a)
Restaurants:					
2,000 sq. ft. or less floor area with no drive-up service	P	P	P	P	
More than 2,000 sq. ft. floor area or with drive-up service	C	C	C	C	
Retail sales:					
30,000 sq. ft. or less floor area	P	P	P	P	
More than 30,000 sq. ft. floor area	C	C	C	C	10-2.911(c)
Snack shops	P	P	P	P	
Thrift shops	C	C	C	C	10-2.1600
Vehicle sales and services:					10-2.911(a); 10-2.1602
Service stations	—	C	—	—	
Car wash	—	C	C (Not MU-3C)	—	
<b>Other Uses</b>					
Adult day care centers	C	C	C	C	
Antennae for public communications	C	C	C	C	
Child day care centers	C	C	C	C	
Churches	C	C	C	C	
Clubs and lodges	C	C	C	C	
Cultural institutions	C	C	C	C	
Government offices	P	P	P	P	10-2.911(d)
Parking lots	C	C	C	C	

Use Classifications	MU-1	MU-3	MU-3A MU-3B MU-3C	CR	Additional Regulations See Section:
Public safety facilities	C	C	C	C	
Public utility facilities	C	C	C	C	10-2.1614
Recreation facilities	C	C	C	C	
Schools, public or private	C	C	C	C	
Senior housing	C	C	C	C	10-2.1624

(Ord. 2756 c.s., eff. January 18, 1996, as amended by Ord. 2801 c.s., eff. June 5, 1997, § 2, Ord. 2818 c.s., eff. May 21, 1998, § 9, Ord. 2927 c.s., eff. March 17, 2004, § 4, Ord. 3076 c.s., eff. July 7, 2011, and § 8, Ord. 3077 c.s., eff. July 7, 2011)

**10-2.911 Additional land use regulations: MU-1, MU-3, MU-3A, MU-3B, and MU-3C mixed-use zones, and CR regional commercial zone.**

(a) **Commercial uses prohibited in mixed-use projects.** The following commercial uses are prohibited when located on a site containing both residential and commercial uses:

- (1) Animal grooming; animal hospitals; animal sales.
- (2) Bars and cocktail lounges.
- (3) Drive-up services associated with any commercial use.
- (4) Liquor stores.
- (5) Recycling collection facilities.
- (6) Service stations.
- (7) Thrift shops.
- (8) Car wash.

(b) **Residential uses.** Residential dwelling units may only be located on the second floor and higher of structures developed with commercial uses on the lower levels, with the following exceptions:

(1) **MU-1 zone.** In the MU-1 zone, lots may be developed exclusively for residential use where the entirety of the block frontage is developed exclusively for residential use.

(2) **MU-3A zone.** In the MU-3A zone, residential dwelling units may be located on any floor in structures located behind street-facing commercial or mixed-use structures, or above parking on the ground floor in structures located behind street-facing commercial or mixed-use structures.

(c) **Uses exceeding 30,000 square feet.** In the MU-1, MU-3, MU-3A, MU-3B, and MU-3C zones, uses exceeding 30,000 square feet shall be prohibited except where they are designed to be compatible with the intended pedestrian-oriented character of the zone, pursuant to the requirements for a Conditional Use Permit (Section 10-2.2506).

(d) **Offices.** Offices may occupy up to a maximum of fifty (50%) percent of the linear frontage of the building in all mixed-use zones, except that such ground floor uses along the street frontage are permitted in the MU-3C zone within the Riviera Village overlay zone (see Section 10-2.1315).

(Ord. 2756 c.s., eff. January 18, 1996, as amended by § 3, Ord. 2818 c.s., eff. May 21, 1998, and § 5, Ord. 3076 c.s., eff. July 7, 2011)

### **10-2.912 Performance standards: MU-1, MU-3, MU-3A, MU-3B, and MU-3C mixed-use zones, and CR regional commercial zone.**

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(a) **Purpose.** The purpose of this section is to ensure that residential uses in mixed-use zones are not adversely impacted by the adjacent commercial uses, including, but not limited to, traffic, noise, and safety impacts. In the interests of both the residents and the businesses, no Conditional Use Permit shall be approved for a mixed-use project combining residential and commercial uses on the same site, unless the project is designed to meet the following performance standards, in addition to all other applicable regulations of this chapter.

(1) **Noise.**

a. Residential units shall be constructed so that interior noise levels do not exceed an Ldn of 45 dB(A) in any habitable room.

b. Commercial uses shall be designed and operated, and hours of operation limited where appropriate, so that neighboring residents are not exposed to offensive noise, especially from traffic or late-night activity. No amplified music shall be audible to neighboring residents.

c. Common walls between residential and nonresidential uses shall be constructed to minimize the transmission of noise and vibration.

(2) **Security.**

a. The residential units shall be designed to ensure the security of residents, including, but not limited to, the provision of separate and secured entrances and exits that are directly accessible to secured parking areas.

b. Nonresidential and residential uses located on the same floor shall not have common entrance hallways or common balconies.

c. Parking spaces for nonresidential and residential uses shall be specifically designated by posting, pavement marking, and/or physical separation.

(3) **Lighting.**

a. All outdoor lighting associated with commercial uses shall be designed so as not to adversely impact surrounding residential uses, while also providing a sufficient level of illumination for access and security purposes. Such lighting shall not blink, flash, oscillate, or be of unusually high intensity of brightness.

b. Parking areas shall be illuminated so as to provide appropriate visibility and security during hours of darkness.

(4) **Odors, dust, vibration.** No commercial use shall be designed or operated so as to expose residents to offensive odors, dust, electrical interference, and/or vibration.

(5) **Refuse storage and location.** The residential units shall maintain a separate refuse storage container from that used by the commercial uses. It shall be clearly marked for residential use only and use by commercial uses is prohibited.

(Ord. 2756 c.s., eff. January 18, 1996, as amended by Ord. 2786 c.s., eff. January 2, 1997, and § 6, Ord. 3076 c.s., eff. July 7, 2011)

### **10-2.913 Development standards: MU-1 mixed-use zone.**

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(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 0.5.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be no more than one unit for each 1,245 square feet of lot area.

(c) **Minimum lot size, mixed-use projects.** 15,000 square feet of lot area.

(d) **Building height.** (See definition of building height in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.

(3) **Residential uses.** For projects containing only residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.

(e) **Stories.** (See definition of story in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building shall exceed two (2) stories.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building shall exceed three (3) stories.

(3) **Residential uses.** For projects containing only residential uses, no building shall exceed three (3) stories.

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback average of five (5) feet, but at no point less than three (3) feet the full width of the lot, except as follows (see setback averaging in Section 10-2.1520):

1. Display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade.

2. Where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.

b. **Maximum permitted.** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.**

a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.

b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:

1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;

2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot; and

b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(4) **Second story setback.** The second story shall have a minimum setback of fifteen (15) feet from any property line abutting a street.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **Usable public open space.** Spaces such as public plazas, public walkways and other public spaces of at least ten (10%) percent of the F.A.R. shall be provided.

(1) Public open space shall be accessible to the public and not be fenced or gated so as to prevent public access.

(2) Public open space shall be contiguous to the maximum extent feasible.

(3) Areas less than ten (10) feet in width shall not count as public open space.

(4) The requirement of ten (10%) percent public open space may be modified by the Planning Commission for projects developed on lots less than 20,000 square feet in size.

(i) **General regulations.** See Article 3 of this chapter.

(j) **Parking regulations.** See Article 5 of this chapter.

(k) **Sign regulations.** See Article 6 of this chapter.

(l) **Landscaping regulations.** See Article 7 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(Ord. 2756 c.s., eff. January 18, 1996, as amended by § 7, Ord. 3076 c.s., eff. July 7, 2011)

### **10-2.915 Development standards: MU-3 mixed-use zone.**

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(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be no more than one unit for each 1,245 square feet of lot area.

(c) **Minimum lot size, mixed-use projects.** 15,000 square feet of lot area.

(d) **Building height.** (See definition of building height in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.

(e) **Stories.** (See definition of story in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building shall exceed two (2) stories.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building shall exceed three (3) stories.

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback of ten (10) feet the full width of the lot, except as follows:

1. Display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade.

2. Unenclosed pedestrian arcades, outdoor dining areas, and similar unenclosed features contributing to a pedestrian-oriented environment may project seven (7) feet into the required setback.

3. Where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.

b. **Maximum permitted.** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.**

a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.

b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:

1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;

2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot;

b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(4) **Second story setback.** The second story shall have a minimum setback of eighteen (18) feet from any property line abutting a street.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **Usable public open space.** Spaces such as public plazas, public walkways and other public spaces of at least ten (10%) percent of the F.A.R. shall be provided.

(1) Public open space shall be accessible to the public and not be fenced or gated so as to prevent public access.

(2) Public open space shall be contiguous to the maximum extent feasible.

(3) Areas less than ten (10) feet in width shall not count as public open space.

(4) The requirement of ten (10%) percent public open space may be modified by the Planning Commission for projects developed on lots less than 20,000 square feet in size.

(i) **General regulations.** See Article 3 of this chapter.

(j) **Parking regulations.** See Article 5 of this chapter.

(k) **Sign regulations.** See Article 6 of this chapter.

(l) **Landscaping regulations.** See Article 7 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(Ord. 2756 c.s., eff. January 18, 1996, as amended by § 8, Ord. 3076 c.s., eff. July 7, 2011)

### **10-2.916 Development standards: MU-3A mixed-use zone.**

(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area of 0.3 multiplied by the lot area within 130 feet of the property line abutting Pacific Coast Highway.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be no more than one unit for each 1,245 square feet of lot area.

(c) **Minimum lot size, mixed-use projects.** 15,000 square feet of lot area.

(d) **Building height.** (See definition of building height in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.

(e) **Stories.** (See definition of story in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building shall exceed two (2) stories.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building shall exceed three (3) stories.

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback of ten (10) feet the full width of the lot, except as follows:

1. Display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade.

2. Unenclosed pedestrian arcades, outdoor dining areas, and similar unenclosed features contributing to a pedestrian-oriented environment may project seven (7) feet into the required setback.

3. Where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.

b. **Maximum permitted.** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.**

a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.

b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:

1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;

2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

- a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot;
- b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(4) **Second story setback.** The second story shall have a minimum setback of eighteen (18) feet from any property line abutting a street.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **Usable public open space.** Spaces such as public plazas, public walkways and other public spaces of at least ten (10%) percent of the F.A.R. shall be provided.

(1) Public open space shall be accessible to the public and not be fenced or gated so as to prevent public access.

(2) Public open space shall be contiguous to the maximum extent feasible.

(3) Areas less than ten (10) feet in width shall not count as public open space.

(4) The requirement of ten (10%) percent public open space may be modified by the Planning Commission for projects developed on lots less than 20,000 square feet in size.

(i) **General regulations.** See Article 3 of this chapter.

(j) **Parking regulations.** See Article 5 of this chapter.

(k) **Sign regulations.** See Article 6 of this chapter.

(l) **Landscaping regulations.** See Article 7 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(Ord. 2756 c.s., eff. January 18, 1996, as amended by § 9, Ord. 3076 c.s., eff. July 7, 2011)

### **10-2.917 Development standards: MU-3B mixed-use zone.**

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(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be no more than one unit for each 1,245 square feet of lot area.

(c) **Minimum lot size, mixed-use projects.** 15,000 square feet of lot area.

(d) **Building height.** (See definition of building height in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.

(e) **Stories.** (See definition of story in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building shall exceed two (2) stories.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building shall exceed three (3) stories.

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback of ten (10) feet the full width of the lot, except as follows:

1. Display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade.

2. Unenclosed pedestrian arcades, outdoor dining areas, and similar unenclosed features contributing to a pedestrian-oriented environment may project seven (7) feet into the required setback.

3. Where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.

b. **Maximum permitted.** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.**

a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.

b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:

1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;

2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot;

b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(4) **Second story setback.** The second story shall have a minimum setback of eighteen (18) feet from any property line abutting a street.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **Usable public open space.** Spaces such as public plazas, public walkways and other public spaces of at least ten (10%) percent of the F.A.R. shall be provided.

(1) Public open space shall be accessible to the public and not be fenced or gated so as to prevent public access.

(2) Public open space shall be contiguous to the maximum extent feasible.

(3) Areas less than ten (10) feet in width shall not count as public open space.

(4) The requirement of ten (10%) percent public open space may be modified by the Planning Commission for projects developed on lots less than 20,000 square feet in size.

(i) **General regulations.** See Article 3 of this chapter.

(j) **Parking regulations.** See Article 5 of this chapter.

(k) **Sign regulations.** See Article 6 of this chapter.

(l) **Landscaping regulations.** See Article 7 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(Ord. 2756 c.s., eff. January 18, 1996, as amended by § 10, Ord. 3076 c.s., eff. July 7, 2011)

### **10-2.918 Development standards: MU-3C mixed-use zone.**

(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be no more than one unit for each 1,245 square feet of lot area.

(c) **Minimum lot size, mixed-use projects.** 15,000 square feet of lot area.

(d) **Building height.** For projects including both commercial and residential uses, no building or structure shall exceed a height of thirty-eight (38) feet, except that building heights or structures up to a maximum of forty-five (45) feet may be approved upon portions of the lot, subject to Planning Commission Design Review.

(e) **Stories.** No building shall exceed three (3) stories (see definition of story in Section 10-2.402).

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback of three (3) feet the full width of the lot, except that display windows may project to the front property line, provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade. However, where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.

b. **Maximum permitted.** The front setback shall not exceed ten (10) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.**

a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.

b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:

1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;

2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot;

b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-2.2502).

(4) **Second story setback for residential uses.** All residential uses on the second floor shall be set back from the first floor building elevation facing the street, pursuant to Planning Commission Design Review (Section 10-2.2502), in order to provide appropriate separation from activity in the public right-of-way.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **Usable public open space.** Spaces such as public plazas, public walkways and other public spaces of at least ten (10%) percent of the F.A.R. shall be provided.

(1) Public open space shall be accessible to the public and not be fenced or gated so as to prevent public access.

(2) Public open space shall be contiguous to the maximum extent feasible.

(3) Areas less than ten (10) feet in width shall not count as public open space.

(4) The requirement of ten (10%) percent public open space may be modified by the Planning Commission for projects developed on lots less than 20,000 square feet in size.

(i) **General regulations.** See Article 3 of this chapter.

(j) **Parking regulations.** See Article 5 of this chapter.

(k) **Sign regulations.** See Article 6 of this chapter.

(l) **Landscaping regulations.** See Article 7 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(§ 4, Ord. 2818 c.s., eff. May 21, 1998, as amended by § 11, Ord. 3076 c.s., eff. July 7, 2011)

### **10-2.919 Development standards: CR regional commercial zone.**

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(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-2.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 1.0 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be one unit for each 1,245 square feet of lot area.

(c) **Minimum lot size, mixed-use projects.** 15,000 square feet of lot area.

(d) **Building height.** No building or structure shall exceed a height of sixty (60) feet (see definition of building height in Section 10-2.402).

(e) **Stories.** No building shall exceed four (4) stories (see definition of story in Section 10-2.402).

(f) **Setbacks.** Setbacks shall be determined pursuant to Planning Commission Design Review (see Section 10-2.2502).

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-2.1510).

(h) **Usable public open space.** Spaces such as public plazas, public walkways and other public spaces of at least ten (10%) percent of the F.A.R. shall be provided.

(1) Public open space shall be accessible to the public and not be fenced or gated so as to prevent public access.

(2) Public open space shall be contiguous to the maximum extent feasible.

(3) Areas less than ten (10) feet in width shall not count as public open space.

(4) The requirement of ten (10%) percent public open space may be modified by the Planning Commission for projects developed on lots less than 20,000 square feet in size.

(i) **Parking requirements.** The parking provisions of Article 5 of this chapter shall apply, except that an allowance for overlapping the parking requirements of activities having non-simultaneous usage peaks may be permitted pursuant to Planning Commission Design Review.

(j) **General regulations.** See Article 3 of this chapter.

(k) **Parking regulations.** See Article 5 of this chapter.

(l) **Sign regulations.** See Article 6 of this chapter.

(m) **Landscaping regulations.** See Article 7 of this chapter.

(n) **Procedures.** See Article 12 of this chapter.

(Ord. 2756 c.s., eff. January 18, 1996, as amended by § 4, Ord. 2818 c.s., eff. May 21, 1998, and § 12, Ord. 3076 c.s., eff. July 7, 2011)

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[Title 10 PLANNING AND ZONING](#)  
[Chapter 5 COASTAL LAND USE PLAN IMPLEMENTING ORDINANCE](#)  
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**Division 4. MU Mixed-Use Zones\***

\* **CodeAlert:** This topic has been affected by Ordinance No. [3147-15](#). To view amendments and newly added provisions, please refer to the [CodeAlert Amendment List](#).

**10-5.900 Specific purposes: MU-2 and MU-3 mixed-use zones.**

In addition to the general purposes listed in Section 10-5.102, the specific purposes of the MU-2 and MU-3 mixed use zones are to:

- (a) Encourage residential uses in conjunction with commercial activities in order to create an active street life, enhance the vitality of businesses, and reduce vehicular traffic;
- (b) Provide appropriately located areas consistent with the Coastal Land Use Plan for a full range of neighborhood and community-oriented and visitor serving retail sales, services, professional offices, and other commercial uses;
- (c) Strengthen the City’s economic base, and provide employment opportunities close to home for residents of the City;
- (d) Ensure that commercial and residential uses in a development are designed to be compatible with each other;
- (e) Ensure that the appearance and effects of buildings and uses are harmonious with the character of the area in which they are located.

(§ 1, Ord. 2905 c.s., eff. August 5, 2003, as amended by § 7, Ord. 2971 c.s., eff. September 2, 2005 [effective date pending subject to litigation])

** 10-5.910 Land use regulations: MU-2, MU-3, MU-3B and MU-3C mixed-use zones.**

In the following schedule the letter “P” designates use classifications permitted in the specified zone and the letter “C” designates use classifications permitted subject to approval of a Conditional Use Permit, as provided in Section 10-5.2506. Where there is neither a “P” nor a “C” indicated under a specified zone, or where a use classification is not listed, that classification is not permitted. The “Additional Regulations” column references regulations located elsewhere in the Municipal Code.

Use Classifications	MU-2	MU-3	MU-3B MU-3C	Additional Regulations See Section:
<b>Residential Uses</b>				
Multi-family residential	C	C	C	10-5.911(b)
Condominiums	C	C	C	10-5.911(b)
Family day care home, small	P	P	P	

Family day care home, large	P	P	P	
Residential care, limited	P	P	P	
<b>Commercial Uses</b>				
Animal sales and services:				
Animal feed and supplies	P	P	P	
Animal grooming	C	C	C	10-5.911(a)
Animal hospitals	C	C	C	10-5.911(a)
Animal sales	C	C	C	10-5.911(a)
Artist's studios	P	P	P	
Banks and savings and loans	P	P	P	
with drive-up service	C	C	C	10-5.911(a)
Bars and cocktail lounges	C	C	C	10-5.1600
Business and trade schools	C	C	C	
Commercial printing, limited	P	P	P	
Commercial recreation	C	C	C	10-5.1600
Communications facilities	C	C	C	
Drive-up services	C	C	C	10-5.911(a)
Food and beverage sales:				
30,000 sq. ft. or less floor area	P	P	P	
more than 30,000 sq. ft. floor area	C	C	C	10-5.911(c)
Hotels and motels	C	C	C	10-5.911(a)
Liquor stores	C	C	C	10-5.1600
Maintenance and repair services	P	P	P	
Offices	P	P	P	10-5.911(d)
Personal convenience services	P	P	P	
Personal improvement services	C	C	C	
Plant nurseries	C	C	C	
Recycling collection facilities:				10-5.1616
Reverse vending machines	P	P	P	10-5.911(a)
Small collection facilities	C	C	C	10-5.911(a)
Restaurants:				
2,000 sq. ft. or less floor area with no drive-up service	P	P	P	
more than 2,000 sq. ft. floor area or with drive-up service	C	C	C	

Retail sales: less than 30,000 sq. ft. floor area	P	P	P	
30,000 sq. ft. or more floor area	C	C	C	10-5.911(c)
<b>Use Classifications</b>	<b>MU-2</b>	<b>MU-3</b>	<b>MU-3B MU-3C</b>	<b>Additional Regulations See Section:</b>
Snack shops	P	P	P	
Thrift shops	C	C	C	10-5.1600
Vehicle sales and services: Service stations	—	C	—	10-5.1602; 10-5.911(a)
<b>Other Uses</b>				
Adult day care centers	C	C	C	
Antennae for public communications	C	C	C	
Child day care centers	C	C	C	
Churches	C	C	C	
Clubs and lodges	C	C	C	
Cultural institutions	C	C	C	
Government offices	P	P	P	10-5.911(d)
Parking lots	C	C	C	
Public safety facilities	C	C	C	
Public utility facilities	C	C	C	10-5.1614
Recreation facilities	C	C	C	
Schools, public or private	C	C	C	
Senior housing	C	C	C	10-5.1624

(§ 1, Ord. 2905 c.s., eff. August 5, 2003, as amended by § 10, Ord. 2985 c.s., eff. June 16, 2006, and § 7, Ord. 2971 c.s., eff. September 2, 2005 [effective date pending subject to litigation])

#### **10-5.911 Additional land use regulations: MU-2 and MU-3 mixed-use zones.**

(a) **Commercial uses prohibited in mixed-use projects.** The following commercial uses are prohibited when located on a site containing both residential and commercial uses:

- (1) Animal grooming, animal hospitals, animal sales;
- (2) Bars and cocktail lounges;
- (3) Drive-up services associated with any commercial use;
- (4) Hotels and motels;
- (5) Liquor stores;
- (6) Recycling collection facilities;

- (7) Service stations;
- (8) Thrift shops.

(b) **Residential uses.** Residential dwelling units may only be located on the second floor and higher of structures developed with commercial uses on the lower levels, with the following exception:

- (1) **MU-2 zone.** In the MU-2 zone lots may be developed exclusively for residential use.

(c) **Uses exceeding 30,000 square feet.** In the MU-3, MU-3B and MU-3C zones uses exceeding 30,000 square feet shall be prohibited except where they are designed to be compatible with the intended pedestrian-oriented character of the zone, pursuant to the requirements for a Conditional Use Permit (Section 10-5.2506).

(d) **Offices.** Offices are permitted only on the second floor and/or above, or on the ground floor to the rear of other permitted retail or service uses provided that the pedestrian character of the corridor is not disrupted, except that such ground floor uses along the street frontage are permitted in the MU-3C zone within the Riviera Village overlay zone (see Section 10-5.1315).

(§ 1, Ord. 2905 c.s., eff. August 5, 2003, as amended by § 7, Ord. 2971 c.s., eff. September 2, 2005 [effective date pending subject to litigation])

### **10-5.912 Performance standards: MU-2 and MU-3 mixed-use zones.**

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(a) **Purpose.** The purpose of this section is to ensure that residential uses in mixed-use zones are not adversely impacted by the adjacent commercial uses, including, but not limited to traffic, noise, and safety impacts. In the interests of both the residents and the businesses, no Conditional Use Permit shall be approved for a mixed-use project combining residential and commercial uses on the same site, unless the project is designed to meet the following performance standards, in addition to all other applicable regulations of this chapter.

- (1) **Noise.**

a. Residential units shall be constructed so that interior noise levels do not exceed an Ldn of 45 dB(A) in any habitable room.

b. Commercial uses shall be designed and operated, and hours of operation limited where appropriate, so that neighboring residents are not exposed to offensive noise, especially from traffic or late-night activity. No amplified music shall be audible to neighboring residents.

c. Common walls between residential and nonresidential uses shall be constructed to minimize the transmission of noise and vibration.

- (2) **Security.**

a. The residential units shall be designed to ensure the security of residents, including, but not limited to, the provision of separate and secured entrances and exits that are directly accessible to secured parking areas.

b. Nonresidential and residential uses located on the same floor shall not have common entrance hallways or common balconies.

c. Parking spaces for nonresidential and residential uses shall be specifically designated by posting, pavement marking, and/or physical separation.

- (3) **Lighting.**

a. All outdoor lighting associated with commercial uses shall be designed so as not to adversely impact surrounding residential uses, while also providing a sufficient level of illumination for access and security purposes. Such lighting shall not blink, flash, oscillate, or be of unusually high intensity or brightness.

b. Parking areas shall be illuminated so as to provide appropriate visibility and security during hours of darkness.

(4) **Odors, dust, vibration.** No commercial use shall be designed or operated so as to expose residents to offensive odors, dust, electrical interference, and/or vibration.

(5) **Refuse storage and location.** The residential units shall maintain a separate refuse storage container from that used by the commercial uses. It shall be clearly marked for residential use only and use by commercial uses is prohibited.

(§ 1, Ord. 2905 c.s., eff. August 5, 2003, as amended by § 7, Ord. 2971 c.s., eff. September 2, 2005 [effective date pending subject to litigation])

#### **10-5.914 Development standards: MU-2 mixed-use zone.**

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(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (FAR) of all buildings on a lot shall not exceed 0.7.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (FAR) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be one unit for each 1,245 square feet of lot area.

(c) **Minimum lot size, mixed-use projects.** No projects containing both commercial and residential uses shall be permitted on lots with less than 15,000 square feet of lot area.

(d) **Building height.** (See definition of building height in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building or structure shall exceed a height of forty-five (45) feet.

(3) **Residential uses.** For projects containing only residential uses, no building or structure shall exceed a height of forty-five (45) feet.

(e) **Stories.** (See definition of “story” in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building shall exceed two (2) stories.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building shall exceed three (3) stories.

(3) **Residential uses.** For projects containing only residential uses, no building shall exceed three (3) stories.

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback of fifteen (15) feet the full width of the lot, except that display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three feet above the adjacent sidewalk grade.

b. **Maximum permitted.** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.** There shall be a minimum side setback of not less than ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot. No side setback shall be required along the interior lot lines.

(3) **Rear setback.** There shall be a rear setback of not less than ten (10) feet the full length of the lot.

(4) **Second story setback.** The second story shall have a minimum setback of twenty-five (25) feet from any property line abutting a street.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **General regulations.** See Article 3 of this chapter.

(i) **Parking regulations.** See Article 5 of this chapter.

(j) **Sign regulations.** See Article 6 of this chapter.

(k) **Landscaping regulations.** See Article 7 of this chapter.

(l) **Coastal Development Permits.** See Article 10 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(n) **Water Quality Measures.** See Chapter 7, Title 5 of the Redondo Beach Municipal Code.

(§ 7, Ord. 2971 c.s., eff. September 2, 2005 [effective date pending subject to litigation])

#### **10-5.915 Development standards: MU-3 mixed-use zone.**

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(a) **Floor area ratio.** (See definition of floor area ratio in Section 10-5.402).

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be one unit for each 1,245 square feet of lot area, not to exceed thirty-five (35) dwelling units per net acre except one unit may be constructed on any legal lot as defined in Section 10-5.402 and Section 10-5.1528 of this chapter.

(c) **Minimum lot size, mixed-use projects.** No projects containing both commercial and residential uses shall be permitted on lots with less than 15,000 square feet of lot area.

(d) **Building height.** (See definition of “building height” in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building or structure shall exceed a height of forty-five (45) feet.

(e) **Stories.** (See definition of “story” in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building shall exceed two (2) stories.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building shall exceed three (3) stories.

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback of ten (10) feet the full width of the lot, except as follows:

1. Display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade.

2. Unenclosed pedestrian arcades, outdoor dining areas, and similar unenclosed features contributing to a pedestrian-oriented environment may project seven (7) feet into the required setback.

3. Where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.

b. **Maximum permitted.** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.**

a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.

b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:

1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;

2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-5.2502).

(3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot;

b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-5.2502).

(4) **Second story setback.** The second story shall have a minimum setback of eighteen (18) feet from any property line abutting a street.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **General regulations.** See Article 3 of this chapter.

(i) **Parking regulations.** See Article 5 of this chapter.

(j) **Sign regulations.** See Article 6 of this chapter.

(k) **Landscaping regulations.** See Article 7 of this chapter.

(l) **Coastal Development Permits.** See Article 10 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(n) **Water Quality Measures.** See Chapter 7, Title 5 of the Redondo Beach Municipal Code.

(§ 1, Ord. 2905 c.s., eff. August 5, 2003)

#### **10-5.917 Development standards: MU-3B mixed-use zone.**

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(a) **Floor area ratio.** (See definition of “floor area ratio” in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.

(2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:

a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.

b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.

(b) **Residential density.** The maximum number of dwelling units on a lot shall be one unit for each 1,245 square feet of lot area, not to exceed thirty-five (35) dwelling units per net acre except one unit may be constructed on any legal lot as defined in Section 10-5.402 and Section 10-5.1528 of this chapter.

(c) **Minimum lot size, mixed-use projects.** No projects containing both commercial and residential uses shall be permitted on lots with less than 15,000 square feet of lot area.

(d) **Building height.** (See definition of “building height” in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building or structure shall exceed a height of thirty (30) feet.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building or structure shall exceed a height of forty-five (45) feet.

(e) **Stories.** (See definition of “story” in Section 10-5.402.)

(1) **Commercial uses.** For projects containing only commercial uses, no building shall exceed two (2) stories.

(2) **Mixed-use.** For projects including both commercial and residential uses, no building shall exceed three (3) stories.

(f) **Setbacks.** The minimum setback requirements shall be as follows:

(1) **Front setback.**

a. **Minimum required.** There shall be a minimum front setback of ten (10) feet the full width of the lot, except as follows:

1. Display windows may project three (3) feet into the required front setback provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade.

2. Unenclosed pedestrian arcades, outdoor dining areas, and similar unenclosed features contributing to a pedestrian-oriented environment may project seven (7) feet into the required setback.

3. Where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.

b. **Maximum permitted.** In commercial or mixed-use projects, the front setback shall not exceed fifteen (15) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.

(2) **Side setback.**

a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.

b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:

1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;

2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-5.2502).

(3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot;

b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-5.2502).

(4) **Second story setback.** The second story shall have a minimum setback of eighteen (18) feet from any property line abutting a street.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space (see standards for outdoor living space in Section 10-5.1510).

(h) **General regulations.** See Article 3 of this chapter.

(i) **Parking regulations.** See Article 5 of this chapter.

(j) **Sign regulations.** See Article 6 of this chapter.

(k) **Landscaping regulations.** See Article 7 of this chapter.

(l) **Coastal Development Permits.** See Article 10 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(n) **Water Quality Measures.** See Chapter 7, Title 5 of the Redondo Beach Municipal Code. (§ 1, Ord. 2905 c.s., eff. August 5, 2003)

#### **10-5.918 Development standards: MU-3C mixed-use zone.**

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- (a) **Floor area ratio.** (See definition of “floor area ratio” in Section 10-5.402.)
- (1) **Commercial uses.** For projects containing only commercial uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.0.
- (2) **Mixed-use.** For projects including both commercial and residential uses, the floor area ratio (F.A.R.) of all buildings on a lot shall not exceed 1.5. The following shall also apply:
- a. **Maximum commercial floor area.** All floor area exceeding a floor area ratio of 0.7 shall be developed for residential uses.
- b. **Minimum commercial floor area.** The commercial component of mixed-use projects shall have a minimum floor area ratio of 0.3.
- (b) **Residential density.** The maximum number of dwelling units on a lot shall be one unit for each 1,245 square feet of lot area, not to exceed thirty-five (35) dwelling units per net acre except one unit may be constructed on any legal lot as defined in Section 10-5.402 and Section 10-5.1528 of this chapter.
- (c) **Minimum lot size, mixed-use projects.** No projects containing both commercial and residential uses shall be permitted on lots with less than 15,000 square feet of lot area.
- (d) **Building height.** No building or structure shall exceed a height of forty-five (45) feet (see definition of building height in Section 10-5.402).
- (e) **Stories.** No building shall exceed three (3) stories. (See definition of “story” in Section 10-5.402.)
- (f) **Setbacks.** The minimum setback requirements shall be as follows:
- (1) **Front setback.**
- a. **Minimum required.** There shall be a minimum front setback of three (3) feet the full width of the lot, except that display windows may project to the front property line, provided that the bottom of the projection is no less than three (3) feet above the adjacent sidewalk grade. However, where a lot is contiguous to a residentially zoned lot fronting on the same street, the required front setback shall be the same as required for the contiguous residential lot.
- b. **Maximum permitted.** The front setback shall not exceed ten (10) feet for fifty (50%) percent of the linear frontage of the building, except areas contiguous with the structure and used for outdoor dining or courtyards shall be exempt from this requirement. This setback area shall not be used for parking.
- (2) **Side setback.**
- a. There shall be a minimum side setback of ten (10) feet the full length of the lot on the street side of a corner or reverse corner lot.
- b. No side setback shall be required along the interior lot lines, except where the side lot line is contiguous to a residential zone, in which case the following standards shall apply:
1. There shall be a minimum side setback of twenty (20) feet the full length of the lot;
2. The required side setback may be modified pursuant to Planning Commission Design Review (Section 10-5.2502).
- (3) **Rear setback.** No rear setback shall be required, except where the rear lot line is contiguous to a residential zone, in which case the following standards shall apply:

a. There shall be a minimum rear setback of twenty (20) feet the full width of the lot;

b. The required rear setback may be modified pursuant to Planning Commission Design Review (Section 10-5.2502).

(4) **Second story setback for residential uses.** All residential uses on the second floor shall be set back from the first floor building elevation facing the street, pursuant to Planning Commission Design Review (Section 10-5.2502), in order to provide appropriate separation from activity in the public right-of-way.

(5) **Third story setback.** Within the first thirty (30) feet of property depth, all building elevations above the second floor shall have a minimum average setback of five (5) feet from the second floor building face.

(g) **Outdoor living space.** Each dwelling unit shall be provided a minimum of 200 square feet of outdoor living space. (See standards for outdoor living space in Section 10-5.1510.)

(h) **General regulations.** See Article 3 of this chapter.

(i) **Parking regulations.** See Article 5 of this chapter.

(j) **Sign regulations.** See Article 6 of this chapter.

(k) **Landscaping regulations.** See Article 7 of this chapter.

(l) **Coastal Development Permits.** See Article 10 of this chapter.

(m) **Procedures.** See Article 12 of this chapter.

(n) **Water Quality Measures.** See Chapter 7, Title 5 of the Redondo Beach Municipal Code.

(§ 1, Ord. 2905 c.s., eff. August 5, 2003)

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